

**FHWA-Indiana Environmental Document**  
**CATEGORICAL EXCLUSION / ENVIRONMENTAL ASSESSMENT FORM**  
**GENERAL PROJECT INFORMATION**

**Road No./County:**

South Miller Street and McKay Road / Shelby County

**Designation Number(s):**

1702775

**Project Description/Termini:**

Intersection Improvement Project

Project limits will extend approximately 584 ft. west and 250 ft. east along McKay Road and 313 ft. north and 470 ft. south along South Miller Street for a total of 1,617 ft. (0.31 mile).

X	Categorical Exclusion, Level 2 – Required Signatories: INDOT DE and/or INDOT ESD
	Categorical Exclusion, Level 3 – Required Signatories: INDOT ESD
	Categorical Exclusion, Level 4 – Required Signatories: INDOT ESD and FHWA
	Environmental Assessment (EA) – Required Signatories: INDOT ESD and FHWA
	Additional Investigation (AI) – The proposed action included a design change from the original approved environmental document. Required Signatories must include the appropriate environmental approval authority

**Approval**

\_\_\_\_\_  
INDOT DE Signature and Date

\_\_\_\_\_  
INDOT ESD Signature and Date

\_\_\_\_\_  
FHWA Signature and Date

**Release for Public Involvement**

N/A

\_\_\_\_\_  
INDOT DE Initials and Date

 8-3-2021  
\_\_\_\_\_  
INDOT ESD Initials and Date

**Certification of Public Involvement**

\_\_\_\_\_  
INDOT Consultant Services Signature and Date

**INDOT DE/ESD Reviewer Signature and Date:**

\_\_\_\_\_

**Name and Organization of CE/EA Preparer:**

Elayna Stoner, Metric Environmental, LLC

Indiana Department of Transportation

County Shelby Route South Miller Street and McKay Road Des. No. 1702775

**Part I – Public Involvement**

Every Federal action requires some level of public involvement, providing for early and continuous opportunities throughout the project development process. **The level of public involvement should be commensurate with the proposed action.**

Does the project have a historic bridge processed under the Historic Bridges PA\*? ☐ Yes ☒ No

If No, then:

Opportunity for a Public Hearing Required? ☒ Yes ☐ No

*\*A public hearing is required for all historic bridges processed under the Historic Bridges Programmatic Agreement between INDOT, FHWA, SHPO, and the ACHP.*

*Discuss what public involvement activities (legal notices, letters to affected property owners and residents (i.e. notice of entry), meetings, special purpose meetings, newspaper articles, etc.) have occurred for this project.*

Notice of Entry letters were mailed to potentially affected property owners near the project area on June 21, 2019, notifying them about the project and that individuals responsible for land surveying and field activities may be seen in the area. A sample copy of the Notice of Entry letter is provided in Appendix G.

To meet the public involvement requirements of Section 106, a legal notice of FHWA's finding of No Historic Properties Affected was published in the *Shelbyville News* on March 12, 2021 offering the public an opportunity to submit comment pursuant to 36 CFR 800.2(d), 800.3(e), and 800.6(a)(4). The public comment period closed 30 days later, on April 11, 2021. No comments or responses were received. The text of the public notice and the affidavit of publication appear in Appendix D, page D-64.

The project will meet the minimum requirements described in the current *Indiana Department of Transportation (INDOT) Public Involvement Manual* which requires the project sponsor to offer the public an opportunity to submit comment and/or request a public hearing. Therefore, a legal notice will appear in a local publication contingent upon the release of this document for public involvement. This document will be revised after the public involvement requirements are fulfilled.

**Public Controversy on Environmental Grounds**

*Discuss public controversy concerning community and/or natural resource impacts, including what is being done during the project to minimize impacts.*

At this time, there is no substantial public controversy concerning impacts to the community or to natural resources.

**Part II - General Project Identification, Description, and Design Information**

Sponsor of the Project: City of Shelbyville INDOT District: Greenfield

Local Name of the Facility: McKay Road and South Miller Street

Funding Source (mark all that apply): Federal ☒ State ☐ Local ☒ Other\* ☐

\*If other is selected, please identify the funding source: \_\_\_\_\_

**PURPOSE AND NEED:**

*The need should describe the specific transportation problem or deficiency that the project will address. The purpose should describe the goal or objective of the project. The solution to the traffic problem should NOT be discussed in this section.*

The primary need for this project is due to congestion, delays and an undesirable Level of Service (LOS). Level of Service is a term used to qualitatively describe the operating conditions of a transportation facility. It is based on factors such as speed, travel time, maneuverability and delay. There are six Levels of Service, and each level is assigned a letter designation from A to F, with LOS A representing the best operating conditions, and LOS F the worst operating conditions.

The project is located adjacent to the Shelbyville High School and Middle School facilities and the high volume of school buses, student and staff traffic combined with local traffic utilizing the intersection concurrently, results in an undesirable LOS during morning, afternoon, and evening peak travel times. The current guidelines for LOS suggest that all legs of an intersection be operating at a LOS D or better. Per the results of the Operational Analysis Report developed for the project, the current traffic volumes (2018) show a LOS D or lower for all approaches with the exception of the north and south bound thru traffic in the evening peak hour, which is currently operating at a LOS C.

**Indiana Department of Transportation**

County Shelby Route South Miller Street and McKay Road Des. No. 1702775

For the construction year (2022), the current intersection configuration is projected to be operating at a LOS E or lower based on the attached report and tables, except for the northbound thru traffic during the morning, afternoon, and evening peak travel hours. The projected traffic volumes for the design year (2042) illustrate the current intersection to be operating at a LOS F. Excerpts from the Operational Analysis Report is provided in Appendix J, page J-2 to J-7.

The purpose of the project is to achieve a LOS D, or better for the intersection during peak morning and afternoon travel times when school traffic would be the major contributor to the intersection operations.

**PROJECT DESCRIPTION (PREFERRED ALTERNATIVE):**

County: Shelby Municipality: City of Shelbyville

Limits of Proposed Work: Roadway reconstruction limits will extend approximately 584 ft. west and 250 ft. east along McKay Road. Along South Miller Street, limits will extend approximately 313 ft. north and 470 ft. south for a total of 1,617 ft.

Total Work Length: 0.31 Mile Total Work Area: 3.00 Acre(s)

Is an Interstate Access Document (IAD)<sup>1</sup> required? ☐ Yes<sup>1</sup> ☒ No

If yes, when did the FHWA provide a Determination of Engineering and Operational Acceptability? Date:

<sup>1</sup>If an IAD is required; a copy of the approved CE/EA document must be submitted to the FHWA with a request for final approval of the IAD.

*Describe location of project including township, range, city, county, roads, etc. Existing conditions should include current conditions, current deficiencies, roadway description, surrounding features, etc. Preferred alternative should include the scope of work, anticipated impacts, and how the project will meet the Purpose and Need. Logical termini and independent utility also need discussed.*

The City of Shelbyville with oversight from the Indiana Department of Transportation (INDOT) and partial funding from the Federal Highway Administration (FHWA) intend to proceed with a proposed intersection improvement project at McKay Road and South Miller Street in Shelby County. Specifically, the project is located approximately 0.87 mile west of State Road (SR) 9 in Section 7, Township 12 North, Range 7 East as illustrated on the *Shelbyville, Indiana 7.5-minute United States Geological Survey (USGS) topographic quadrangle* (Appendix B, page B-2).

South Miller Street is a north/south roadway classified as a Minor Arterial north of the intersection and a Major Collector south of the intersection with a posted speed limit of 30 miles per hour. The posted speed limit is reduced to 20 miles per hour within the school zone during designated hours. The cross-section of the north leg consists of one 15 ft. travel lane in each direction, bordered by integral concrete curbs and an enclosed storm drainage system. A 5 ft. grass buffer and concrete sidewalk is located along the west side of the north leg. The south leg consists of 11 ft. travel lanes in each direction bordered by earthen shoulders with no curb or sidewalks. There are no dedicated turn lanes for any direction of travel on either leg of South Miller Street.

McKay Road is an east/west roadway classified as a Minor Arterial with a posted speed limit of 30 miles per hour. The posted speed limit is reduced to 20 miles per hour within the school zone during designated hours. The roadway cross-section consists of one 15 ft. travel lane in each direction, bordered by integral concrete curbs and no shoulders. The pavement width is 30 ft. from curb face to curb face. A 5 ft. grass buffer and concrete sidewalk is located along the south side of the east leg and a sidewalk is located along the north side of the west leg. The south leg consists of 11 ft. travel lanes in each direction bordered by earthen shoulders with a small section of sidewalk at the southwest quadrant. There are no dedicated turn lanes for any direction of travel on either leg of McKay Road. The intersection of South Miller Street and McKay Road is a four way stop, controlled with signage.

Land use in the project area consists of a combination of residential and commercial. The Shelbyville High School and Middle School campuses are located in the northwest quadrant of the intersection, and a residential assisted living facility is located in the southwest quadrant. The northeast quadrant is occupied with single-family residential dwellings, and a single-family home is situated in the southeast quadrant, with a large set back from the roadway.

**Preferred Alternative**

The preferred alternative will construct a single-lane roundabout at the intersection. The roundabout will consist of one 20 ft. circulatory travel lane with two percent grade away from the roundabout center. The circulatory travel lane will be bordered by a 3.5 inch mountable curb between the circulatory travel lane and apron to assist with truck movements and a 6 inch concrete curb on the exterior. The inscribed diameter of the roundabout will be 40 ft. with a 10 ft. truck apron. The roadway cross-section will provide entry legs on the east and west approaches of McKay Road and the north and south approaches of South Miller Street.

Roadway widths of the approach legs will be a minimum of 15 ft. along both east and west McKay Road as well as the north approach of South Miller Street. The width of the approach roadway along the south approach of South Miller Street will be 11 ft. All four approach legs of the roundabout widen to 20 ft. just before entering the 20ft. circulatory travel lane. The lanes will be bordered by concrete curbs.

The vertical alignment of the west leg of McKay Road will be lowered by approximately 1.5 ft. to improve the roadway drainage along this stretch of road. The vertical profile along the east leg of McKay Road will remain as is. In addition, the vertical alignment of the south leg of South Miller Street will also be lowered approximately 0.75 ft. to accommodate improved drainage characteristics in this area. The vertical profile on the north leg of South Miller Street will remain unchanged. All four approach legs, with the exception of the east side of the South Miller Street approach, will be bordered by concrete curbs. The east side of the South Miller Street south approach will be bordered by a 2 ft. aggregate shoulders, beginning south of the one residential driveway located on the west side of the road. The design has been modified to allow left turns for residents in the northeast quadrant of the project so, no coordination with residents was conducted. Updated plan sheets have been included in Appendix B, pages B-19 to B-20. In addition, there are eight decorative brick columns located near the perimeter of the residence in the northeast quadrant of the intersection. These will not be disturbed and will remain in place. This has been included as a firm commitment in the *Environmental Commitments* section of this document and will be included on the final plans. The existing sign in the southwest quadrant for the Heritage House senior living facility will be removed and replaced as part of the project design. There is no longer any fencing located in the southeast quadrant of the project so, there will be no need to replace the fence or install temporary fencing during construction.

Other features of the roundabout will include raised splitter islands on each leg of the roundabout to provide pedestrian refuge between the travel lanes. Specialized pavement markings will include directional and yield arrows to assist motorists with lane direction. Sidewalks that are 8 ft. in width will be constructed in each quadrant of the roundabout. The sidewalks will be separated from the roadway by 3 ft. grass buffers. Designated Americans with Disabilities Act (ADA) compliant crosswalks will be provided as part of the new sidewalk construction in addition to being installed through the splitter islands. In addition, new, decorative lighting will also be installed. The lighting will consist of decorative post lights, approximately 30 ft. in height with downward facing pendent style luminaires. The lights will be installed in a staggered orientation and will extend approximately 400 ft. along the west leg of McKay Road and 225 ft. along the east leg. Along South Miller Street, the lights will extend approximately 412 ft. along the south leg and 225 ft. along the north leg. Project limits will extend approximately 584 ft. west and 250 ft. east along McKay Road. Along South Miller Street, improvement limits will extend approximately 313 ft. north and 470 ft. south for a total of 1,617 ft. (0.31 mile).

Project plan sheets are provided in Appendix B, pages B-8 to B-20. The project termini are logical because they encompass only the area necessary to construct the roundabout and tie in the improvements into the existing roadway for a smooth transition. The project has independent utility as it does not depend on the construction of a secondary project. Construction is anticipated to begin in late Spring 2023 with the expected closure of the intersection to start in June after general school operations have stopped for the summer. The maintenance of traffic (MOT) plan will utilize a detour as the intersection will be closed for a limited time during construction. Additional details on the MOT plan are provided in the MOT Section of this document.

The preferred alternative will meet the purpose and need of the project by providing an improved LOS D, or better that meets current and projected traffic capacity.

#### **OTHER ALTERNATIVES CONSIDERED:**

*Provide a header for each alternative. Describe all discarded alternatives, including the No Build Alternative. Explain why each discarded alternative was not selected. Make sure to state how each alternative meets or does not meet the Purpose and Need and why.*

Two other alternatives considered for the project.

##### **Alternative No. 1 Do Nothing/No Build Alternative**

The "Do Nothing/No Build" alternative would not require any action to address the concerns at the project intersection. The existing intersection would continue to function with an undesirable LOS greater than D with the vast majority of the intersection legs operating at a LOS F. This option would also not require the acquisition of additional right-of-way nor cause any environmental impacts thus minimizing the impact to the surrounding landscape. This alternative does not meet the purpose and need of the project because it would not address any of the operational concerns and was discarded from further consideration.

##### **Alternative No. 2 Signalized Intersection with Dedicated Left-Turn Lanes**

This alternative would reconfigure the intersection to include traffic signals with dedicated left-turn lanes on all approaches. The Traffic Study that was conducted in 2018 revealed that the installation of a traffic signal with left-turn lanes would improve the overall LOS from an F to a B during peak morning, afternoon and evening hours.



**Indiana Department of Transportation**

County Shelby Route South Miller Street and McKay Road Des. No. 1702775

In accordance with the federal government and INDOT there are several standard criteria (warrants) that an intersection would need to meet to have a signal installed, traffic volumes being a major justification. For this particular project, signal warrants were not met for either of the analysis years (2022 and 2042) due to lower than acceptable, current and projected average daily traffic volumes. As a result, since the traffic volumes are such that none of the required signal warrants were met, this alternative was discarded from further consideration.

**The No Build Alternative is not feasible, prudent or practicable because** (Mark all that apply):

- It would not correct existing capacity deficiencies;  
 It would not correct existing safety hazards;  
 It would not correct the existing roadway geometric deficiencies;  
 It would not correct existing deteriorated conditions and maintenance problems; or  
 It would result in serious impacts to the motoring public and general welfare of the economy.  
 Other (Describe):

<b>X</b>

**ROADWAY CHARACTER:**

*If the proposed action includes multiple roadways, complete and duplicate for each roadway.*

Name of Roadway **South Miller Street**  
 Functional Classification: Minor Arterial (north) Major Collector (south)  
 Current ADT: 5,267 VPD (2022) Design Year ADT: 7,218 VPD (2042)  
 Design Hour Volume (DHV): 9 Truck Percentage (%) 4  
 Designed Speed (mph): 30 Legal Speed (mph): 30

	<b>Existing</b>		<b>Proposed</b>	
Number of Lanes:	2		2	
Type of Lanes:	15 ft. travel lane north leg 11 ft. travel lane south leg		11 ft. entry lane on south leg 15 ft. entry lane on north leg 20 ft. circulatory travel lanes	
Pavement Width:	30 North / 22 South	ft.	30 North / 22 South	ft.
Shoulder Width:	0 North / 1 South	ft.	0 North / 3 South	ft.
Median Width:	N/A	ft.	N/A	ft.
Sidewalk Width:	5 on west side of north leg	ft.	8 west side of north leg and south leg	ft.

Name of Roadway **McKay Road**  
 Functional Classification: Minor Arterial  
 Current ADT: 6,977 VPD (2022) Design Year ADT: 9,561 VPD (2042)  
 Design Hour Volume (DHV): 10 Truck Percentage (%) 6  
 Designed Speed (mph): 30 Legal Speed (mph): 30

	<b>Existing</b>		<b>Proposed</b>	
Number of Lanes:	2		2	
Type of Lanes:	15ft. travel lane east leg and west leg		15 ft. entry lanes on east and west legs 20 ft. circulatory travel lanes	
Pavement Width:	30	ft.	30	ft.
Shoulder Width:	0	ft.	0	ft.
Median Width:	N/A	ft.	N/A	ft.
Sidewalk Width:	5 north side of west leg and south side of east leg	ft.	8 north side of west leg and south side of east leg	ft.

Setting: ☒ Urban ☐ Suburban ☐ Rural  
 Topography: ☒ Level ☐ Rolling ☐ Hilly

**Indiana Department of Transportation**

County Shelby Route South Miller Street and McKay Road Des. No. 1702775

**BRIDGES AND/OR SMALL STRUCTURE(S):**

*If the proposed action includes multiple structures, complete and duplicate for each bridge and/or small structure. Include both existing and proposed bridge(s) and/or small structure(s) in this section.*

Structure/NBI Number(s): \_\_\_\_\_ Sufficiency Rating: \_\_\_\_\_  
(Rating, Source of Information)

	<b>Existing</b>	<b>Proposed</b>
Bridge/Structure Type:		
Number of Spans:		
Weight Restrictions:	ton	ton
Height Restrictions:	ft.	ft.
Curb to Curb Width:	ft.	ft.
Outside to Outside Width:	ft.	ft.
Shoulder Width:	ft.	ft.

*Describe impacts and work involving bridge(s), culvert(s), pipe(s), and small structure(s). Provide details for small structure(s): structure number, type, size (length and dia.), location and impacts to water. Use a table if the number of small structures becomes large. If the table exceeds a complete page, put it in the appendix and summarize the information below with a citation to the table.*

There are no bridges, small structures or culverts involved with this project.

**MAINTENANCE OF TRAFFIC (MOT) DURING CONSTRUCTION:**

	<b>Yes</b>	<b>No</b>
Is a temporary bridge proposed?		<b>X</b>
Is a temporary roadway proposed?		<b>X</b>
Will the project involve the use of a detour or require a ramp closure? (describe below)	<b>X</b>	
Provisions will be made for access by local traffic and so posted.	<b>X</b>	
Provisions will be made for through-traffic dependent businesses.	<b>X</b>	
Provisions will be made to accommodate any local special events or festivals.	<b>X</b>	
Will the proposed MOT substantially change the environmental consequences of the action?		<b>X</b>
Is there substantial controversy associated with the proposed method for MOT?		<b>X</b>

*Discuss closures and/or facilities (if any) that will be provided for maintenance of traffic. Any known impacts from these temporary measures should be quantified to the extent possible, particularly with respect to properties such as Section 4(f) resources and wetlands. Any local concerns about access and traffic flow should be detailed as well.*

The maintenance of traffic (MOT) plan will require the use of a detour during the time that the intersection is completely closed for construction. The current specifications for the project call for this closure to be no more than two months. The detour will utilize Miller Avenue, CR 250 South, Colescott Street, Harrison Street (State Road 9). Traffic approaching McKay Road will be directed onto Miller Avenue at the Miller Avenue/McKay Road intersection. Traffic along South Miller Street, from the south) will be directed to utilize CR 250 South eastbound, out to SR 9 where it will be able to travel north to the east side of McKay Road or up to the north to Colescott Road where it will be able to access South Miller Street, north of the project. The various detour routes range from approximately 3.69 miles to 2.23 miles in additional travel and the additional travel times range from 5 to 10 minutes, depending on the direction of travel and destination. The MOT sheet is provided in Appendix B, page B-12.

The City of Shelbyville anticipates limiting the time associated with the detour (for total closure) to approximately two months. Access to all residential and commercial properties located north and south of the intersection will be maintained for the duration of construction. This contract specifications for construction of this project are being written such that construction of the roundabout itself will be constructed during the months of June and July when school operations are out for summer vacation. This option should minimize impacts to the Shelbyville High School and Middle School facilities. Access to the school facilities will be maintained utilizing the secondary entrances located north of the intersection on South Miller Street, and west on McKay Road. These entrances have a connecting internal road system on the school property and allow thru access to both roadways. Coordination with the school system is ongoing to ensure all access needs are incorporated into the project design. Construction of the project will contribute to temporary traffic restrictions and possible delays but, these inconveniences will cease upon project completion.

**Indiana Department of Transportation**

County Shelby Route South Miller Street and McKay Road Des. No. 1702775

**ESTIMATED PROJECT COST AND SCHEDULE:**

Engineering: \$ 367,500.00 (2021) Right-of-Way: \$ 129,000.00 (2022) Construction: \$ 3,052,500.00 (2023)

Anticipated Start Date of Construction: Spring 2023

**RIGHT OF WAY:**

Land Use Impacts	Amount (acres)	
	Permanent	Temporary
Residential	0.24	0.14
Commercial	1.55	0.06
Agricultural	0.00	0.00
Forest	0.00	0.00
Wetlands	0.00	0.00
Other:	0.00	0.00
<b>TOTAL</b>	<b>1.79</b>	<b>0.20</b>

*Describe both Permanent and Temporary right-of-way and describe their current use. Typical and Maximum right-of-way widths (existing and proposed) should also be discussed. Any advance acquisition, reacquisition or easements, either known or suspected, and their impacts on the environmental analysis should be discussed.*

There are no existing right-of-way limits recorded for South Miller Street or McKay Road. The City of Shelbyville will need to re-acquire approximately 1.22 acres of unrecorded right-of-way to construct the project. This land was not properly recorded when the intersecting roads were initially constructed so, it is now necessary that the land be recorded as owned by the City of Shelbyville as part of this undertaking. The 1.22 acres of re-acquired right-of-way consists of the pavement areas of the project roadways.

In addition, the project will require approximately 1.79 acres of new, additional permanent right-of-way. Approximately 0.24 acre of the new, additional permanent right-of-way consists of residential land in the northeast, southeast and southwest quadrants. Approximately 1.55 acres of the new, permanent right-of-way consists of commercial land in the northwest and southwest quadrants. The project will also require approximately 0.20 acre of temporary right-of-way from the residential and commercial parcels for driveway reconstruction.

The proposed permanent right-of-way limits along the south leg of South Miller Street will vary from a minimum of 25 ft. on the east side of the roadway and extend to a maximum of 125 ft. on the west side of the roadway. Along the north leg, the proposed right-of-way limits will extend approximately 20 ft. to the east and 80 ft. to the west. The proposed permanent right-of-way limits along the east leg of McKay Road will extend 15 ft. on the north side and a maximum of 60 ft. on the south side of the roadway. Along the west leg of McKay Road, the proposed right-of-way limits will extend a maximum of 110 ft. along the north side and a maximum of 35 ft. on the south side.

If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately.

**Part III – Identification and Evaluation of Impacts of the Proposed Action****SECTION A - EARLY COORDINATION:**

List the date(s) coordination was sent and all resource agencies that were contacted as a part of the development of this Environmental Study. Also, include the date of their response or indicate that no response was received.

Early coordination letters were sent on February 26, 2020, to the agencies listed below. A copy of the early coordination letter is provided in Appendix C, pages C-1 to C-2. All applicable recommendations are included in the Environmental Commitments section of this CE document.

Agency	Date Sent	Response Received	Appendix C
Indiana Department of Natural Resources Division of Fish and Wildlife (IDNR-DFW)	February 26, 2020	March 26, 2020	Page C-4
U.S. Fish and Wildlife Service (USFWS)	February 26, 2020	No Response	
U.S. Army Corps of Engineers (USACE)	February 26, 2020	No Response	
IDEM Proposed Roadway Construction Projects letter	February 26, 2020	Auto Response	Pages C-27 to C-31
US Department of Housing and Urban Development	February 26, 2020	No Response	
National Parks Service, Midwest Regional Office	February 26, 2020	No Response	
INDOT, Office of Aviation	February 26, 2020	No Response	
State Conservationist, Natural Resources Conservation Service	July 7, 2020	August 20, 2020	Page C-32
Indiana Geological and Water Survey	March 19, 2020	March 19, 2020	Pages C-25 to C-26
IDEM Wellhead Proximity Determinator	February 26, 2020	February 26, 2020	
Shelbyville Fire Department	February 26, 2020	No Response	
Shelbyville Police Department	February 26, 2020	No Response	
Shelbyville Central Schools	February 26, 2020	No Response	
Shelbyville High School	February 26, 2020	No Response	
City of Shelbyville, Mayor's Office	February 26, 2020	No Response	
Shelby County Commissioner	February 26, 2020	No Response	
Shelbyville MS4 Coordinator	February 26, 2020	No Response	
Shelbyville City Utilities	February 26, 2020	No Response	
Shelbyville Parks and Recreation	February 26, 2020	No Response	

**SECTION B – ECOLOGICAL RESOURCES:**

	<u>Presence</u>	<u>Impacts</u>	
		<u>Yes</u>	<u>No</u>
<b>Streams, Rivers, Watercourses &amp; Other Jurisdictional Features</b>			
Federal Wild and Scenic Rivers			
State Natural, Scenic or Recreational Rivers			
Nationwide Rivers Inventory (NRI) listed			
Outstanding Rivers List for Indiana			
Navigable Waterways			

Total stream(s) in project area: N/A Linear feet      Total impacted stream(s): N/A Linear feet

Stream Name	Classification	Total Size in Project Area (linear feet)	Impacted linear feet	Comments (i.e. location, flow direction, likely Water of the US, appendix reference)
<u>N/A</u>	<u>N/A</u>	<u>N/A</u>	<u>N/A</u>	<u>N/A</u>

Describe all streams, rivers, watercourses and other jurisdictional features adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if the streams or rivers are listed on any federal or state lists for Indiana. Include if features are subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.

Based on the desktop review, the 2013 aerial photograph of the project area (Appendix B, page B-3), and the water resources map in the Red Flag Investigation (RFI) report (Appendix E, page E-9), there are eight (8) streams located within the 0.5 mile search radius. Metric Environmental conducted a site visit on September 9, 2019, and it was determined that no streams, rivers, watercourses, or other jurisdictional features are present within or adjacent to the project area, therefore, no impacts are expected.

**Indiana Department of Transportation**

County Shelby Route South Miller Street and McKay Road Des. No. 1702775

A *Waters of the U.S. Determination/Wetland Delineation Report* was completed by Metric Environmental on April 29, 2020. Please refer to Appendix F for the *Waters of the U.S. Determination/Wetland Delineation Report*. It was determined that no streams or roadside ditches are located within or adjacent to the project area.

The IDNR-DFW responded on March 26, 2020, with recommendations to avoid or minimize impacts to waterways; however, since there are no waterways present in the project area there are no recommendations that are applicable (Appendix C, page C-4). The USFWS and the US Army Corps of Engineers (USACE) did not respond.

Open Water Feature(s)	Presence	Impacts	
		Yes	No
Reservoirs			
Lakes			
Farm Ponds			
Retention/Detention Basin			
Storm Water Management Facilities			
Other: _____			

Describe all open water feature(s) identified adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if features are subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.

Based on the desktop review, the 2013 aerial photograph of the project area (Appendix B, page B-3), and the water resources map in the RFI report (Appendix E, page E-9), there are two (2) open water features mapped within the 0.5 mile search radius. Metric Environmental conducted a site visit on September 9, 2019 and determined that no open water features are located within or adjacent to the project area, therefore, no impacts are expected.

A *Waters of the U.S. Determination/Wetland Delineation Report* was completed by Metric Environmental on April 29, 2020. Please refer to Appendix F for the *Waters of the U.S. Determination/Wetland Delineation Report*. It was determined that no open water features are located within or adjacent to the project area.

The IDNR-DFW response on March 26, 2020, had no recommendations regarding open water habitat (Appendix C, page C-4).

Wetlands	Presence	Impacts	
		Yes	No

Total wetland area: 0.00 Acre(s) Total wetland area impacted: 0.00 Acre(s)

(If a determination has not been made for non-isolated/isolated wetlands, fill in the total wetland area impacted above.)

Wetland No.	Classification	Total Size (Acres)	Impacted Acres	Comments (i.e. location, likely Water of the US, appendix reference)
N/A	N/A	N/A	N/A	N/A

Wetlands (Mark all that apply)	Documentation	ESD Approval Dates
Wetland Delineation		
USACE Isolated Waters Determination		

**Improvements that will not result in any wetland impacts are not practicable because such avoidance would result in** (Mark all that apply and explain):

- Substantial adverse impacts to adjacent homes, business or other improved properties;
- Substantially increased project costs;
- Unique engineering, traffic, maintenance, or safety problems;
- Substantial adverse social, economic, or environmental impacts, or
- The project not meeting the identified needs.


**Indiana Department of Transportation**

County Shelby Route South Miller Street and McKay Road Des. No. 1702775

*Describe all wetlands identified adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if features are subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.*

Based on a review of the National Wetlands Inventory (NWI) online mapper (<https://www.fws.gov/wetlands/data/Mapper.html>), the USGS topographic map (Appendix B page B-2), and the RFI report (Appendix E page E-2) there are four (4) wetlands within the 0.5 mile search radius. Metric Environmental conducted a site visit on September 9, 2019, and it was determined that no wetlands are located within or adjacent to the project area, therefore, no impacts are expected.

A *Waters of the U.S. Determination/Wetland Delineation Report* was completed by Metric Environmental on April 29, 2020. Please refer to Appendix F for the *Waters of the U.S. Determination/Wetland Delineation Report*. It was determined that no wetlands are located within or adjacent to the project area.

	<u>Presence</u>	<u>Impacts</u>
		<u>Yes</u> <u>No</u>
<b>Terrestrial Habitat</b>	<b>X</b>	<b>X</b>

Total terrestrial habitat in project area: 1.78 Acre(s)

Total tree clearing: 0.0092 Acre(s)

*Describe types of terrestrial habitat (i.e. forested, grassland, farmland, lawn, etc.) adjacent or within the project area. Include whether or not impacts will occur to habitat identified. Include total terrestrial habitat impacted and total tree clearing that will occur. Discuss measure to avoid, minimize, and mitigate if impacts will occur.*

Based on the desktop review, a site visit on September 9, 2019, by Metric Environmental, a review of the 2013 aerial photograph (Appendix B, page B-3), the predominant land use in the project area consists of commercial and residential dwellings with maintained lawns. The dominant vegetation along the project roadways consists of red fescue (*Festuca rubra*), red clover, (*Trifolium pratense*), tall false rye grass (*Schedonorus arundinaceus*) and Kentucky bluegrass (*Poa pratensis*). There are some small to moderately sized native and ornamental trees located in the northeast and northwest quadrants of the intersection. The trees consist of silver maple (*Acer saccharinum*) and red maple (*Acer rubrum*) and white oak (*Quercus alba*).

Approximately 1.78 acres of terrestrial habitat will be impacted to build the proposed roundabout. All efforts to minimize terrestrial impacts were considered during the design phase of the project. The construction limits have been reduced to the extent that is practical to build the project while limiting terrestrial disturbance and does not reflect the 0.36 acre estimate that was initially submitted through the USFWS's Information for Planning and Consultation (IPaC) portal in February 2021 (Appendix C, page C-15). Approximately four to five small, ornamental trees (0.0092 acre) will be removed from the maintained lawn area of the Shelbyville High School, in the northwest quadrant of the intersection. Re-coordination with IPaC was not initiated as this slight change would not impact the Avoidance and Mitigation Measures (AMMs) assigned to the project, or the Section 7 determination of the Endangered Species Act. The City of Shelbyville proposes to replace the trees in this general area once the project is complete.

The existing trees in the northeast quadrant will not require removal and it is not anticipated that any adverse impacts to the root systems will be incurred due to the construction. All disturbed lawn areas will be stabilized, graded and re-seeded per INDOT standard specifications. No terrestrial habitat restoration or mitigation will be necessary.

The IDNR responded on March 26, 2020, with recommendations to minimize terrestrial impacts including revegetating all bare and disturbed areas with a mixture of grasses (excluding all varieties of tall fescue) and legumes as soon as possible upon project completion. The IDNR also recommended that appropriately designed measures for controlling erosion and sediment must be implemented to prevent sediment from leavening the construction area and maintaining these measures until construction is complete and all disturbed areas are stabilized (Appendix C, page C-4). The IDEM Proposed Roadway Construction Projects letter recommended limiting disturbance, implementing temporary erosion and sediment control methods within areas of disturbed soils, and coordinating with the appropriate permitting agencies (Appendix C, pages C-27 to C-31).

**Indiana Department of Transportation**

County Shelby Route South Miller Street and McKay Road Des. No. 1702775

**Protected Species**

**Federally Listed Bats**

Information for Planning and Consultation (IPaC) determination key completed  
 Section 7 informal consultation completed (IPaC cannot be completed)  
 Section 7 formal consultation Biological Assessment (BA) required

Yes	No
<b>X</b>	
	<b>X</b>
	<b>X</b>

Determination Received for Listed Bats from USFWS: NE ☐ NLAA ☒ LAA ☐

**Other Species not included in IPaC**

Additional federal species found in project area (based on IPaC species list)  
 State species (not bird) found in project area (based upon consultation with IDNR)

Yes	No
	<b>X</b>
	<b>X</b>

**Migratory Birds**

Known usage or presence of birds (i.e. nests)  
 State bird species based upon coordination with IDNR

Yes	No
	<b>X</b>
	<b>X</b>

*Discuss IDNR coordination and species identified. Describe USFWS Section 7 consultation and determination received for Indiana bat and northern long-eared bat impacts. Discuss if other federally listed species were identified. If so, include consultation that has occurred and the determination that was received. Discuss if migratory birds have been observed and any impacts.*

Based on a desktop review and the RFI report (Appendix E) completed by Metric Environmental on May 15, 2020, the IDNR Shelby County Endangered, Threatened and Rare (ETR) Species List has been checked and is included in Appendix E, page E-12. The highlighted species on the list reflect the federal and state identified ETR species located within the county. According to the IDNR-DFW early coordination response letter, the Natural Heritage Program's Database has been checked and no plant or animal species listed as state or federally endangered, threatened and/or rare have been reported to occur in the project vicinity (Appendix C, page C-4). The USFWS did not respond to early coordination.

Project information was submitted through the USFWS's Information for Planning and Consultation (IPaC) portal, and an official species list was generated (Appendix C, pages C-18 to C-23). The project is within range of the federally endangered Indiana bat (*Myotis sodalis*) and the federally threatened northern long-eared bat (NLEB) (*Myotis septentrionalis*). No additional species were found within or adjacent to the project area, other than the Indiana bat and northern long-eared bat. On June 9, 2020, the INDOT Greenfield District Office reviewed the USFWS database and no reports of the Indiana bat or the northern long-eared bat have been documented within 0.5 mile of the project site (Appendix C, page C-24).

The project qualifies for the *Range-wide Programmatic Informal Consultation for the Indiana bat and northern long-eared bat (NLEB)*, dated May 2016 (revised February 2018), between FHWA, Federal Railroad Administration (FRA), Federal Transit Administration (FTA), and USFWS. An effect determination key was completed on February 3, 2021 and based on the responses provided, the project was found to "May Affect/Not Likely to Adversely Affect" the Indiana bat and the NLEB. INDOT reviewed and verified the effect finding on March 3, 2021 and requested USFWS's review of the finding. No response was received from USFWS within the 14-day review period; therefore, it was concluded they concur with the finding. Avoidance and Mitigation Measures (AMMs) are included as firm commitments in the *Environmental Commitments* section of this document.

This precludes the need for further consultation on this project as required under Section 7 of the Endangered Species Act, as amended. If new information on endangered species at the site becomes available, or if project plans are changed, USFWS will be contacted for consultation.

**Geological and Mineral Resources**

Project located within the Potential Karst Features Area of Indiana  
 Karst features identified within or adjacent to the project area  
 Oil/gas or exploration/abandoned wells identified in the project area

Yes	No
	<b>X</b>
	<b>X</b>
	<b>X</b>

Date Karst Study/Report reviewed by INDOT EWPO (if applicable): \_\_\_\_\_

*Discuss if project is located in Potential Karst Features Area of Indiana and if any karst features have been identified in the project area (from RFI). Discuss response received from IGWS coordination. Discuss if any mines, oil/gas, or exploration/abandoned wells were identified and if impacts will occur. Describe if any impacts will occur to any karst features. Include discussion of karst study/report was completed and results. (Karst investigation must comply with the current Karst MOU and coordinated and reviewed by INDOT EWPO)*

Based on a desktop review, the project is located outside the designated karst region of Indiana as outlined in the October 13, 1993 Memorandum of Understanding (MOU).

**Indiana Department of Transportation**

County Shelby Route South Miller Street and McKay Road Des. No. 1702775

According to a review of the topo map of the project area (Appendix B, page B-2), and the RFI report (Appendix E, page E-2) there no karst features identified within or adjacent to the project area. The Indiana Geological Survey (IGS) did not indicate that karst features exist in the project area (Appendix C, page C-25 to C-26). The IGS did identify geological hazards including a moderate liquefaction potential, a 1% annual chance flood hazard, a high potential for bedrock resources and sand and gravel resources. No documented active or abandoned mineral resource extraction sites are documented within the search radius. The aforementioned geological features will not be affected because scope of work will not involve deep excavation (i.e., greater than 2-3 feet below ground surface). Response from IGS has been communicated with the designer on March 20, 2020. No impacts are expected.

**SECTION C – OTHER RESOURCES**

**Drinking Water Resources**

Wellhead Protection Area(s)  
Source Water Protection Area(s)  
Water Well(s)  
Urbanized Area Boundary  
Public Water System(s)

**Presence**

<b>X</b>
<b>X</b>
<b>X</b>

**Impacts**

**Yes** **No**

	<b>X</b>
	<b>X</b>
	<b>X</b>

Is the project located in the St. Joseph Sole Source Aquifer (SSA):

If Yes, is the FHWA/EPA SSA MOU Applicable?

If Yes, is a Groundwater Assessment Required?

**Yes** **No**

	<b>X</b>

*Check the appropriate boxes and discuss each topic below. Provide details about impacts and summarize resource-specific coordination responses and any mitigation commitments. Reference responses in the Appendix.*

The project is located in Shelby County, which is not located within the area of the St. Joseph Sole Source Aquifer, the only legally designated sole source aquifer in the state of Indiana. Therefore, the FHWA/EPA Sole Source Aquifer Memorandum of Understanding (MOU) is not applicable to this project, a detailed groundwater assessment is not needed, and no impacts are expected.

The Indiana Department of Environmental Management's Wellhead Proximity Determinator website (<http://www.in.gov/idem/cleanwater/pages/wellhead/>) was accessed on January 3, 2021, by Metric Environmental. This project is not located within a Wellhead Protection or Source Water Protection Area. No impacts are expected.

The Indiana Department of Natural Resources Water Well Record Database website (<https://www.in.gov/dnr/water/3595.htm>) was accessed on January 3, 2021, by Metric Environmental. Eleven (11) drinking water wells are mapped within 0.5 mile of the project area. The wells are associated with the residential parcels located to the north, south, east and west of the project area. The nearest well is located approximately 0.12 mile west of the project site. The well will not be affected because of its distance from the project site and there being no excavation deeper than approximately 2-3 ft. Therefore, no impacts are expected. Should it be determined during the right-of-way phase that wells are affected, a cost to cure will likely be included in the appraisal to restore the wells.

Based on a desktop review of the INDOT MS4 website (<https://entapps.indot.in.gov/MS4/>) by Metric Environmental on January 3, 2021, and the RFI report, this project is located within the City of Shelbyville Urban Area Boundary. An early coordination letter was sent on April 8, 2020 to the City of Shelbyville MS4 coordinator. The MS4 coordinator did not respond within the 30-day time frame. All necessary storm water/erosion control measures will be implemented during project construction, as required.

Based on a desktop review, a site visit on September 9, 2019, by Metric Environmental, a review of the 2013 aerial photograph (Appendix B, page B-3) the project is located where there is a public water system. Early coordination letters were sent on April 8, 2020, to the City of Shelbyville Utilities Department. No response was received; however, the project designer has coordinated with the City Utilities Department as part of the project design process.

The City of Shelbyville has drinking water supply and storm water/sewage disposal infrastructure in the geographical area of the project consisting of a 36 inch closed storm sewer system and an 18 inch sanitary sewer system. Several additional manholes have been added along the exiting line where new inlets will be connected. The castings of manholes (whether storm or sanitary) will be adjusted to match the proposed grade. The public supply and disposal system will not be affected because of the limited scope of the proposed improvements.



**Indiana Department of Transportation**

County Shelby Route South Miller Street and McKay Road Des. No. 1702775

Additional coordination with the City of Shelbyville on March 25, 2021, confirmed that there will be no relocation of any primary supply or disposal lines. The road work will not extend any deeper below the ground surface than approximately 2-4 ft. for storm water drainage inlet installation. Therefore, no impacts are expected.

<b>Floodplains</b>	<u>Presence</u>	<u>Impacts</u>	
		<b>Yes</b>	<b>No</b>
Project located within a regulated floodplain	<input type="text"/>	<input type="text"/>	<input type="text"/>
Longitudinal encroachment	<input type="text"/>	<input type="text"/>	<input type="text"/>
Transverse encroachment	<input type="text"/>	<input type="text"/>	<input type="text"/>
Homes located in floodplain within 1000' up/downstream from project	<input type="text"/>	<input type="text"/>	<input type="text"/>

If applicable, indicate the Floodplain Level?

Level 1  Level 2  Level 3  Level 4  Level 5

*Use the IDNR Floodway Information Portal to help determine potential impacts. Include floodplain map in appendix. Discuss impacts according to the classification system. If encroachment on a flood plain will occur, coordinate with the Local Flood Plain Administrator during design to insure consistency with the local flood plain planning.*

The Indiana Department of Natural Resources Indiana Floodway Information Portal website (<http://dnrmmaps.dnr.in.gov/appsphp/fdms/>) was accessed on November 5, 2020, by Metric Environmental. This project is not located in a regulatory floodplain as determined from approved IDNR floodplain maps (Appendix F, page F-9). Therefore, it does not fall within the guidelines for the implementation of 23 CFR 650, 23 CFR 771, and 44 CFR. No impacts are expected.

<b>Farmland</b>	<u>Presence</u>	<u>Impacts</u>	
		<b>Yes</b>	<b>No</b>
Agricultural Lands	<input type="text"/>	<input type="text"/>	<input type="text"/>
Prime Farmland (per NRCS)	<input type="text"/>	<input type="text"/>	<input type="text"/>

Total Points (from Section VII of CPA-106/AD-1006\*) \_\_\_\_\_

*\*If 160 or greater, see CE Manual for guidance.*

*Discuss existing farmland resources in the project area, impacts that will occur to farmland, and mitigation and minimization measures considered.*

Based on a desktop review, a site visit on September 9, 2019, by Metric Environmental, a review of the 2013 aerial photograph of the project area (Appendix B, page B-3), there is no land that meets the definition of farmland under the Farmland Protection Policy Act (FPPA) within or adjacent to the project area. The requirements of the FPPA do not apply to this project; therefore, no impacts are expected. An early coordination letter was sent on July 7, 2020, to the Natural Resources Conservation Service (NRCS). In a letter dated August 20, 2020, the NRCS stated that the project will not cause a conversion of prime farmland (Appendix C, page C-32).

**Indiana Department of Transportation**

County Shelby Route South Miller Street and McKay Road Des. No. 1702775

**SECTION D – CULTURAL RESOURCES**

**Minor Projects PA** **Category(ies) and Type(s)** **INDOT Approval Date(s)** **N/A**  
☐ ☐ ☒

**Full 106 Effect Finding**

No Historic Properties Affected ☒ No Adverse Effect ☐ Adverse Effect ☐

**Eligible and/or Listed Resources Present**

NRHP Building/Site/District(s) ☐ Archaeology ☐ NRHP Bridge(s) ☐

**Documentation Prepared** (mark all that apply)

APE, Eligibility and Effect Determination	<input checked="" type="checkbox"/>
800.11 Documentation	<input checked="" type="checkbox"/>
Historic Properties Report or Short Report	<input checked="" type="checkbox"/>
Archaeological Records Check and Assessment	<input checked="" type="checkbox"/>
Archaeological Phase Ia Survey Report	<input checked="" type="checkbox"/>
Archaeological Phase Ic Survey Report	<input type="checkbox"/>
Other: Geophysical Survey	<input checked="" type="checkbox"/>
Cemetery Development Plan	<input checked="" type="checkbox"/>

**ESD Approval Date(s)**

February 24, 2021
February 24, 2021
July 8, 2020
December 22, 2020
December 22, 2020
October 22, 2020
N/A

**SHPO Approval Date(s)**

March 15, 2021
March 15, 2021
July 20, 2020
January 21, 2021
January 21, 2021
January 21, 2021
N/A

**MOA Signature Dates** (List all signatories)

Memorandum of Agreement (MOA) ☐ ☐

*If the project falls under the MPPA, describe the category(ies) that the project falls under and any approval dates. If the project requires full Section 106, use the headings provided. The completion of the Section 106 process requires that a Legal Notice be published in local newspapers. Please indicate the publication date, name of the paper(s) and the comment period deadline. Include any further Section 106 work which must be completed at a later date, such as mitigation from a MOA or avoidance commitments.*

**Area of Potential Effect:**

Qualified professionals working for Metric and meeting the Secretary of the Interior's Professional Qualifications Standards defined an Area of Potential Effect (APE). The Area of Potential Effects (APE) is "the geographic area or areas within which an undertaking may directly or indirectly cause alterations in the character or use of historic properties, if any such properties exist. The area of potential effects is influenced by the scale and nature of an undertaking and may be different for different kinds of effects caused by the undertaking" [36 CFR § 800.16(d)]. The APE for aboveground resources was drawn sufficiently large to encompass potential impacts including visual, physical, and traffic-related impacts that may result from the undertaking, whichever alternative is selected. Due to the urban nature of the project vicinity, with recent dense commercial and residential land use, and the nature of the proposed work, the APE for this project extends approximately 500 ft. beyond the project end points in each direction and was widened in areas with greater viewshed to the project area (Appendix D, pages D-8 to D-10).

**Coordination with Consulting Parties:**

Section 106 of the National Historic Preservation Act (NHPA) requires federal agencies or their representatives to consider the effects of their undertakings on historic properties. In accordance with 36 CFR 800.2(c) and the INDOT *Cultural Resources Manual*, the following organizations were invited on January 23, 2020, to participate in efforts to identify historic properties potentially affected by this project, assess the project's effects, and seek ways to avoid, minimize, or mitigate any adverse effects on historic properties.

Invited Organization	Accepted/No Response
Indiana Landmarks (Central Regional Office)	Accepted
Shelby County Historical Society	None Received
Joseph Boggs Society of Historic Preservation	None Received
Shelby County Commissioners	None Received
Shelbyville Mayor	None Received
Shelbyville City Council President	None Received
Genealogy and History, Shelby County Public Library	Accepted
Indianapolis Metropolitan Planning Organization	None Received
Eastern Shawnee Tribe of Oklahoma	None Received
Miami Tribe of Oklahoma	Accepted
Peoria Tribe of Indians of Oklahoma	None Received
Pokagon Band of Potawatomi Indians	None Received

The Indiana SHPO, as a designated consulting party, was also emailed a copy of this early coordination letter and sent a paper copy of the documentation for review and comment. On February 5, 2020, SHPO replied that they were not aware of any additional parties who should be invited to participate in the Section 106 consultation (Appendix D, page D-42). An affirmative response to join in consultation was received from the Miami Tribe of Oklahoma on February 20, 2020 (Appendix D, page D-44) and the Genealogy and History, Shelby County Public Library. The letter from the Miami Tribe of Oklahoma offered no objection to the project but stated that "if any human remains or Native American cultural items falling under the Native American Graves Protection and Repatriation Act (NAGPRA) or archaeological evidence is discovered during any phase of this project, the Miami Tribe requests immediate consultation with the entity of jurisdiction for the location of discovery." No other responses were received from the invited tribes.

#### **Archaeology:**

Pursuant to 36 CFR § 800.4(b), a Qualified Professional Archaeologist with Metric Environmental conducted an archaeological records check on September 9, 2020, which involved review of the State Historical Architectural and Archaeological Research Database (SHAARD), site maps on file with the IDNR-Division of Historic Preservation and Archaeology, cultural resource management reports, cemetery records, and historical data. The literature review of the SHAARD database indicated that no previously recorded archaeological sites are located within one mile of the project site; however, the unmarked Shanks Cemetery (Indiana Cemetery Record No. CR-73-8) was reported as possibly being in or adjacent to the archaeological APE, defined as the project footprint. The records for this resource describe its location as being approximately 200 yards southeast of the project intersection, but no visible signs of the cemetery currently exist.

In October 2020, INDOT-CRO conducted a geophysical survey within the southeast quadrant of the project intersection. The geophysical survey did not identify any evidence of the cemetery and concluded its likely located further to the southeast of the project footprint. No additional work was recommended (Appendix D, pages D-25 to D-28). Subsequently, a Phase Ia Archaeological Survey (Snell 12/22/20) was conducted on November 6, 2020 and an Archaeology Short Report (ASR) was prepared documenting the findings. The ASR recommended the project be allowed to proceed with no additional work. Excerpts of the ASR are provided in Appendix D, pages D-29 to D-32. On December 22, 2020, INDOT-Cultural Resources Office (INDOT-CRO) approved the ASR and the geophysical survey and released the reports for submittal to the SHPO and consulting parties for review and comment (Appendix D, page D-57). The SHPO concurred with the results of the ASR and geophysical survey in a letter dated January 21, 2021 and recommended that if the project footprint should change within the southeast quadrant, additional survey may be required (Appendix D, pages D-59 to D-60). This has been included a firm environmental commitment for the project. No other replies were received. A Cemetery Development Plan was developed by Metric Environmental and provided to the City of Shelbyville as a planning tool should evidence of the cemetery be discovered during construction.

#### **Historic Properties:**

Pursuant to 36 CFR § 800.4(b), personnel with Metric Environmental, who meet the Secretary of the Interior's Professional Qualification Standards as per 36 CFR Part 61, reviewed the National Register of Historic Places (NRHP), Indiana Register of Historic Sites and Structures Inventory (IHSSI), the State Historical Architectural and Archaeological Research Database (SHAARD), Indiana Historic Buildings, Bridges, and Cemeteries Map (IHBBCM), and the Shelby County Interim Report for previously identified properties. In conducting research, historians examined primary and secondary resources. Documentary research for the project included a review of county histories, aerial photographs, and online resources.

On June 18, 2020, a Historic Properties Short Report (HPSR) (Hudziak 6/18/2020) was prepared and no properties located within the APE were identified as eligible for or listed on the National Register of Historic Properties. Excerpts of the HPSR are provided in Appendix D, pages D-22 to D-24. On July 8, 2020 INDOT-CRO approved the report and the HPSR was uploaded to INSCOPE. Metric Environmental mailed the HPSR to SHPO for review on July 8, 2020. On July 9, 2020, INDOT-CRO notified the Miami Tribe of Oklahoma of the approval of the HPSR (Appendix D, page D-49). The SHPO concurred with the HPSR in a letter dated July 20, 2020 (Appendix D, pages D-52 to D-53). No other comments were received.

#### **Documentation, Findings:**

On February 24, 2021, the INDOT-CRO, on behalf of the FHWA approved the APE and issued a "No Historic Properties Affected" finding for this project (Appendix D, Page D-1 to D-7). INDOT-CRO approved the effect finding and the document was uploaded to INSCOPE on February 25, 2021 (Appendix D, page D-63). The effect documentation was provided to the SHPO for a 30-day review and comment period. On March 15, 2021, the Indiana SHPO responded and concurred with the "No Historic Properties Affected" finding (Appendix D, pages D-61 to D-62). No other comments were received.

#### **Public Involvement:**

In accordance with 36 CFR 800.2(d), 800.3(e), and 800.6(a)(4), the views of the public were sought regarding the effect of the proposed project. A legal notice was published in the *Shelbyville News* on March 12, 2021, with a 30-day comment period.

**Indiana Department of Transportation**

County Shelby Route South Miller Street and McKay Road Des. No. 1702775

The 30-day deadline for comments was April 11, 2021. No comments were received by the 30-day deadline. A copy of the legal notice and the publisher's affidavit are provided in Appendix D, page D-64. The Section 106 process is complete and the FHWA responsibilities under Section 106 have been fulfilled.

**SECTION E – SECTION 4(f) RESOURCES/ SECTION 6(f) RESOURCES**

	<u>Presence</u>	<u>Use</u>	
		Yes	No
<b>Parks and Other Recreational Land</b>			
Publicly owned park			
Publicly owned recreation area			
Other (school, state/national forest, bikeway, etc.)	<b>X</b>		<b>X</b>
<b>Wildlife and Waterfowl Refuges</b>			
National Wildlife Refuge			
National Natural Landmark			
State Wildlife Area			
State Nature Preserve			
<b>Historic Properties</b>			
Site eligible and/or listed on the NRHP			

**Evaluations Prepared**

Programmatic Section 4(f)	
“De minimis” Impact	
Individual Section 4(f)	
Any exception included in 23 CFR 774.13	

*Discuss Programmatic Section 4(f) and “de minimis” Section 4(f) impacts in the discussion below. Individual Section 4(f) documentation must be included in the appendix and summarized below. Discuss proposed alternatives that satisfy the requirements of Section 4(f). FHWA has identified various exceptions to the requirement for Section 4(f) approval. Refer to 23 CFR § 774.13 - Exceptions.*

Section 4(f) of the U.S. Department of Transportation Act of 1966 prohibits the use of certain public and historic lands for federally funded transportation facilities unless there is no feasible and prudent alternative. The law applies to significant publicly owned parks, recreation areas, wildlife / waterfowl refuges, and NRHP eligible or listed historic properties regardless of ownership. Lands subject to this law are considered Section 4(f) resources.

Based on a desktop review, the 2013 aerial photograph of the project area (Appendix B, page B-3), and the RFI report (Appendix E, page E-2) there are seven (7) potential Section 4(f) resources located within the 0.5 mile search radius. A site visit conducted on September 9, 2019, by Metric Environmental confirmed that the Section 4(f) resources consisted of three (3) recreational facilities and four (4) trail segments. The nearest trail illustrated on the RFI map (Appendix E, page E-8) is located along McKay Road. The trail is planned to be constructed along the north side of McKay Road but does not currently exist. The early coordination letter was sent to the City of Shelbyville Parks and Recreation and no response was received; however, additional coordination was conducted in June 2021 and the City of Shelbyville Parks and Recreation responded that the proposed project would enhance the future trail that is planned to run from State Road 44 through the project intersection and connect to the Meridian Park Family Aquatic Center. Furthermore, the representative of the Shelbyville Parks and Recreation stated the project will not inhibit the ability of the Section 4(f) resource from being built and able to function as a Section 4(f) resource, therefore, no use is expected.

The nearest recreational facility is mapped approximately 0.28 mile to the northwest of the project site and consists of the Shelbyville High School baseball, tennis and track and field facilities. These recreational resources associated with the school facilities are not located adjacent to or within the project area and are not open for public use. The Shelbyville High School does not consider the front lawn area of the school grounds, which is located adjacent to the northwest quadrant of the project, a public recreational resource that would be afforded protection under Section 4(f). There is a sign for the high school located in this area that will require removal and reinstallation. Discussions with the high school are ongoing concerning the replacement and future location of the sign. There are no additional Section 4(f) resources located within or adjacent to the project area. Therefore, no use is expected.

**Indiana Department of Transportation**

County Shelby Route South Miller Street and McKay Road Des. No. 1702775

**Section 6(f) Involvement**

**Presence**

**Use**

**Yes**

**No**

**Section 6(f) Property**

☐
☐
☐

Discuss Section 6(f) resources present or not present. Discuss if any conversion would occur as a result of this project. If conversion will occur, discuss the conversion approval.

The U.S. Land and Water Conservation Fund Act of 1965 established the Land and Water Conservation Fund (LWCF), which was created to preserve, develop, and assure accessibility to outdoor recreation resources. Section 6(f) of this Act prohibits conversion of lands purchased with LWCF monies to a non-recreation use. A review of Section 6(f) properties provided by INDOT at [https://www.in.gov/indot/files/IN\\_LWCF\\_sites\\_by\\_county.xlsx](https://www.in.gov/indot/files/IN_LWCF_sites_by_county.xlsx) revealed one (1) property in Shelby County that has received LWCF funding (Appendix J, page J-1). The property is not located within or adjacent to the project area. No impacts are expected.

**SECTION F – Air Quality**

**STIP/TIP and Conformity Status of the Project**

**Yes**

**No**

Is the project in the most current STIP/TIP?

☒
☐

Is the project located in an MPO Area?

☐
☒

Is the project in an air quality non-attainment or maintenance area?

☒
☐

If Yes, then:

Is the project in the most current MPO TIP?

☐
☒

Is the project exempt from conformity?

☒
☐

If No, then:

Is the project in the Transportation Plan (TP)?

☐
☐

Is a hot spot analysis required (CO/PM)?

☐
☐

Location in STIP:

Page 473 of the 2020-2024 STIP Updated Project List

Name of MPO (if applicable):

\_\_\_\_\_

Location in TIP (if applicable):

\_\_\_\_\_

Level of MSAT Analysis required?

Level 1a ☒

Level 1b ☐

Level 2 ☐

Level 3 ☐

Level 4 ☐

Level 5 ☐

Describe if the project is listed in the STIP and if it is in a TIP. Describe the attainment status of the county(ies) where the project is located. Indicate whether the project is exempt from a conformity determination. If the project is not exempt, include information about the TP and TIP. Describe if a hot spot analysis is required and the MSAT Level.

The project is included in the 2020-2024 Statewide Transportation Improvement Plan (STIP) (Appendix H, page H-1). The project area is not located within a MPO.

This project is located in Shelby County, which is designated as a maintenance area for 1997 8-hour Ozone according to the United States Environmental Protection Agency (EPA) website <https://www.epa.gov/green-book/green-book-8-hour-ozone-1997-area-information-naaqs-revoked>. This project has been identified as being exempt from air quality analysis in accordance with 40 CFR Part 93.126 and this project is not a project of air quality concern (40 CFR Part 93.123). Therefore, the project will have no significant impact on air quality.

This project is of a type qualifying as a categorical exclusion (Group 1) under 23 CFR 771.117(c), or exempt under the Clean Air Act conformity rule under 40 CFR 93.126, and as such, a Mobile Source Air Toxics analysis is not required.

**SECTION G - NOISE**

**Noise**

**Yes**

**No**

Is a noise analysis required in accordance with FHWA regulations and INDOT's traffic noise policy?

☐
☒

Date Noise Analysis was approved/technically sufficient by INDOT ESD:

\_\_\_\_\_

**Indiana Department of Transportation**

County Shelby Route South Miller Street and McKay Road Des. No. 1702775

*Describe if the project is a Type I or Type III project. If it is a Type I project, describe the studies completed to date and if noise impacts were identified. If noise impacts were identified, describe if abatement is feasible and reasonable and include a statement of likelihood.*

This project is a Type III project. In accordance with 23 CFR 772 and the current *Indiana Department of Transportation Traffic Noise Analysis Procedure*, this action does not require a formal noise analysis.

**SECTION H – COMMUNITY IMPACTS**

**Regional, Community & Neighborhood Factors**

Will the proposed action comply with the local/regional development patterns for the area?  
 Will the proposed action result in substantial impacts to community cohesion?  
 Will the proposed action result in substantial impacts to local tax base or property values?  
 Will construction activities impact community events (festivals, fairs, etc.)?  
 Does the community have an approved transition plan?  
 If No, are steps being made to advance the community's transition plan?  
 Does the project comply with the transition plan? (explain in the discussion below)

Yes	No
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>

*Discuss how the project complies with the area's local/regional development patterns; whether the project will impact community cohesion; and impact community events. Discuss how the project conforms with the ADA Transition Plan.*

The U.S. Department of Housing and Urban Development (HUD) was consulted as part of the early coordination process regarding possible regional, community or neighborhood factors associated with this project. No response was received. On December 21, 2020, Metric conducted an on-line review of the Indiana Festivals website (<http://www.indianafestivals.org>). There are no events identified within or near the project area that would be potentially impacted during construction of the project. Since the project will be constructed after classes at Shelbyville High School end for summer break, there will be no impacts to any scheduled school events.

The Americans with Disabilities Act (ADA) requires a transition plan by local and state governments. Such a plan includes how the government will remove barriers to accessibility over time for persons with disabilities, such as installing curb ramps at intersections, making a web site accessible for persons with low vision, ensuring public meetings are fully accessible to persons with disabilities and other related issues. The City of Shelbyville has an approved ADA transition plan, and the proposed project design does include ADA compliant sidewalks, crosswalk ramps and driveway ramps.

This project will not change the general development patterns, population density, or residential or commercial growth rate of the project area. Furthermore, there will be no permanent impacts to community cohesion, local mobility, access, pedestrian or motorist safety or emergency services as a result of the project. The project will not have any adverse impacts on the local tax base or property values.

**Public Facilities and Services**

*Discuss what public facilities and services are present in the project area and impacts (such as MOT) that will occur to them. Include how the impacts have been minimized and what coordination has occurred. Some examples of public facilities and services include health facilities, educational facilities, public and private utilities, emergency services, religious institutions, airports, transportation or public pedestrian and bicycle facilities.*

Based on a desktop review, a review of the 2013 aerial photograph of the project area (Appendix B, page B-3), and the RFI report (Appendix E, page E-8), there are public facilities located within the 0.5 mile search radius. Although unmapped, there is one (1) religious facility, and seven (7) schools mapped within the 0.5 mile search radius. The site visit conducted on September 9, 2019, by Metric Environmental confirmed that the Shelbyville High School and Middle School facilities are located in the northwest quadrant of the project intersection. Access to the school will be maintained during construction utilizing the secondary entrances located north of the intersection on South Miller Street and west on McKay Road.

The Heritage House senior living facility is located in the southwest quadrant of the project intersection. The Heritage House senior living facility has two entrances on South Miller Street, in addition to an entrance from McKay Road. The northern most entrance on South Miller Street and the McKay Road entrance is located within the construction limits; however, the southern entrance is not. Access to all commercial and residential properties will be maintained during construction. There are no emergency services, religious institutions, airports, transportation or bicycle facilities that will be impacted. Construction of the project will result in temporary traffic restrictions and possible delays but, these inconveniences will cease upon project completion. There will be no substantial impacts on any public facilities.

**Indiana Department of Transportation**

County Shelby Route South Miller Street and McKay Road Des. No. 1702775

Approximately six overhead electric poles will require relocation to construct the project. There is an overhead electric transmission pole located in the southeast quadrant of the project that will be avoided as the design has shifted the roundabout to the north and west. In addition, there are drinking water supply and storm water/sewage disposal infrastructure in the geographical area of the project. The drinking water supply and sewage disposal system will not be affected because of the limited scope of the proposed improvements. All the existing utilities within the project area have been notified and utility verification and conflict analysis has been conducted.

It is the responsibility of the project sponsor to notify school corporations, the Heritage House senior living facility and emergency services at least two weeks prior to any construction that would block or limit access.

**Environmental Justice (EJ) (Presidential EO 12898)**

During the development of the project were EJ issues identified?

Does the project require an EJ analysis?

If YES, then:

Are any EJ populations located within the project area?

Will the project result in adversely high and disproportionate impacts to EJ populations?

**Yes**

<b>X</b>

**No**

<b>X</b>

<b>X</b>

<b>X</b>

*Indicate if EJ issues were identified during project development. If an EJ analysis was not required, discuss why. If an EJ analysis was required, describe how the EJ population was identified. Include if the project has a disproportionately high and adverse effect on EJ populations and explain your reasoning. If yes, describe actions to avoid, minimize and mitigate these effects.*

Under FHWA Order 6640.23A, FHWA and the project sponsor, as a recipient of funding from FHWA, are responsible to ensure that their programs, policies, and activities do not have a disproportionately high and adverse effect on minority or low-income populations. Per the current INDOT *Categorical Exclusion Manual*, an Environmental Justice (EJ) Analysis is required for any project that has two or more relocations or 0.5 acre of additional permanent right-of-way. The project will require approximately 1.79 acres of new, additional permanent right-of-way, but there will be no relocations. Therefore, an EJ Analysis is required.

Potential EJ impacts are detected by locating minority and low-income populations relative to a reference population to determine if populations of EJ concern exists and whether there could be disproportionately high and adverse impacts to them. The reference population may be a county, city or town and is called the community of comparison (COC). In this project, the COC is Shelby County. The community that overlaps the project area is called the affected community (AC). In this project, the AC is Census Tract 7107. An AC has a population of concern for EJ if the population is more than 50% minority or low-income or if the low-income or minority population is 125% of the COC. Data from the 2019 American Community Survey (ACS) 5 year estimates was obtained from the US Census Bureau on March 10, 2021, by Metric Environmental. The data collected for minority and low-income populations within the AC and COC are summarized in the below table.



**Indiana Department of Transportation**

County Shelby

Route South Miller Street and McKay Road

Des. No. 1702775

US Census 2019 American Community Survey 5 year Estimates	Shelby County, Indiana	Census Tract 7107 Shelby County, Indiana
<b>LOW-INCOME</b>		
Population for whom poverty status is determined: Total	43,556	6,108
Population for whom poverty status is determined: Income in 2019 below poverty level	4,903	400
<b>Percent Low-Income</b> (Income in 2019 below poverty level) (Total population)	11.26%	6.55%
<b>125 Percent of COC</b> (125 x COC Percent Low-Income)	14.07%	<b>AC &lt; 125% COC</b>
<b>Low-Income EJ Impact</b>		<b>No</b>
<b>MINORITY</b>		
Total Population: Total	44,438	6,400
Not Hispanic or Latino	42,565	6,201
White alone	41,006	6,006
Black or African American alone	519	108
American Indian and Alaska Native alone	21	0
Asian alone	250	71
Native Hawaiian and Other Pacific Islander alone	0	0
Some other race alone	73	16
Two or more races	696	0
Hispanic or Latino	1,873	199
<b>Number Non-white/minority</b>	3,432	394
<b>Percent Non-white/Minority</b> (Total population - white alone) Total population	7.72%	6.16%
<b>125 Percent of COC</b> (125 x COC Percent Non-white/Minority)	9.65%	<b>AC &lt; 125% COC</b>
<b>Minority EJ Impact</b>		<b>No</b>

The AC, Census Tract 7107 has a percent minority of 6.16% which is above 50% but below the 125% COC threshold. Therefore, Census Tract 7107 is not a minority population of EJ concern. Census Tract 7107 has a 6.55% low-income population which is above 50% but below the 125% COC threshold. Therefore, the AC is not considered a low-income population of EJ concern.

A detour will be required for a minimal time during construction. The detour will utilize Miller Avenue, CR 250 South, Colescott Road and Harrison Street (SR 9). Construction of the project will contribute to temporary traffic restrictions while the intersection is closed but, these inconveniences will cease upon project completion. Access to all residential and commercial properties will be maintained for the duration of construction. There will be no disproportionate or adverse impacts to any EJ populations as a result of the project. The U.S. Census Bureau data is provided in Appendix I, pages I-1 to I-3.

**Relocation of People, Businesses or Farms**

Will the proposed action result in the relocation of people, businesses or farms?

Is a BIS or CSRS required?

**Yes**


**No**

<b>X</b>
<b>X</b>

Number of relocations:      Residences: 0      Businesses: 0      Farms: 0      Other: 0

*Discuss any relocations that will occur due to the project. If a BIS or CSRS is required, discuss the results in the discussion below.*

No relocations of people, businesses or farms will be necessary to complete the proposed project.



County ShelbyRoute South Miller Street and McKay RoadDes. No. 1702775**SECTION I – HAZARDOUS MATERIALS & REGULATED SUBSTANCES****Documentation****Hazardous Materials & Regulated Substances** (Mark all that apply)

Red Flag Investigation (RFI)

Phase I Environmental Site Assessment (Phase I ESA)

Phase II Environmental Site Assessment (Phase II ESA)

Design/Specifications for Remediation required?

**X**

Date RFI concurrence by INDOT SAM (if applicable):

**June 25, 2020**

*Include a summary of the potential hazardous material concerns found during review. Discuss in depth sites found within, directly adjacent to, or ones that could impact the project area. Refer to current INDOT SAM guidance. If additional documentation (special provisions, pay quantities, etc.) will be needed, include in discussion. Include applicable commitments.*

Based on a review of GIS and available public records, and a RFI completed by Metric Environmental on May 15, 2020, and concurred by INDOT-SAM Unit on June 25, 2020 (Appendix E, page E-5), there is one (1) State Cleanup site, one (1) Brownfield site, and one (1) NPDES site is located within 0.5 mile of the project area. None of the hazmat sites identified will impact the project. Further investigation for hazardous material concerns is not required at this time.

**Part IV – Permits and Commitments****PERMITS CHECKLIST****Permits** (mark all that apply)**Army Corps of Engineers (404/Section10 Permit)**

Nationwide Permit (NWP)

Regional General Permit (RGP)

Individual Permit (IP)

Other

**Likely Required****IN Department of Environmental Management (401/Rule 5)**

Nationwide Permit (NWP)

Regional General Permit (RGP)

Individual Permit (IP)

Isolated Wetlands

Rule 5

Other

**X****IN Department of Natural Resources**

Construction in a Floodway

Navigable Waterway Permit

Other

**Mitigation Required****US Coast Guard Section 9 Bridge Permit****Others (Please discuss in the discussion below)**

*List the permits likely required for the project and summarize why the permits are needed, including permits designated as "Other."*

The project will require a Rule 5 permit due to the disturbance of more than 1.0 acre of land. Applicable recommendations provided by the resource agencies are included in the *Environmental Commitments* section of this document. If permits are found to be necessary, the conditions of the permit will be requirements of the project and will supersede these recommendations.

It is the responsibility of the project sponsor to identify and obtain all required permits.

**Indiana Department of Transportation**

County Shelby Route South Miller Street and McKay Road Des. No. 1702775

**ENVIRONMENTAL COMMITMENTS**

*List all commitments and include the name of agency/organization requesting/requiring the commitment(s). Listed commitments should be numbered.*

Remarks:

**Firm:**

1. If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately. (INDOT ESD)
2. It is the responsibility of the project sponsor to notify school corporations, the Heritage House senior living facility and emergency services at least two weeks prior to any construction that would block or limit access. (INDOT ESD)
3. Any work in a wetland area within INDOT's right of way or in borrow/waste areas is prohibited unless specifically allowed in the US Army Corps of Engineers or IDEM permit. (INDOT ESD)
4. General AMM 1: Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs. (USFWS)
5. Lighting AMM 1: Direct temporary lighting away from suitable habitat during the active season. (USFWS)
6. Lighting AMM 2: When installing new or replacing existing permanent lights, use downward-facing, full cut-off lens lights (with same intensity or less for replacement lighting); or for those transportation agencies using the BUG system developed by the Illuminating Engineering Society, be as close to 0 for all three ratings with a priority of "uplight" of 0 and "backlight" as low as practicable. (USFWS)
7. Tree Removal AMM 1: Modify all phases/aspects of the project (e.g., temporary work areas, alignments) to the extent practicable to avoid tree removal in excess of what is required to implement the project safely. (USFWS)
8. Tree Removal AMM 2: Apply time of year restrictions for tree removal (No tree removal from April 1 to September 30) when bats are not likely to be present, or limit tree removal to 10 or fewer trees per project at any time of year within 100 feet of existing road/ rail surface and outside of documented roosting/foraging habitat or travel corridors; visual emergence survey must be conducted with no bats observed. (USFWS, and IDNR-DFW)
9. Tree Removal AMM 3: Ensure tree removal is limited to that specified in project plans. Install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits. Ensure that contractors understand clearing limits and how they are marked in the field. (USFWS)
10. Tree Removal AMM 4: Do not cut down documented Indiana bat or NLEB roosts (that are still suitable for roosting) or trees within 0.25 mile of roosts or documented foraging habitat at any time of year. (USFWS)
11. If the project footprint should change within the southeast quadrant, an additional archaeological survey may be required due to the presence of the unmarked Shanks cemetery. (SHPO)
12. There are eight decorative brick columns located near the perimeter of the residence in the northeast quadrant of the intersection. These will not be disturbed and will remain in place. (INDOT ESD)

**For Further Consideration:**

13. Plant five trees, at least 2 inches in diameter-at-breast height (dbh), for each tree which is removed that is 10 inches dbh or greater (IDNR-DFW)

## APPENDICES

### APPENDIX A: INDOT Supporting Documentation

- CE Threshold Chart A-1

### APPENDIX B: Graphics

- Project Location Map B-1
- USGS Topographic Map B-2
- Aerial Photograph B-3
- Ground Level Photographs B-4
- Design Plans B-8

### APPENDIX C: Early Coordination

- Sample Early Coordination Letter C-1
- Early Coordination Recipients List C-3
- IDNR-DFW Response C-4
- USFWS Concurrence Verification C-5
- USFWS Official Species List C-18
- INDOT Bat Database Response C-24
- Indiana Geological Survey Response C-25
- IDEM Proposed Roadway Construction Projects letter C-27
- NRCS Response C-32

### APPENDIX D: Section 106 of the National Historic Preservation Act

- No Historic Properties Effected Finding D-1
- Area of Potential Effects Map D-9
- Historic Properties Report Excerpts D-22
- Geophysical Report Excerpts D-25
- Archaeological Short Report Excerpts D-29
- SHPO Response to Early Coordination D-42
- SHPO Approval of HPR D-52
- SHPO Approval of Archaeological Report D-59
- SHPO Approval of Effect Determination Finding D-61
- Publishers Claim D-64

### APPENDIX E: Red Flag and Hazardous Materials

- Red Flag Investigation E-1
- Red Flag Maps E-7
- Endangered Threatened Rare Species List E-12

### APPENDIX F: Water Resources

- Waters Determination Report F-1
- Exhibit 3 - NWI Wetland Inventory Map F-9
- Exhibit 5 - Waters Delineation Map F-10
- Site Photographs F-12

### APPENDIX G: Public Involvement

- Example Notice of Survey Letter G-1

### APPENDIX H: Air Quality

- FY 2020-2024 INDOT STIP Project List H-1

**APPENDIX I: Environmental Justice**

- Census Bureau Maps I-1
- Census Bureau Tables I-2
- EJ Calculations Table I-3

**APPENDIX J: Additional Studies**

- LWCF Project List, December 2019 J-1
- Traffic Study Excerpts J-2

# **APPENDIX A**

## **INDOT Supporting Documentation**

## Categorical Exclusion Level Thresholds

	PCE	Level 1	Level 2	Level 3	Level 4 <sup>1</sup>
<b>Section 106</b>	Falls within guidelines of Minor Projects PA	“No Historic Properties Affected”	“No Adverse Effect”	-	“Adverse Effect” Or Historic Bridge involvement <sup>2</sup>
<b>Stream Impacts<sup>3</sup></b>	No construction in waterways or water bodies	< 300 linear feet of stream impacts	≥ 300 linear feet of stream impacts	-	USACE Individual 404 Permit <sup>4</sup>
<b>Wetland Impacts<sup>3</sup></b>	No adverse impacts to wetlands	< 0.1 acre	-	< 1.0 acre	≥ 1.0 acre
<b>Right-of-way<sup>5</sup></b>	Property acquisition for preservation only or none	< 0.5 acre	≥ 0.5 acre	-	-
<b>Relocations</b>	None	-	-	< 5	≥ 5
<b>Threatened/Endangered Species (Species Specific Programmatic for Indiana bat &amp; northern long eared bat)*</b>	“No Effect”, “Not likely to Adversely Affect” (With select AMMs <sup>6</sup> )	“Not likely to Adversely Affect” (With any AMMs or commitments)	-	“Likely to Adversely Affect”	Project does not fall under Species Specific Programmatic <sup>7</sup>
<b>Threatened/Endangered Species (Any other species)*</b>	Falls within guidelines of USFWS 2013 Interim Policy or “No Effect”	“Not likely to Adversely Affect”	-	-	“Likely to Adversely Affect”
<b>Environmental Justice</b>	No disproportionately high and adverse impacts	-	-	-	Potential <sup>8</sup>
<b>Sole Source Aquifer</b>	No Detailed Groundwater Assessment	-	-	-	Detailed Groundwater Assessment
<b>Floodplain</b>	No Substantial Impacts	-	-	-	Substantial Impacts
<b>Section 4(f) Impacts</b>	None	-	-	-	Any <sup>9</sup>
<b>Section 6(f) Impacts</b>	None	-	-	-	Any
<b>Permanent Traffic Alteration</b>	None	-	-	-	Any
<b>Noise Analysis Required</b>	No	-	-	-	Yes
<b>Air Quality Analysis Required</b>	No	-	-	-	Yes <sup>10</sup>
<b>Approval Level</b>  <ul style="list-style-type: none"> <li>• District Env. (DE)</li> <li>• Env. Serv. Div. (ESD)</li> <li>• FHWA</li> </ul>	Concurrence by DE or ESD	DE or ESD	DE or ESD	DE and/or ESD	DE and/or ESD; and FHWA

<sup>1</sup> Coordinate with INDOT Environmental Services Division. INDOT will then coordinate with the appropriate FHWA Environmental Specialist.

<sup>2</sup> Any involvement with a bridge processed under the Historic Bridge Programmatic Agreement.

<sup>3</sup> Total permanent impacts to streams (linear feet) and wetlands (acres).

<sup>4</sup> US Army Corps of Engineers Individual 404 Permit

<sup>5</sup> Total permanent and temporary right-of-way. This does not include reacquisition of existing apparent right-of-way.

<sup>6</sup> Avoidance and Mitigation Measures (AMMs) determined by the IPAC determination key to be required that are not tree AMMs, bridge AMMs, or structure AMMs.

<sup>7</sup> Projects that do not fall under a Species Specific Programmatic and results in a “Likely to Adversely Affect”. Other findings can be processed as a lower level CE.

<sup>8</sup> Potential for causing a disproportionately high and adverse impact.

<sup>9</sup> Section 4(f) use resulting in an Individual, Programmatic, or *de minimis* evaluation. The only exception is a *de minimis* evaluation for historic properties (Effective January 2, 2020). If a historic property *de minimis* and no other use, mark the *None* column.

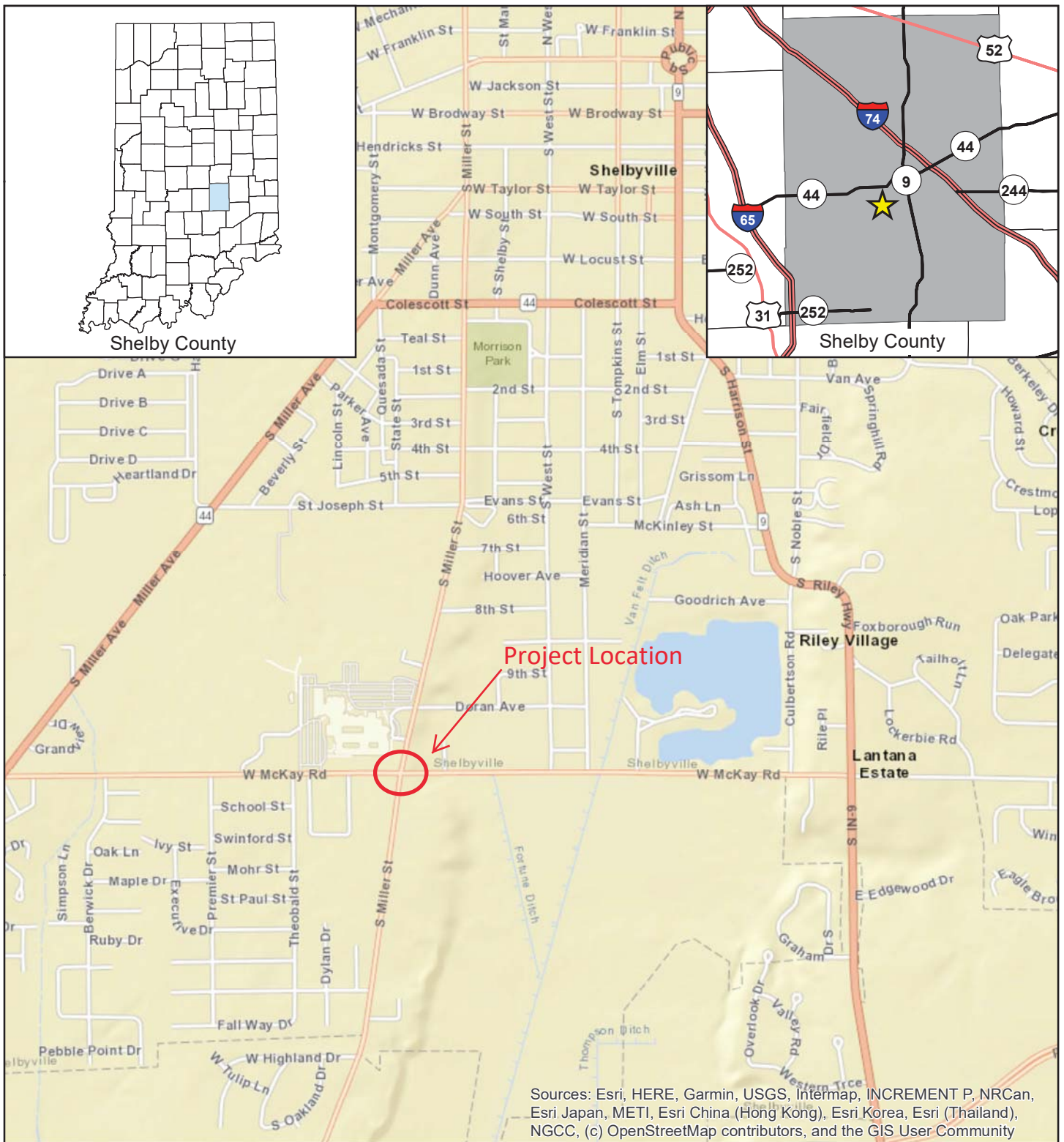
<sup>10</sup> Hot Spot Analysis and/or MSAT Quantitative Emission Analysis.

\* Includes the threatened/endangered species critical habitat

Note: Substantial public or agency controversy may require a higher-level NEPA document.

# **APPENDIX B**

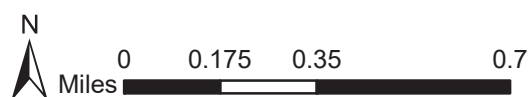
## **Graphics**



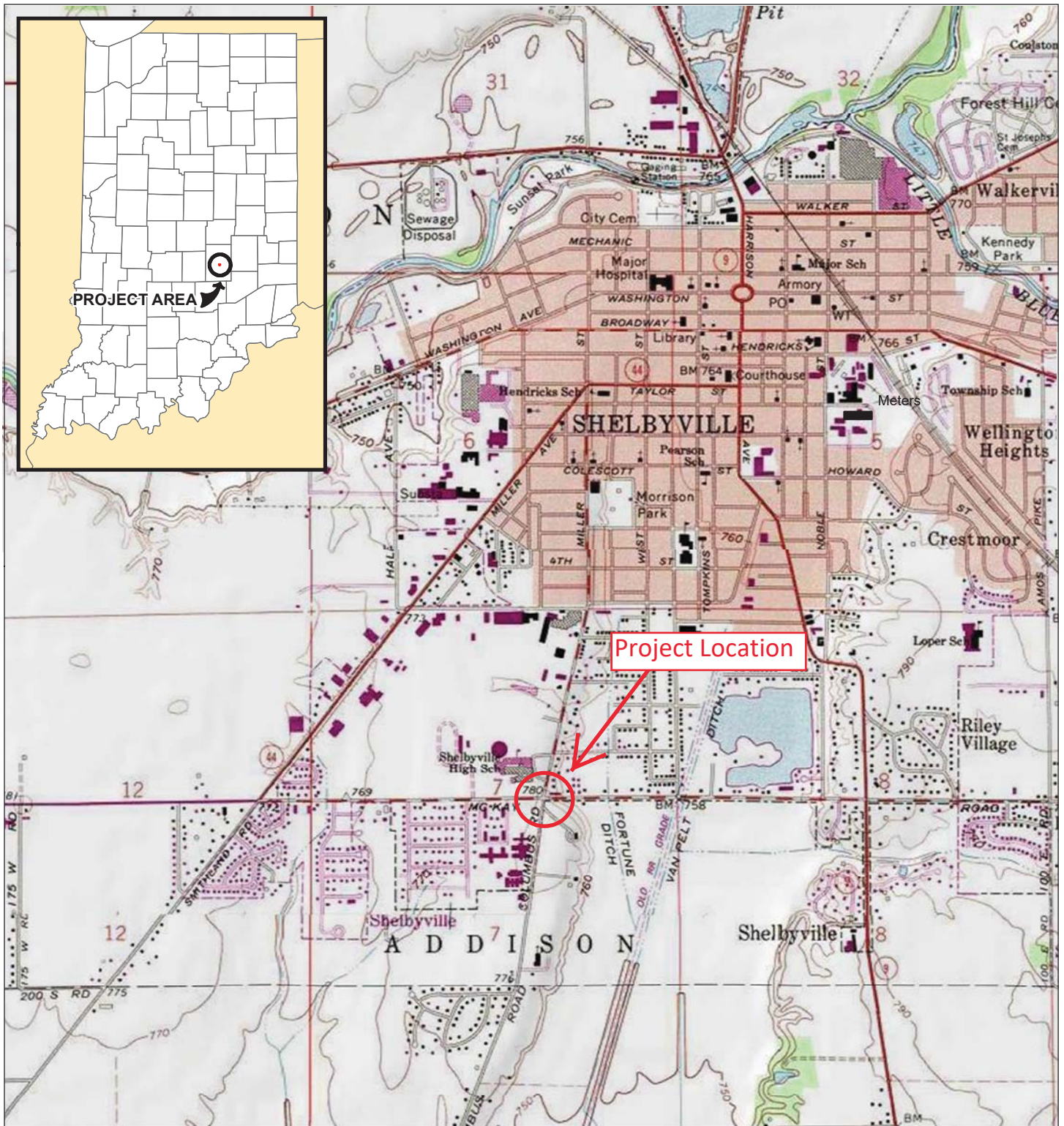
## Project Location Map

Intersection Improvement Project  
Des. No.1702775  
South Miller Street and McKay Road  
Shelby County, Indiana

All locations approximate  
2018 Basemap  
Latitude: 39.504700 Longitude: -85.786973







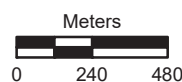
# USGS Topographical Map

Intersection Improvement Project  
Des. No.1702775  
South Miller Street and McKay Road  
Shelby County, Indiana

All Locations Approximate  
1994 and 1988 Basemaps



1 cm = 240 m







## 2013 Aerial Photograph

Intersection Improvement Project  
Des. No.1702775  
South Miller Street and McKay Road  
Shelby County, Indiana

All Locations Approximate  
2013 Basemap  
Service Layer Credits: Source: Esri, DigitalGlobe, GeoEye,  
Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AeroGRID,  
IGN, and the GIS User Community

Meters  
0 20 40  
1 cm = 20 m







Photo 1. View Looking Northwest from the South Leg of Miller Street



Photo 2. View Looking Southwest from the Northeast Quadrant of the Intersection





Photo 3. View Looking Northeast from the Southwest Quadrant of the Intersection



Photo 4. View Looking East along the South Side of McKay Road toward the Project Intersection





Photo 5. View Looking West along the North Side of McKay Road toward the Project Intersection



Photo 6. View Looking East along the South Side of McKay Road from the Project Intersection

**SITE PHOTOGRAPHS 9/9/2019**  
Intersection Improvement Project  
Des. No. 1702775  
Miller Street at McKay Road  
City of Shelbyville, Shelby County, Indiana





Photo 7. View Looking East Along the North Side of McKay Road toward the Project Intersection



Photo 8. View Looking South Along the West Side of Miller Street from Entrance Drive to the High School

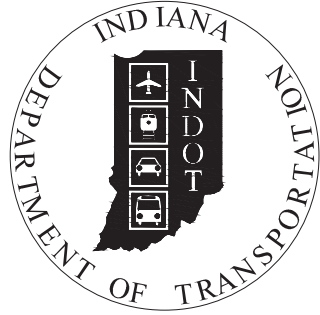
**SITE PHOTOGRAPHS 9/9/2019**  
 Intersection Improvement Project  
 Des. No. 1702775  
 Miller Street at McKay Road  
 City of Shelbyville, Shelby County, Indiana





PROJECT	DESIGNATION
1702775	1702775
CONTRACT	BRIDGE FILE
R-41302	N/A

INDIANA DEPARTMENT  
OF TRANSPORTATION



ROAD PLANS

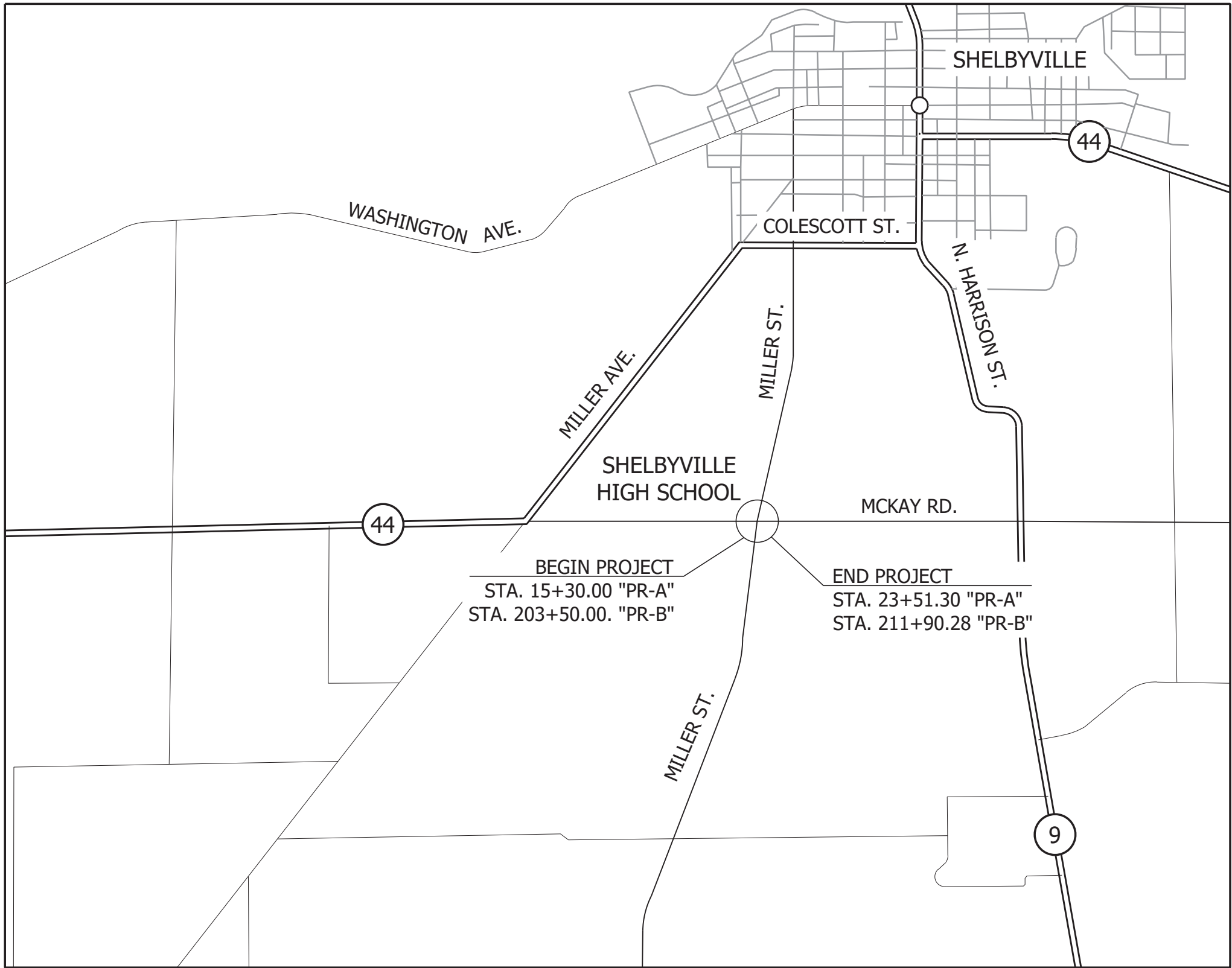
SOUTH MILLER STREET AND MCKAY ROAD ROUNDABOUT

PROJECT NO.

1702775 P.E.  
1702775 R/W  
1702775 CONST.

ROAD RECONSTRUCTION AT THE INTERSECTION OF SOUTH MILLER STREET AND MCKAY ROAD APPROXIMATELY 0.87 MILES WEST OF S.R. 9 IN SECTION 7, T-12-N, R-7-E, IN ADDISON TOWNSHIP, SHELBY COUNTY, INDIANA

R-7-E



R-7-E

STAGE 2 PLANS  
(60%)

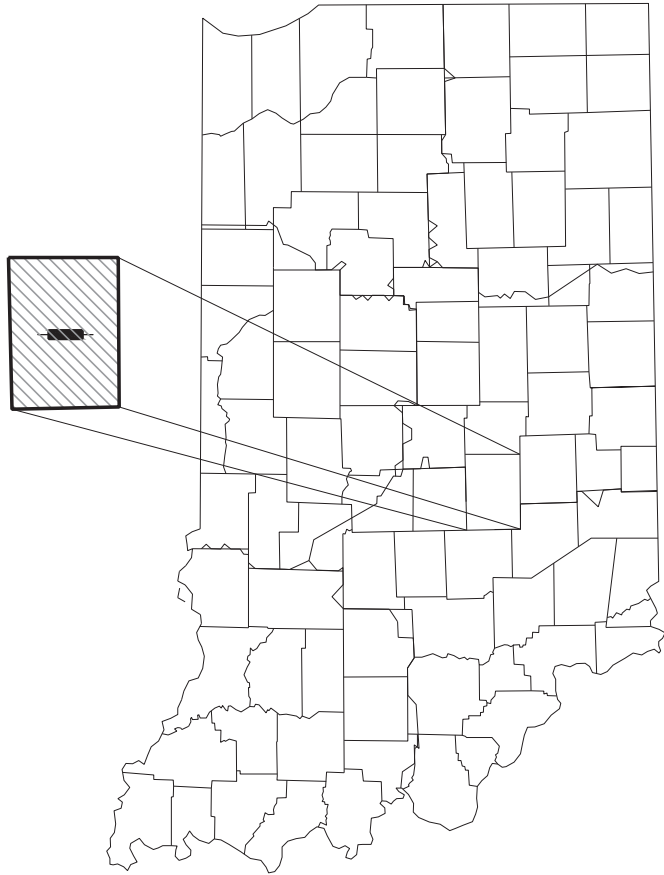
THOMAS D. DEBAUN MAYOR

MATTHEW J. HOUSE ERC



SCALE:  
1" = 2000'

TRAFFIC DATA	S.MILLER STREET	MCKAY ROAD
A.A.D.T. (2022)	5,267 V.P.D.	6,977 V.P.D.
A.A.D.T. (2042)	7,218 V.P.D.	9,561 V.P.D.
D.H.V. (2042)	9% V.P.H.	10% V.P.H.
DIRECTIONAL DISTRIBUTION	58% P.O.S.	53% P.O.S.
TRUCKS	6% D.H.V.	6% D.H.V.
	4% A.A.D.T.	4% A.A.D.T.
DESIGN DATA		
DESIGN SPEED	30 M.P.H.	30 M.P.H.
PROJECT DESIGN CRITERIA	4R (NON-FREEWAY)	4R (NON-FREEWAY)
FUNCTIONAL CLASSIFICATION	MINOR ARTERIAL (NORTH) MAJOR COLLECTOR (SOUTH)	MINOR ARTERIAL
RURAL/URBAN	URBAN	URBAN (SUBURBAN)
TERRAIN	LEVEL	LEVEL
ACCESS CONTROL	NONE	NONE



PROJECT LOCATION SHOWN BY  
SHELBY COUNTY

LATITUDE: 39°30'16.73" N LONGITUDE: 85°47'13.12" W

BRIDGE LENGTH:	X	FT.
ROADWAY LENGTH:	1,617	FT.
TOTAL LENGTH:	1,617	FT.
MAX. GRADE:	3.5	%



INDIANA DEPARTMENT OF TRANSPORTATION  
STANDARD SPECIFICATIONS DATED 2020 TO  
BE USED WITH THESE PLANS.

PRELIMINARY

PLANS  
PREPARED BY: CORRADINO, LLC 317-488-2363  
PHONE NUMBER  
CERTIFIED BY: DATE  
APPROVED FOR LETTING: INDIANA DEPARTMENT OF TRANSPORTATION DATE

	BRIDGE FILE
	N/A
	DESIGNATION
	1702775
SURVEY BOOK	SHEETS
1	of 47
CONTRACT	PROJECT
R-41302	1702775

File Name: F:\4561-Shelbyville Roundabout\50 Plans\30 Sheet Drawings\20 Road Sheets\R-INDEX-01.dwg - Layout1  
Modified / By: April 9, 2021 3:51:10 PM / zshiers  
Plotted / By: April 9, 2021 3:57:21 PM / Stacey Johnson

UTILITIES

COMMUNICATION:

AT&T  
MATT SPINDLER  
317-265-3050  
MS4822@ATT.COM  
  
COMCAST  
THOMAS DAVIS  
THOMAS\_DAVIS3@COMCAST.COM  
  
WINDSTREAM  
MARK MILLS  
217-876-7194  
MARK.MILLS@WINDSTREAM.COM

WATER:

INDIANA AMERICAN WATER  
JARED BURNETT  
317- 885-2428  
JARED.BURNETT@AMWATER.COM

SEWER:

CITY OF SHELBYVILLE  
  
MATTHEW J. HOUSE  
CITY OF SHELBYVILLE  
317-392-5102  
MHOUSE@CITYOFSHELBYVILLE.IN

ELECTRIC:

DUKE ENERGY  
CINDY ROWLAND  
317-776-5341  
CINDY.ROWLAND@DUKE-ENERGY.COM

SHELBYVILLE WATER  
RESOURCE RECOVERY  
FACILITY


CITY OF SHELBYVILLE  
KEVIN KREDIT  
317-392-5131  
KKREDIT@CITYOFSHELBYVILLE.IN

GAS:

VECTREN  
JOHN EASTHAM  
765-287-2119  
JEASTHAM@VECTREN.COM

"HOLEY MOLEY SAVES"

DON'T  
DIG  
BLIND



1-800-382-5544  
CALL TOLL FREE  
1-800-428-5200  
FOR CALLS OUTSIDE OF INDIANA

811

Know what's below.  
Call before you dig.

	GENERAL NOTES
**	ALL EARTH SHOULDERS, MEDIAN AREAS, AND CUT OR FILL SLOPES SHALL BE PLAIN OR MULCH SEEDED EXCEPT WHERE SODDING IS SPECIFIED
	THE FINAL CROSS SECTIONS OF THE GRADING CONTRACT WILL BE THE ORIGINAL CROSS SECTIONS OF THE PAVING CONTRACT. HOWEVER, PARTIAL OR COMPLETE CROSS SECTIONS SHALL BE TAKEN IF NECESSARY TO DETERMINE THE ACTUAL EXCAVATION QUANTITIES.
	THE PAPER RELOCATION WILL BE CROSS SECTIONED BY THE ENGINEER BEFORE CONSTRUCTION.
	EXISTING ASPHALT PAVEMENT LOCATED OUTSIDE THE CONSTRUCTION LIMITS, BETWEEN STA. ____ AND STA. ____, SHALL BE REMOVED AS DIRECTED.
	THE QUANTITY OF PEAT EXCAVATION SHOWN ON THE PLANS HAS BEEN ESTIMATED ON THE BASIS OF THEORETICAL CROSS SECTIONS BY USING TREATMENT OF EXISTING FILLS, TREATMENT BY REMOVAL, OR TREATMENT BY DISPLACEMENT, WHERE EACH TREATMENT APPLIES.
**	SOILS UNDER THE EXISTING PAVEMENT MAY BECOME UNSTABLE DURING CONSTRUCTION WHEN EXPOSED TO PRECIPITATION AND CONSTRUCTION TRAFFIC. IN SUCH EVENT, THE FOUNDATION MATERIAL SHALL BE IMPROVED BY REMOVING SOFT OR LOOSE MATERIAL AND REPLACING WITH COMPACTED B-BORROW. THE TOTAL FOUNDATION IMPROVEMENT OF 1100 CYS. IS BASED ON 15% OF THE SUBGRADE AREA PER THE GEOTECHNICAL REPORT.
**	IF THE SOILS UNDER THE PROPOSED MODULAR BLOCK WALL ARE DEEMED UNSUITABLE FOR CONSTRUCTION OF THE WALL, SAID SOILS SHALL BE UNDERCUT AND RE-ESTABLISHED TO GRADE WITH B-BORROW COMPACTED TO 100% OF THE MAXIMUM DENSITY. DEPTH AND EXTENT OF UNDERCUTTING WILL BE DEPENDANT ON SITE CONDITIONS AT THE TIME OF CONSTRUCTION.

INDEX	
SHEET NO.	DESIGNATION
1	TITLE
2	INDEX AND GENERAL NOTES
3	TYPICAL CROSS SECTIONS
4	PLAT NO. 1
5 - 9	ROUNDAABOUT GEOMETRIC TIE-IN DETAILS
10	CONTROL POINTS
11 - 12	MAINTENANCE OF TRAFFIC
13	PLAN AND PROFILE - SOUTH MILLER STREET
14	PLAN AND PROFILE - MCKAY ROAD
15-20	CONSTRUCTION DETAILS
21	CURB RAMP DETAILS
22	EROSION CONTROL DETAILS
23	JOINT DETAILS
24	SIGN DETAILS
25	LIGHTING DETAILS
26	PAVEMENT MARKING DETAILS
27	SHEET SIGN SUMMARY TABLES
28	PAVEMENT MARKINGS TABLE
29	APPROACH TABLES
30	MISCELLANEOUS DETAILS
31	UNDERDRAIN TABLES
32	STRUCTURE DATA TABLE
33	PIPE MATERIAL TABLE
34-57	CROSS SECTIONS

REVISIONS		
SHEET NO.	DATE	REVISED
.	.	.

PRELIMINARY

RECOMMENDED  
FOR APPROVAL

DESIGN ENGINEER

DATE

DESIGNED: AJD

DRAWN: SEJ

CHECKED: ZRE

CHECKED: AJD

INDIANA  
DEPARTMENT OF TRANSPORTATION

INDEX AND GENERAL NOTES

HORIZONTAL SCALE  
NONE

VERTICAL SCALE  
NONE

SHEET  
2 of 47

CONTRACT  
R-41302

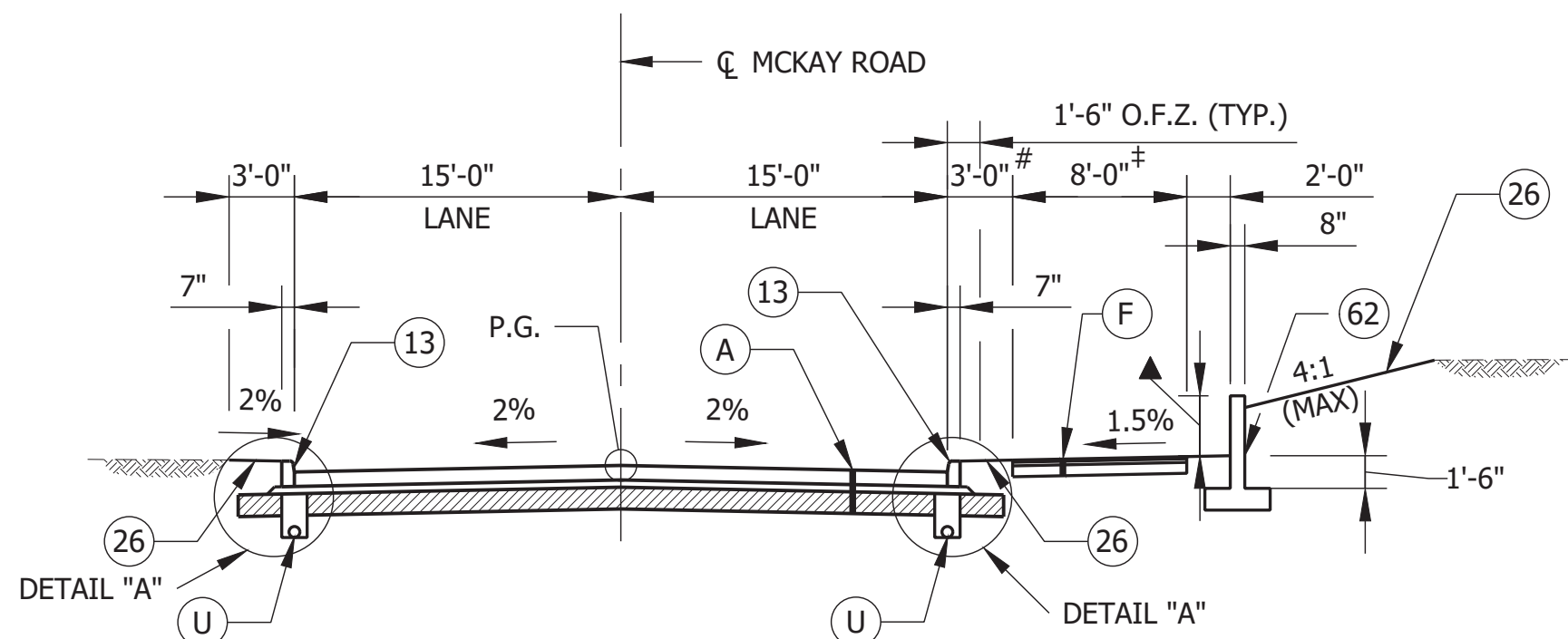
BRIDGE FILE  
N/A

DESIGNATION  
1702775

PROJECT  
1702775

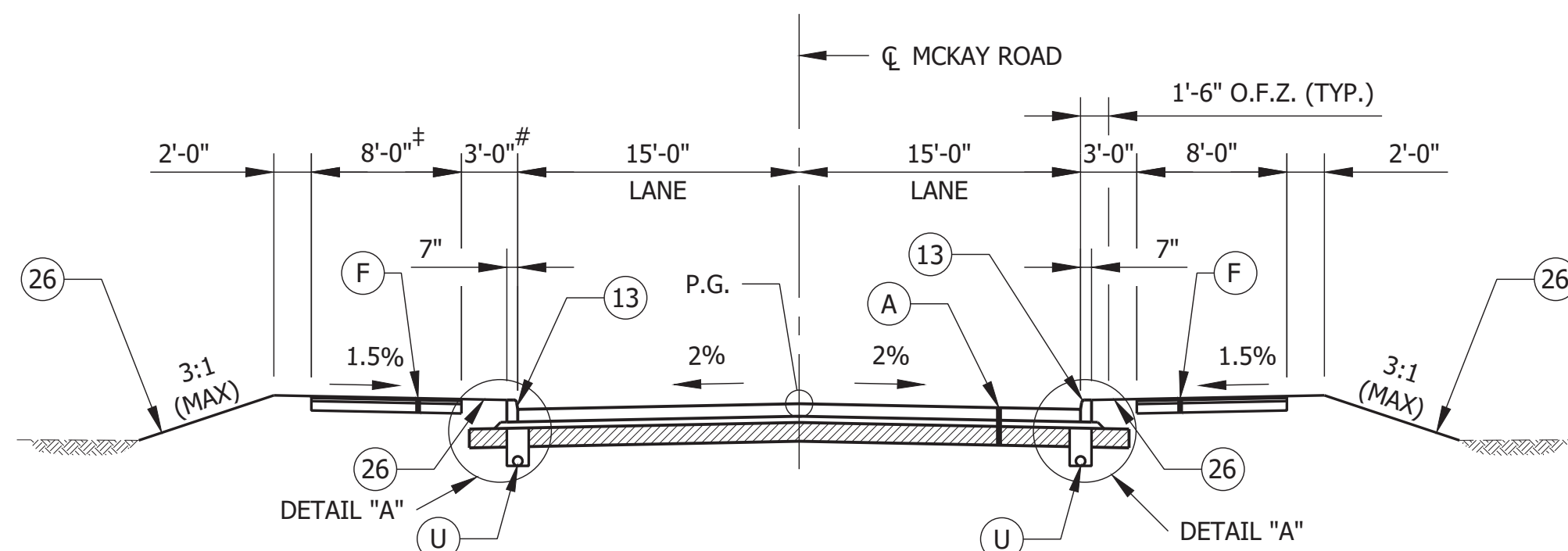


File Name: F:\4561-Shelbyville Roundabout\50 Plans\30 Sheet Drawings\20 Road Sheets\S-TYP-SEC-01.dwg - Layout1  
Modified / By: April 9, 2021 3:51:14 PM / zehlers  
Plotted / By: April 9, 2021 3:57:30 PM / Stacey Johnson



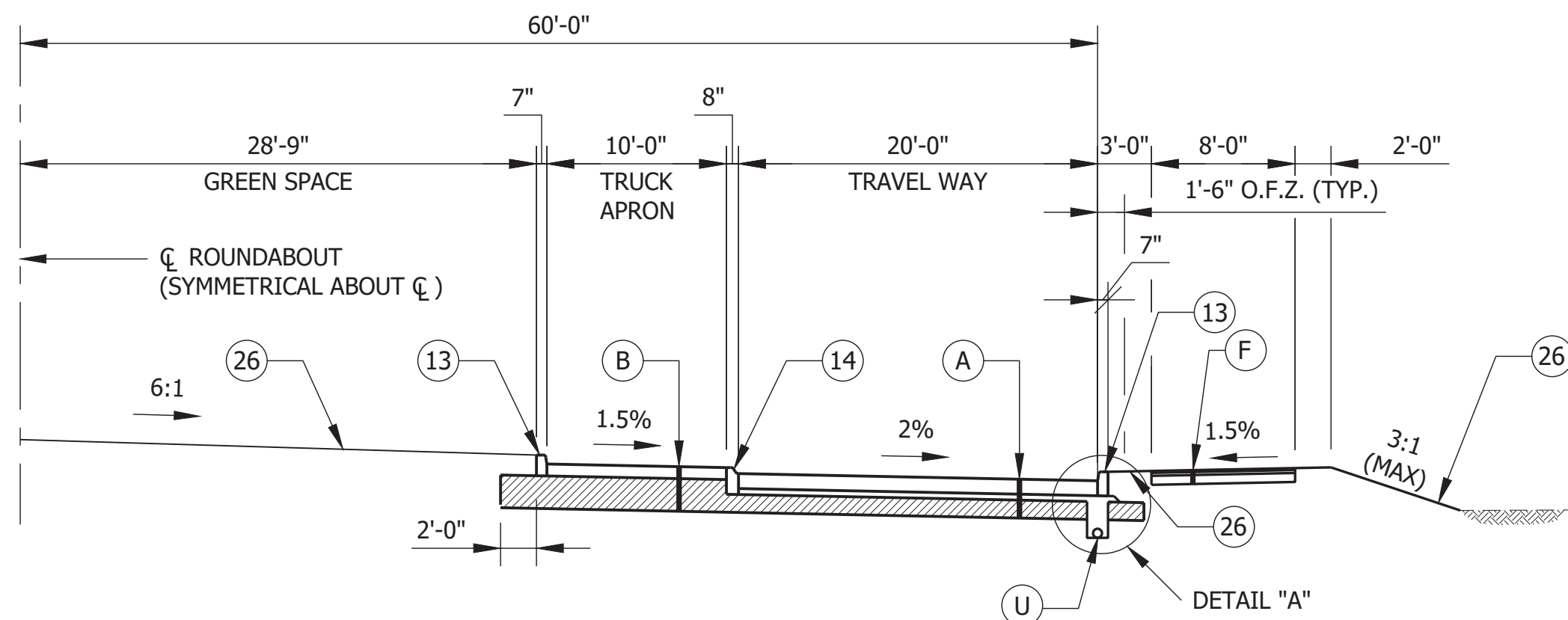
MCKAY ROAD - EAST END

STA. 209+25.09 "PR-A" TO STA. 211+90.27 "PR-A"  
# - VARIES FROM 3'-0" AT STA. 211+40.27 TO 5'-0" AT STA. 211+90.27  
+ - VARIES FROM 8'-0" AT STA. 211+40.27 TO 5'-0" AT STA. 211+90.27  
▲ - VARIES FROM 1'-6" AT STA. 210+00.00 TO 2'-9" AT STA. 210+75.00  
AND FROM 2'-9" AT STA. 211+00 TO 1'-6" AT STA. 211+75.00



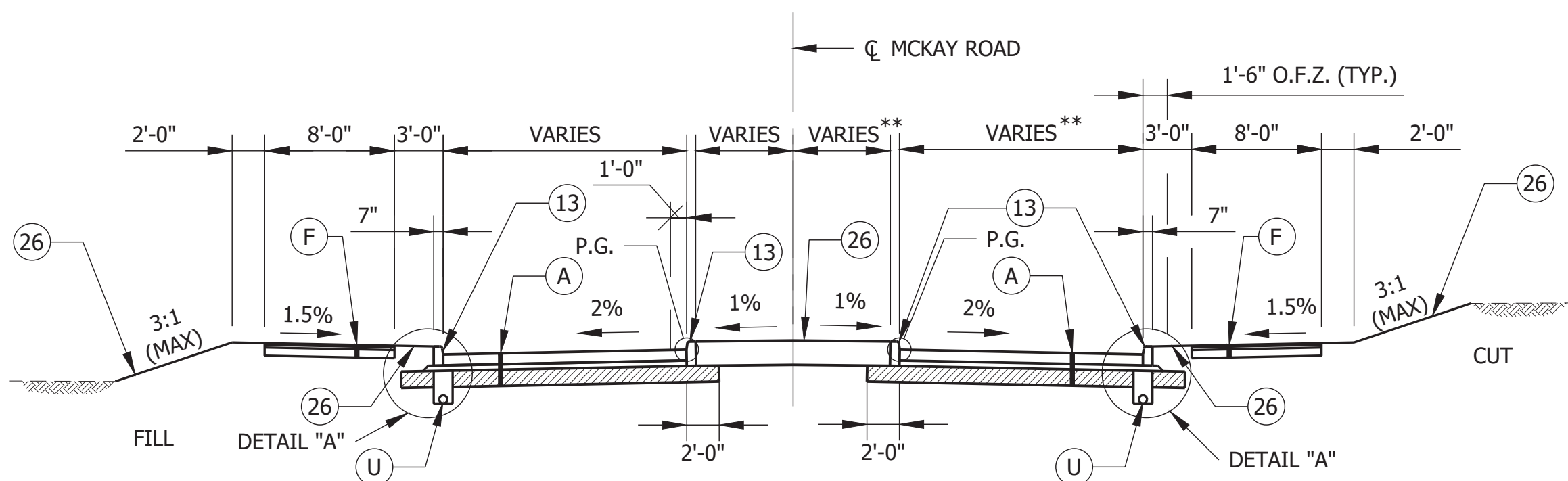
MCKAY ROAD - WEST END

STA. 203+50.00 "PR-A" TO STA. 208+05.09 "PR-A"  
# - VARIES FROM 5'-0" AT STA. 203+50.00 TO 3'-0" AT STA. 204+00.00  
+ - VARIES FROM 5'-0" AT STA. 203+50.00 TO 8'-0" AT STA. 204+00.00



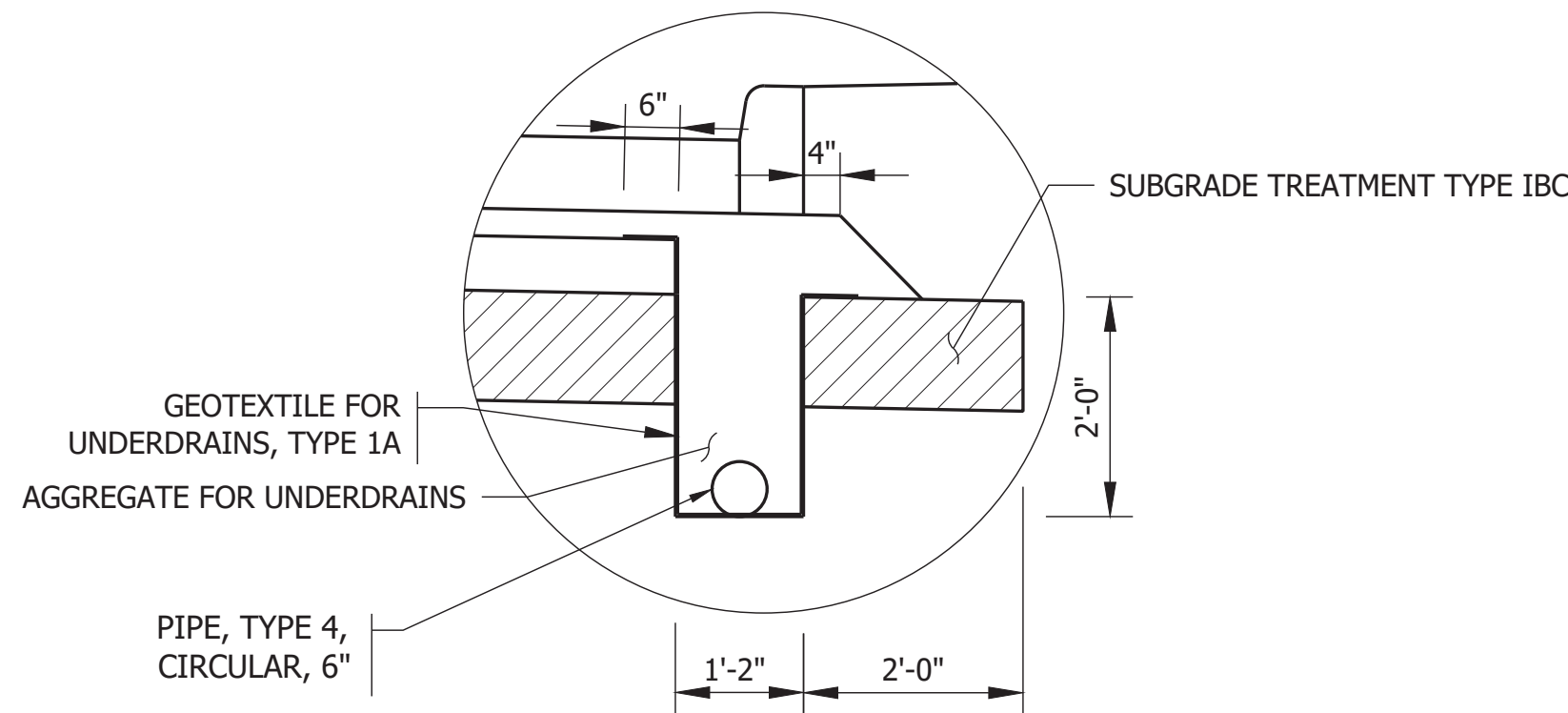
TYPICAL HALF-SECTION  
SOUTH MILLER STREET AND MCKAY ROAD ROUNDABOUT

STA. 208+05.09 "PR-A" TO STA. 209+25.09 "PR-A"  
STA. 19+58.07 "PR-B" TO STA. 20+78.07 "PR-B"

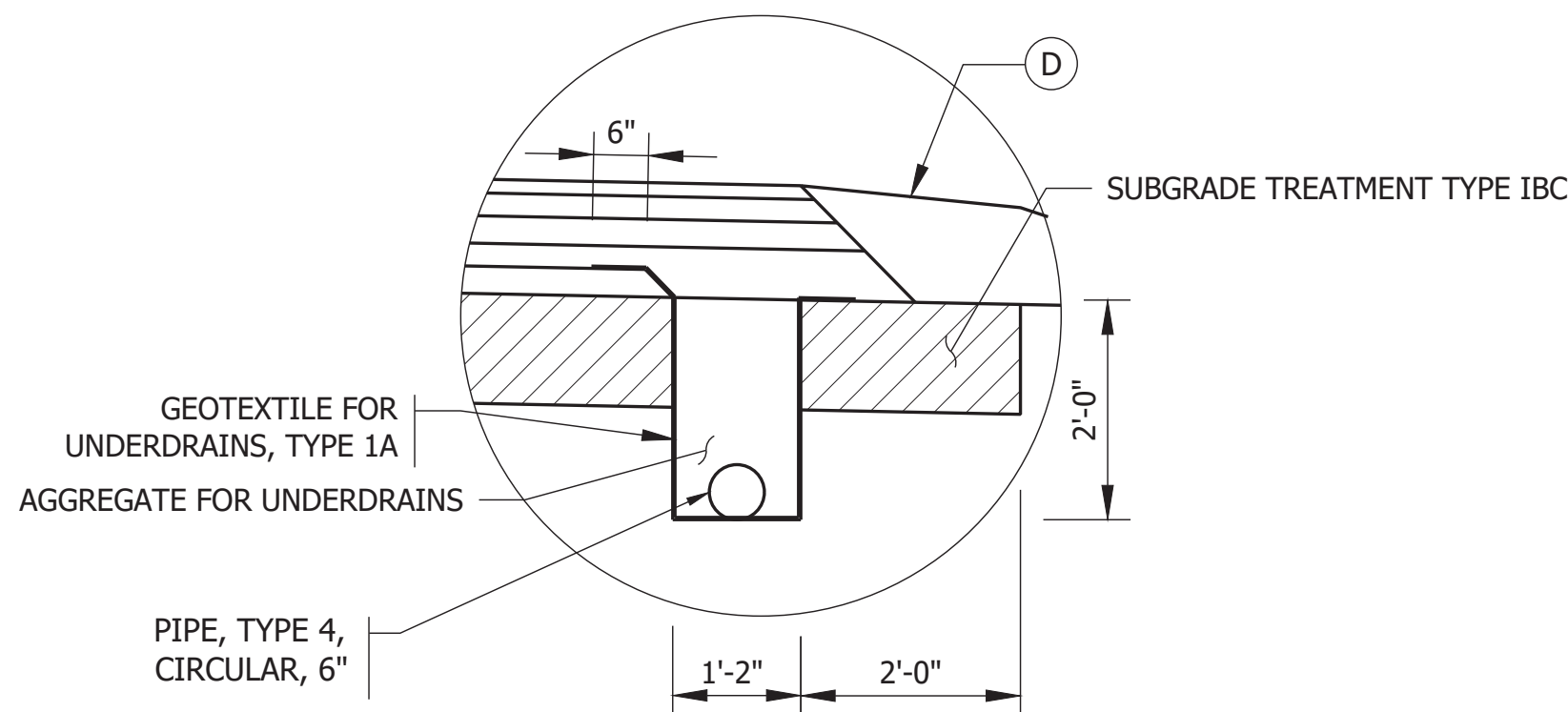


SPLITTER ISLAND TYPICAL SECTION

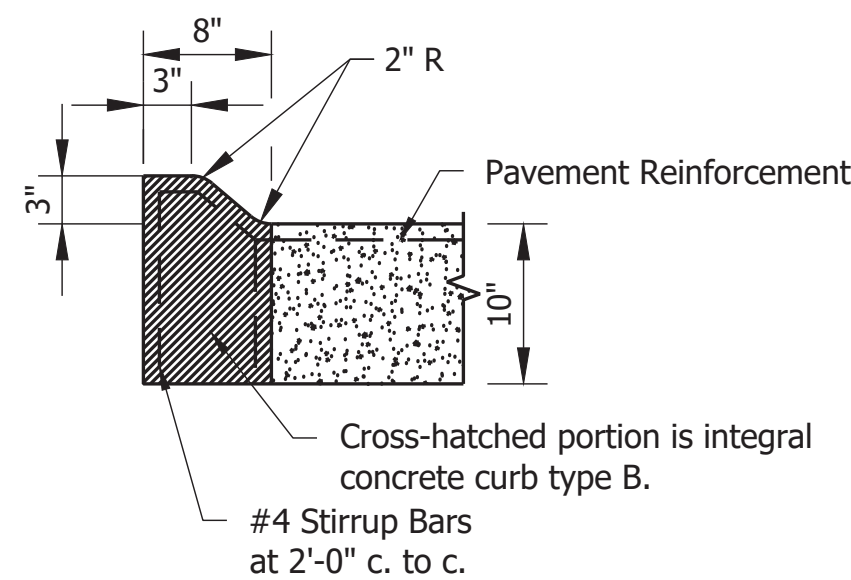
STA. 206+34.42 TO STA. 208+05.09 "PR-A"  
STA. 209+25.09 TO STA. 209+95.19 "PR-A"  
STA. 18+34.85 TO STA. 19+58.07 "PR-B"  
STA. 20+78.07 TO STA 21+33.20 "PR-B"  
\*\* - SEE ROUNDABOUT LAYOUT DETAILS



DETAIL "A"  
NOT TO SCALE



DETAIL "B"  
NOT TO SCALE



CONCRETE CURB, INTEGRAL, TYPE B, MODIFIED  
NOT TO SCALE

LEGEND

- (A) QC/QA-PCCP, 10" ON SUBBASE FOR PCCP (3" COARSE AGGREGATE NO. 8 ON 6" COARSE AGGREGATE NO. 53) ON SUBGRADE TREATMENT, TYPE IBC
- (B) PCCP 8" ON 15" COMPACTED AGGREGATE, NO. 53 BASE, ON SUBGRADE TREATMENT, TYPE IBC
- (C) 220 LBS/SYS QC/QA HMA, 3, 70, SURFACE, 9.5 MM ON 330 LBS/SYS QC/QA HMA, 3, 70, INTERMEDIATE, 19.0 MM ON 550 LBS/SYS QC/QA HMA, 3, 64, BASE, 19.0 MM ON 300 LBS/SYS QC/QA-HMA, 3, 76, INTERMEDIATE OG, 19.0 MM ON 6" COMPACTED AGGREGATE, NO. 53 BASE, ON SUBGRADE TREATMENT, TYPE IBC
- (13) CONCRETE CURB, INTEGRAL
- (14) CONCRETE CURB, INTEGRAL, TYPE B, MODIFIED (SEE DETAIL THIS SHEET)
- (20) CONCRETE CURB
- (26) SODDING
- (62) MODULAR BLOCK RETAINING WALL
- (F) HMA FOR SIDEWALKS, CONSISTING OF 140 LBS/SYS HMA, SURFACE, TYPE B ON 220 LBS/SYS HMA, INTERMEDIATE, TYPE B ON 6" COMPACTED AGGREGATE, NO. 53 BASE ON SUBGRADE TREATMENT, TYPE IBC
- (D) VARIABLE DEPTH COMPACTED AGGREGATE, NO. 53
- (U) PIPE, TYPE 4, CIRCULAR, 6"

File Name: F:\4561-Shelbyville Roundabout\50 Plans\30 Sheet Drawings\20 Road Sheets\S-TYP-SEC-01.dwg - Layout1  
Modified / By: April 9, 2021 3:51:14 PM / zehlers  
Plotted / By: April 9, 2021 3:57:30 PM / Stacey Johnson

PRELIMINARY

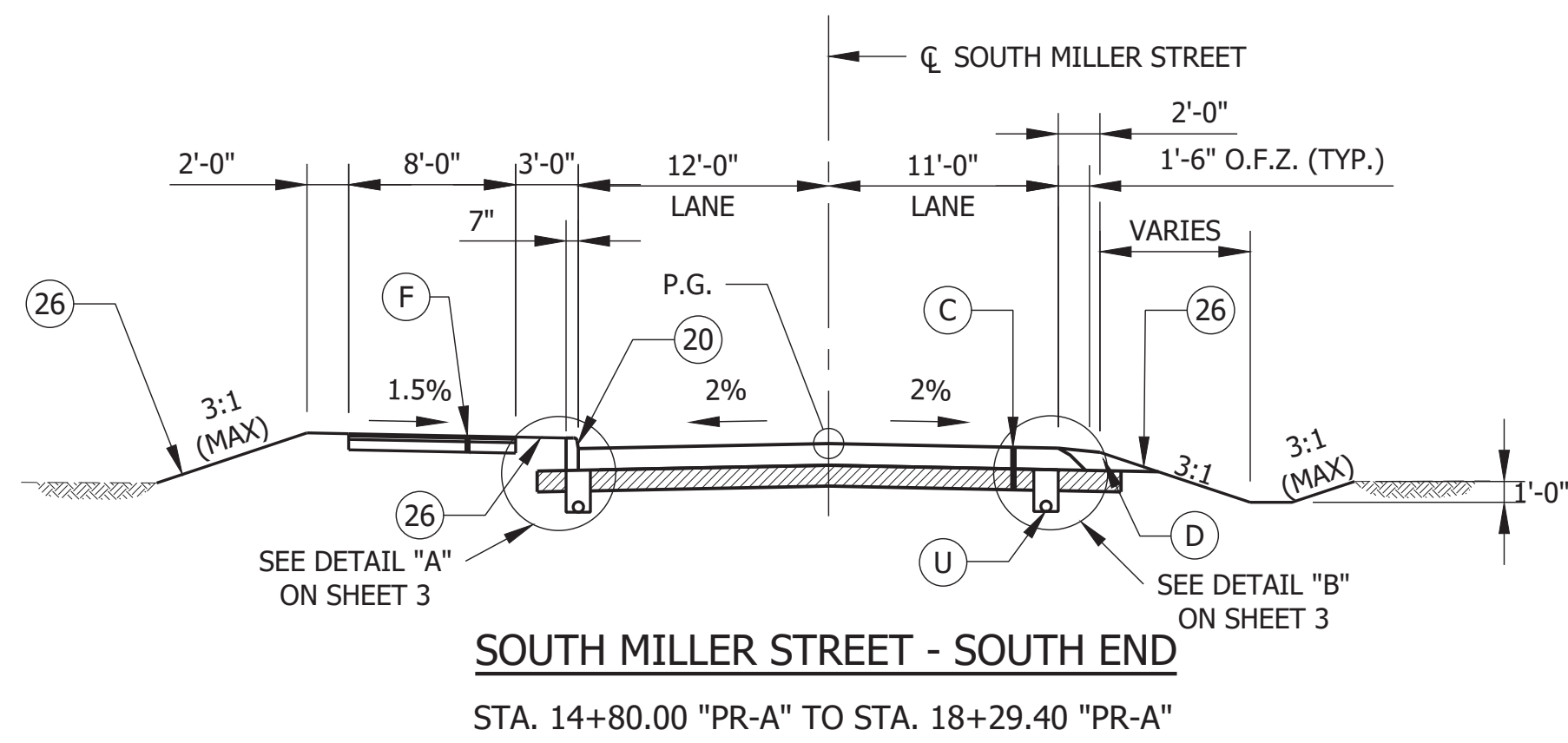
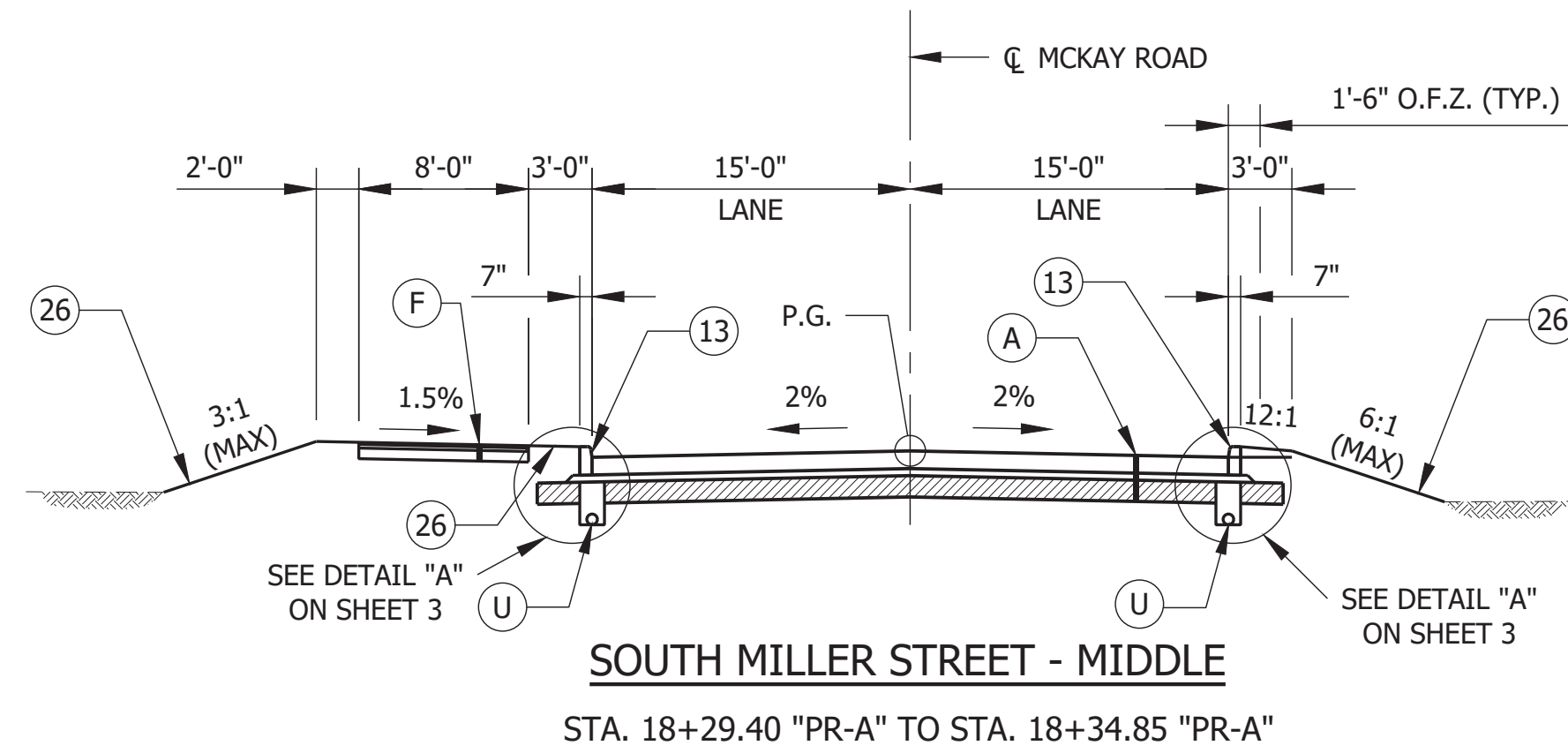
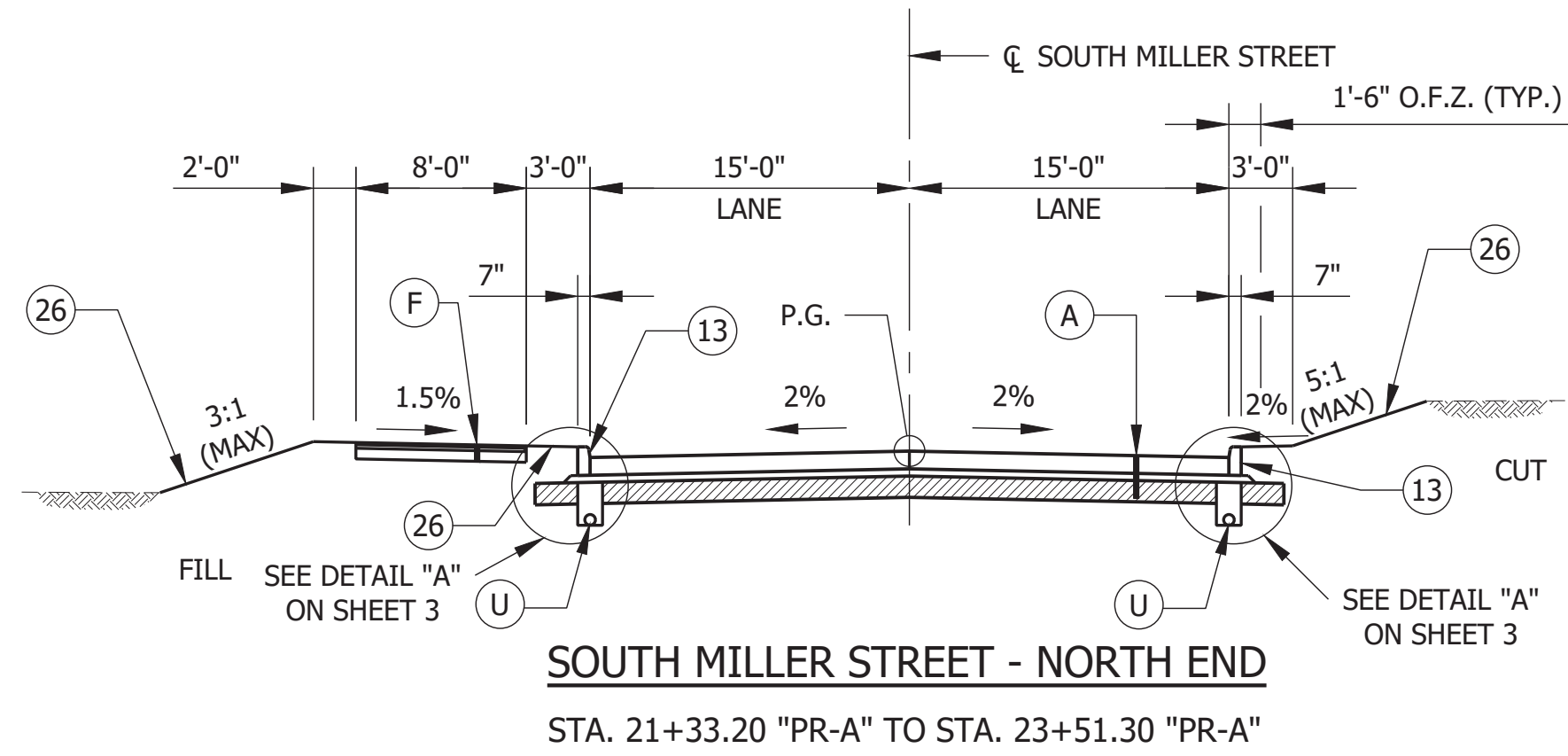
RECOMMENDED FOR APPROVAL \_\_\_\_\_ DESIGN ENGINEER \_\_\_\_\_ DATE \_\_\_\_\_  
DESIGNED: \_\_\_\_\_ AJD \_\_\_\_\_ DRAWN: \_\_\_\_\_ ZRE \_\_\_\_\_  
CHECKED: \_\_\_\_\_ ZRE \_\_\_\_\_ CHECKED: \_\_\_\_\_ AJD \_\_\_\_\_

INDIANA  
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS

HORIZONTAL SCALE 1/8" = 1'-0" UNLESS NOTED	BRIDGE FILE N/A
VERTICAL SCALE N/A	DESIGNATION 1702775
	SHEET 3 of 47
CONTRACT R-41302	PROJECT 1702775

F:\4561-Shelbyville Roundabout\50 Plans\30 Sheet Drawings\20 Road Sheets\S-TYP-SEC-01.dwg - Layout2  
April 9, 2021 3:51:14 PM / zehlers  
April 9, 2021 3:57:31 PM / Stacey Johnson  
File Name:  
Modified / By:  
Plotted / By:



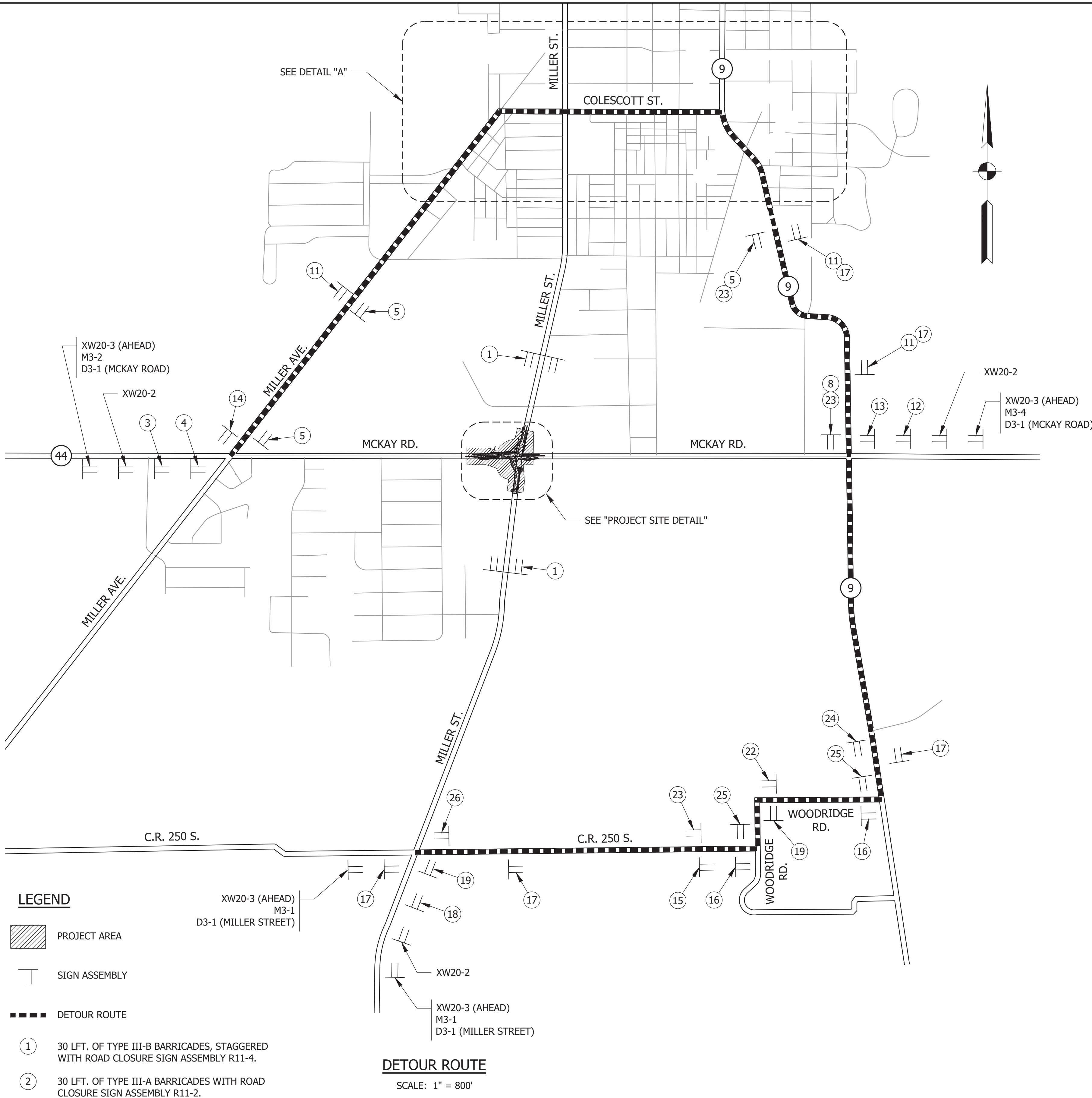
#### LEGEND

- (A) QC/QA-PCCP, 10" ON SUBBASE FOR PCCP (3" COARSE AGGREGATE NO. 8 ON 6" COARSE AGGREGATE NO. 53) ON SUBGRADE TREATMENT, TYPE IBC
- (B) PCCP 8" ON 15" COMPACTED AGGREGATE, NO. 53 BASE, ON SUBGRADE TREATMENT, TYPE IBC
- (C) 220 LBS/SYS QC/QA HMA, 3, 70, SURFACE, 9.5 MM ON 330 LBS/SYS QC/QA HMA, 3, 70, INTERMEDIATE, 19.0 MM ON 550 LBS/SYS QC/QA HMA, 3, 64, BASE, 19.0 MM ON 300 LBS/SYS QC/QA-HMA, 3, 76, INTERMEDIATE OG, 19.0 MM ON 6" COMPACTED AGGREGATE, NO. 53 BASE, ON SUBGRADE TREATMENT, TYPE IBC
- (13) CONCRETE CURB, INTEGRAL
- (14) CONCRETE CURB, INTEGRAL, TYPE B, MODIFIED (SEE DETAIL THIS SHEET)
- (20) CONCRETE CURB
- (26) SODDING
- (62) MODULAR BLOCK RETAINING WALL
- (F) HMA FOR SIDEWALKS, CONSISTING OF 140 LBS/SYS HMA, SURFACE, TYPE B ON 220 LBS/SYS HMA, INTERMEDIATE, TYPE B ON 6" COMPACTED AGGREGATE, NO. 53 BASE ON SUBGRADE TREATMENT, TYPE IBC
- (D) VARIABLE DEPTH COMPACTED AGGREGATE, NO. 53
- (U) PIPE, TYPE 4, CIRCULAR, 6"

	PRELIMINARY	RECOMMENDED FOR APPROVAL _____		INDIANA DEPARTMENT OF TRANSPORTATION	HORIZONTAL SCALE 1/8" = 1'-0" UNLESS NOTED		BRIDGE FILE N/A		
		DESIGN ENGINEER _____			VERTICAL SCALE N/A		DESIGNATION 1702775		
		DATE _____					SHEET		
		DESIGNED: _____			DRAWN: _____		4 of 47		
		CHECKED: _____			CHECKED: _____		PROJECT 1702775		
				TYPICAL SECTIONS					



File Name: F:\4561-Shelbyville Roundabout\50 Plans\30 Sheet Drawings\20 Road Sheets\R-MOT-DETOUR-01.dwg - DETOUR ROUTE  
Modified / By: April 9, 2021 3:50:48 PM / zehlers  
Plotted / By: April 9, 2021 3:57:43 PM / Stacey Johnson



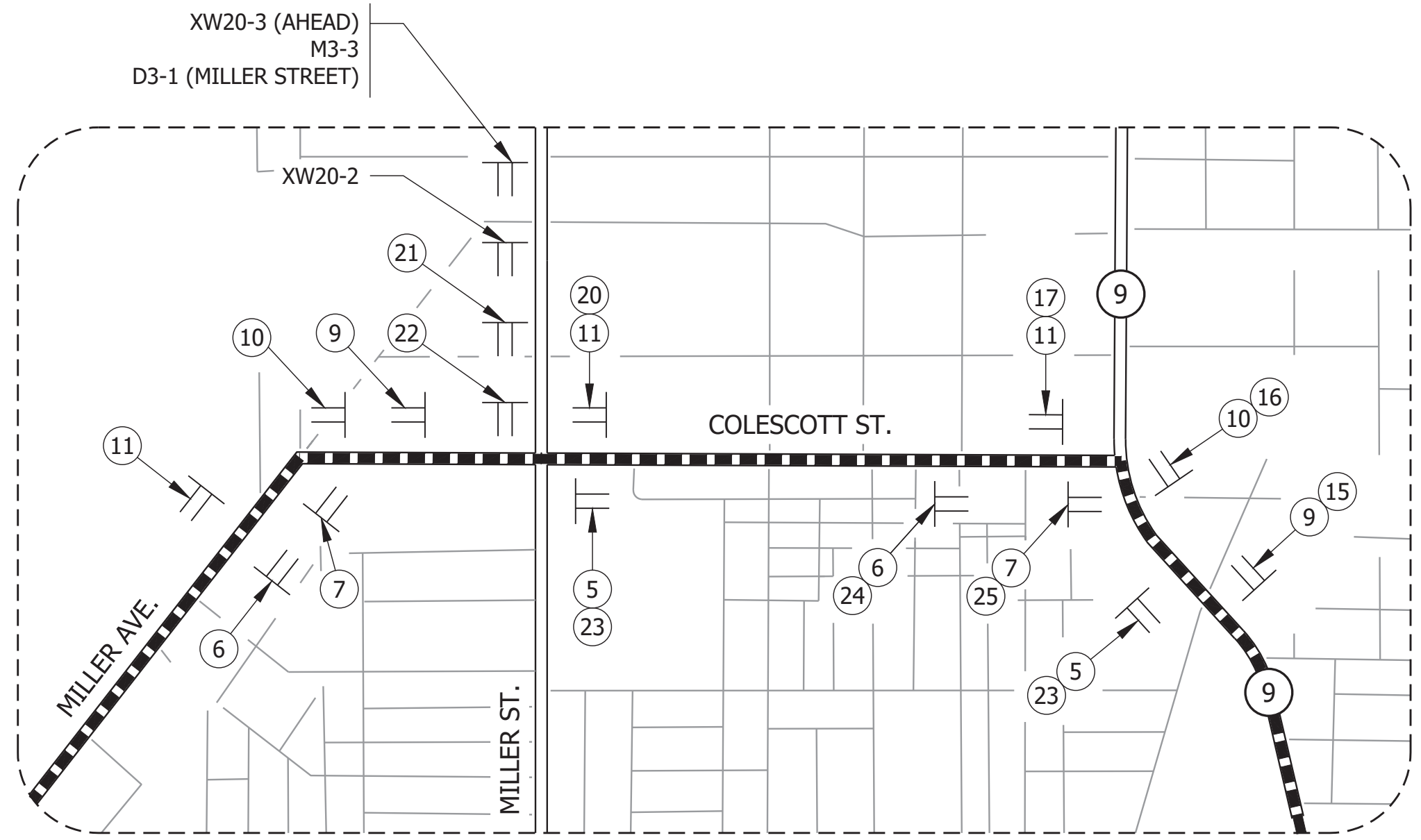
**LEGEND**

- PROJECT AREA
- SIGN ASSEMBLY
- DETOUR ROUTE

- ① 30 LFT. OF TYPE III-B BARRICADES, STAGGERED WITH ROAD CLOSURE SIGN ASSEMBLY R11-4.
- ② 30 LFT. OF TYPE III-A BARRICADES WITH ROAD CLOSURE SIGN ASSEMBLY R11-2.

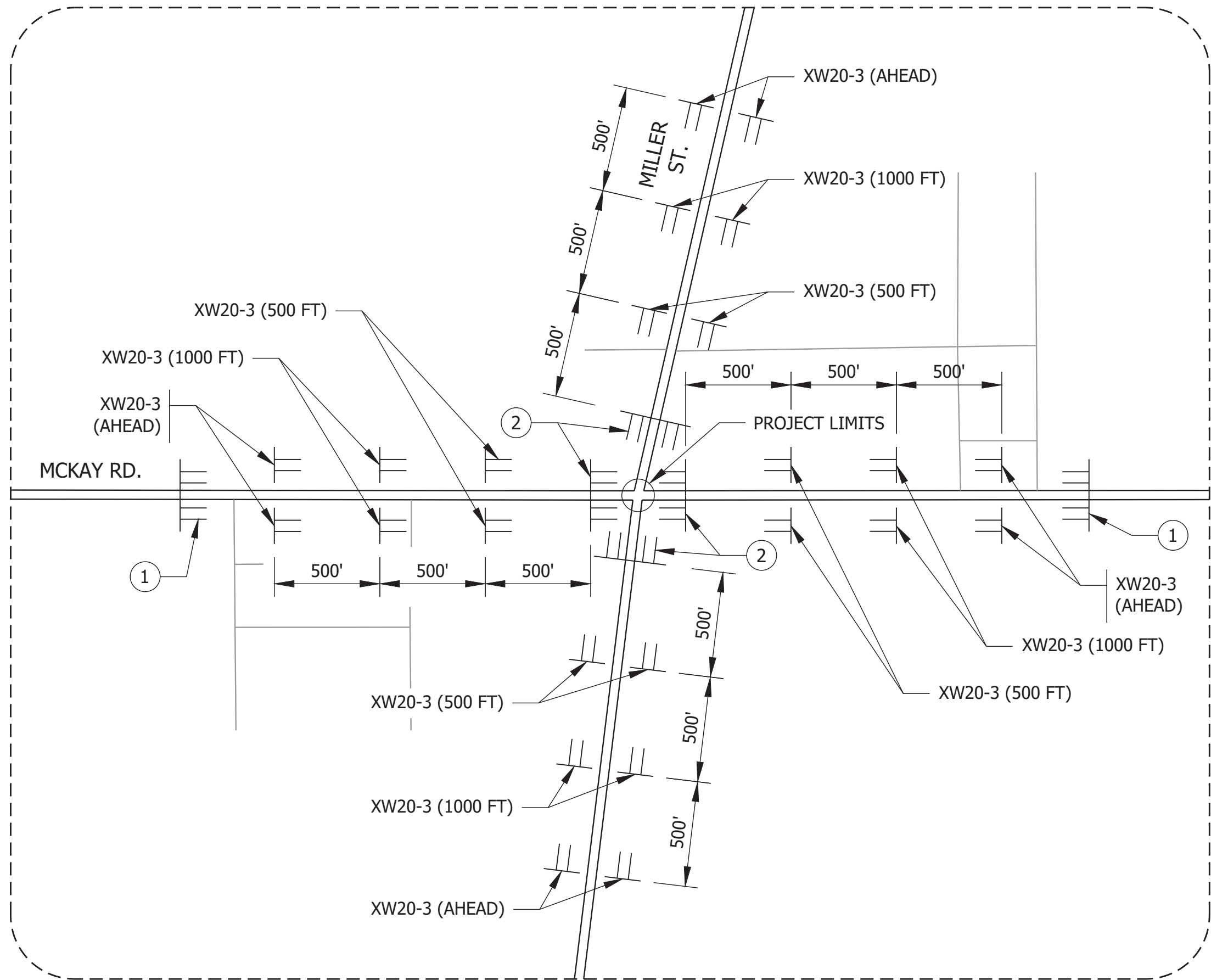
**DETOUR ROUTE**

SCALE: 1" = 800'



**DETAIL "A"**

SCALE: 1" = 500'



**PROJECT SITE DETAIL**

SCALE: 1" = 500'

**PRELIMINARY**

RECOMMENDED FOR APPROVAL _____		
DESIGN ENGINEER _____		DATE _____
DESIGNED: _____	AJD	DRAWN: _____
DRAWN: _____		SEJ
CHECKED: _____	ZRE	CHECKED: _____
CHECKED: _____		AJD

**INDIANA  
DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC**

HORIZONTAL SCALE		BRIDGE FILE	
AS NOTED		N/A	
VERTICAL SCALE		DESIGNATION	
N/A		1702775	
		SHEET	
		11	of 47
CONTRACT		PROJECT	
R-41302		1702775	

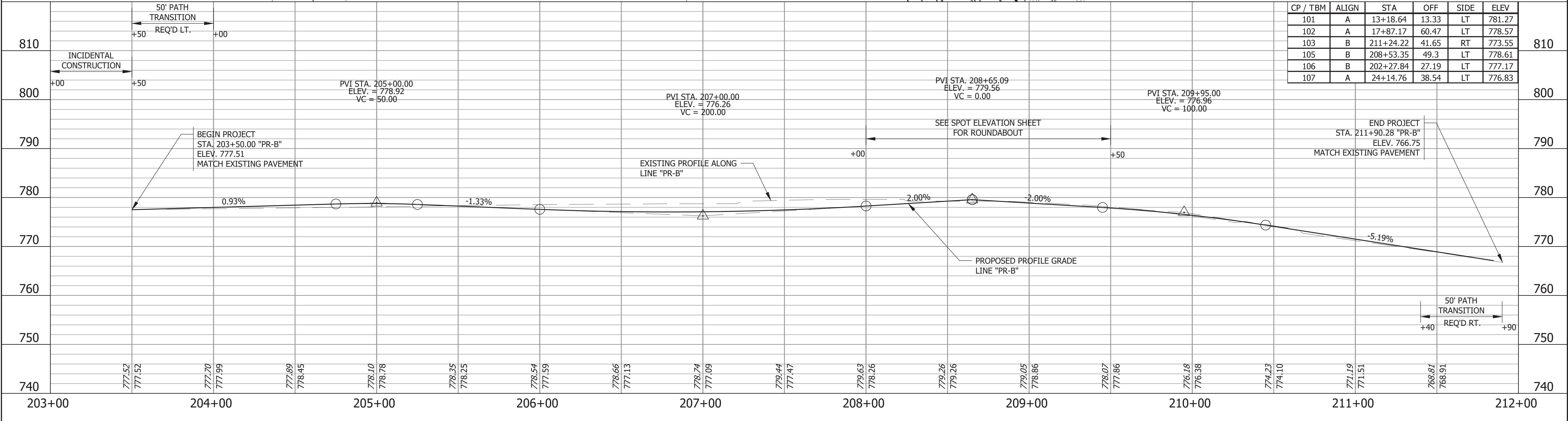
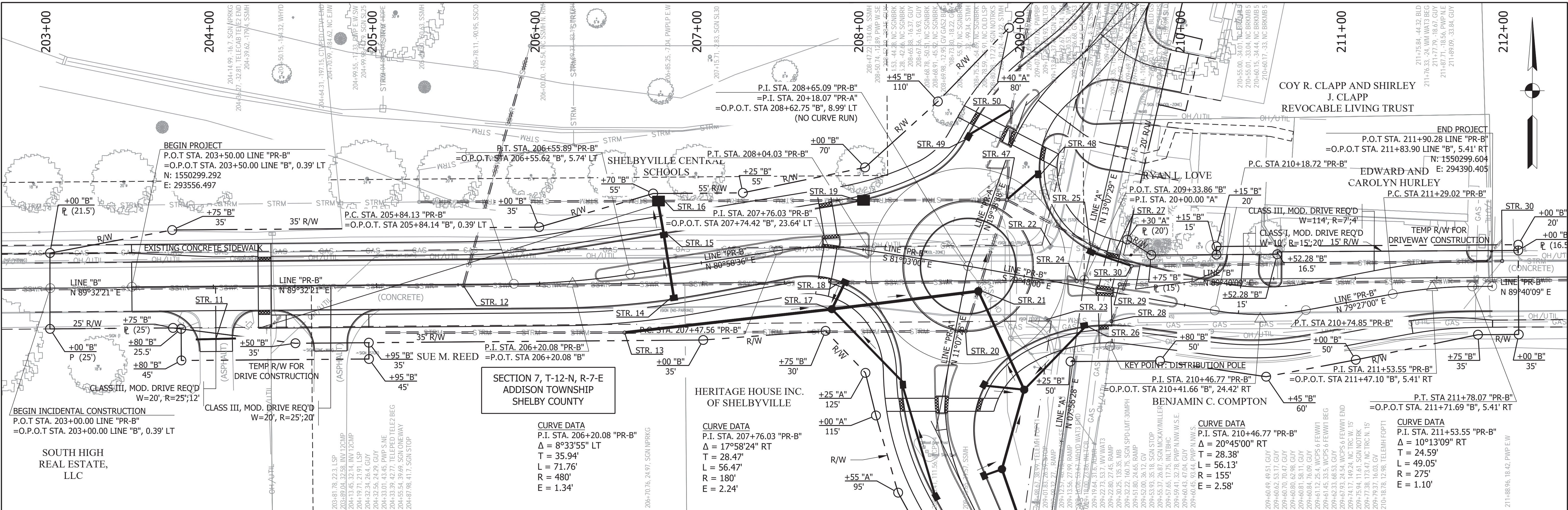






F:\4561-Shelbyville Roundabout\50 Plans\30 Sheet Drawings\20 Road Sheets\PLAN-PROFILE-01.dwg - McKay Road  
April 12, 2021 4:39:25 PM / zehlers  
April 12, 2021 4:42:31 PM / Zach Ehlers

File Name:  
Modified / By:  
Plotted / By:



**NOTES**

- FOR MATERIALS, DIMENSIONS, AND DRAINAGE STRUCTURE INFORMATION SEE CONSTRUCTION DETAILS
- ALL R/W THIS SHEET TO BE AS SHOWN
- ALL R/W ON THIS SHEET DESCRIBED FROM LINE "B" UNLESS OTHERWISE NOTED
- LINE "PR-B" AND LINE "PR-A" TO BE CONSTRUCTED
- SEE SHEET "X" FOR CONTROL POINT REFERENCE
- UTILITY CASTINGS TO BE ADJUSTED TO GRADE, AS REQUIRED, BASED ON FINAL CONDITIONS

**PRELIMINARY**

RECOMMENDED FOR APPROVAL \_\_\_\_\_ DESIGN ENGINEER \_\_\_\_\_ DATE \_\_\_\_\_

DESIGNED: AJD DRAWN: ZRE

CHECKED: ZRE CHECKED: AJD

**INDIANA**  
**DEPARTMENT OF TRANSPORTATION**

**MCKAY ROAD**  
**PLAN AND PROFILE**

HORIZONTAL SCALE 1" = 30'-0" UNLESS NOTED	BRIDGE FILE N/A
VERTICAL SCALE 1" = 10'-0" UNLESS NOTED	DESIGNATION 1702775
CONTRACT R-41302	SHEET 14 of 47
	PROJECT 1702775



F:\4561-Shelbyville Roundabout\50 Plans\30 Sheet Drawings\20 Road Sheets\R-CONSTR-DTLS-01.dwg - NORTHWEST  
April 10, 2021 4:28:48 PM / zehlers  
April 12, 2021 4:43:24 PM / Zach Ehlers  
File Name:  
Modified / By:  
Plotted / By:

24+00

203+00

204+00

205+00

206+00

207+00

208+00

209+00

210+00

211+00

LEGEND

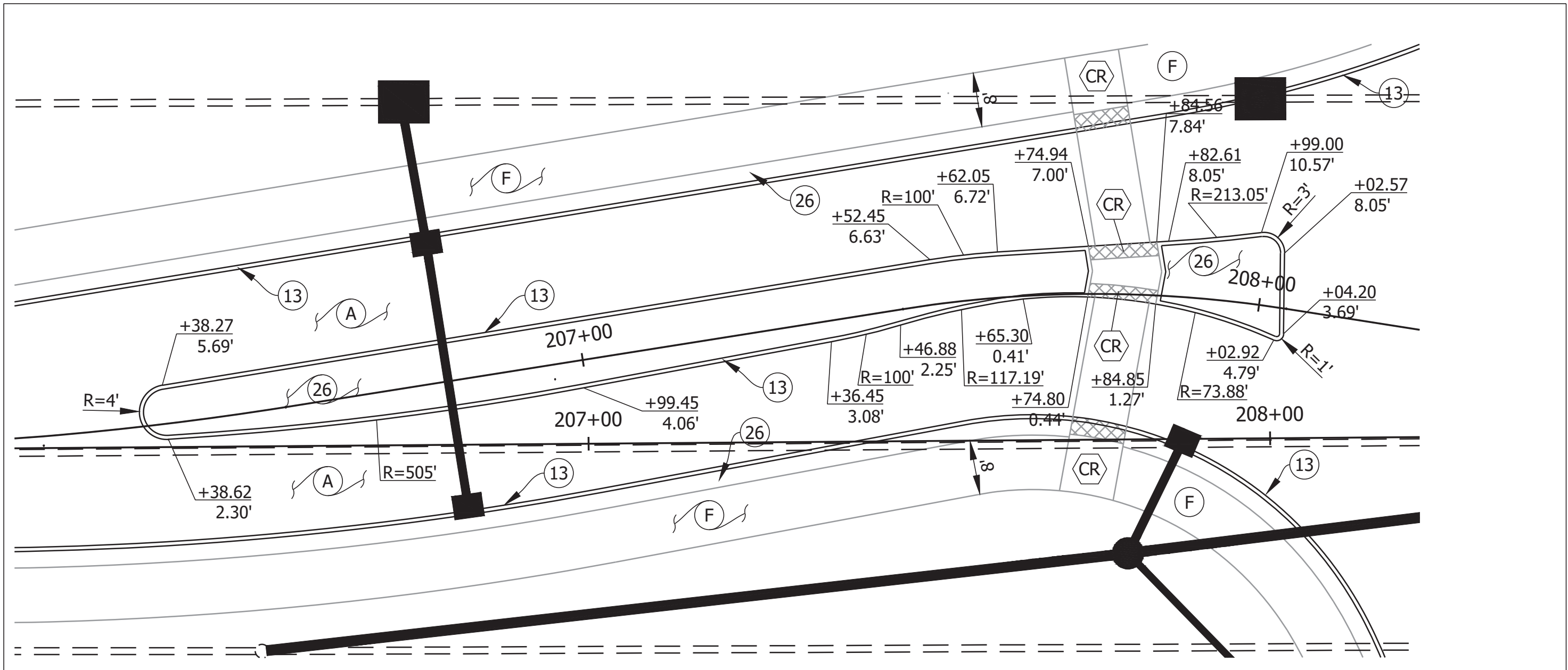
- (A) QC/QA-PCCP, 10" ON  
SUBBASE FOR PCCP  
(3" COARSE AGGREGATE NO. 8 ON  
6" COARSE AGGREGATE NO. 53) ON  
SUBGRADE TREATMENT, TYPE IBC
- (B) PCCP 8" ON  
14" COMPACTED AGGREGATE, NO. 53 BASE, ON  
SUBGRADE TREATMENT, TYPE IBC
- (C) 220 LBS/SYS QC/QA HMA, 3, 70, SURFACE, 9.5 MM ON  
330 LBS/SYS QC/QA HMA, 3, 70, INTERMEDIATE, 19.0 MM ON  
550 LBS/SYS QC/QA HMA, 3, 64, BASE, 19.0 MM ON  
300 LBS/SYS QC/QA-HMA, 3, 76, INTERMEDIATE OG, 19.0 MM  
ON  
6" COMPACTED AGGREGATE, NO. 53 BASE, ON  
SUBGRADE TREATMENT, TYPE IBC
- (13) CONCRETE CURB, INTEGRAL
- (14) CONCRETE CURB, INTEGRAL, TYPE B, MODIFIED
- (20) CONCRETE CURB
- (26) SODDING
- (62) MODULAR BLOCK RETAINING WALL
- (F) HMA FOR SIDEWALKS, CONSISTING OF  
140 LBS/SYS HMA, SURFACE, TYPE B ON  
220 LBS/SYS HMA, INTERMEDIATE, TYPE B ON  
6" COMPACTED AGGREGATE, NO. 53 BASE ON  
SUBGRADE TREATMENT, TYPE IBC
- (D) VARIABLE DEPTH COMPACTED AGGREGATE, NO. 53
- (CR) CURB RAMP
- (B1) PCCP FOR APPROACHES, 6" ON  
DENSE GRADED SUBBASE, 6" ON  
SUBGRADE TREATMENT, TYPE II
- (X) DETECTABLE WARNING SURFACE
- (ST) SIDEWALK TRANSITION

23+00

22+00

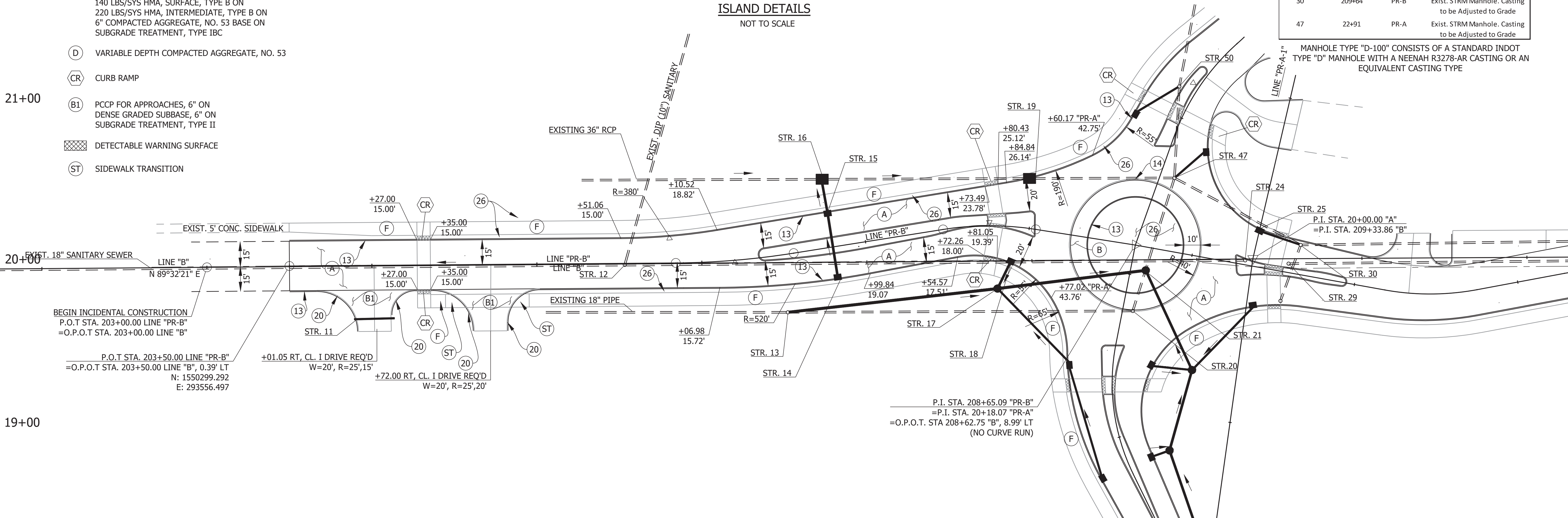
21+00

19+00



ISLAND DETAILS  
NOT TO SCALE

DRAINAGE STRUCTURE INFORMATION			
STURCTURE NUMBER	STATION	LINE	DESCRIPTION
11	204+00	PR-B	30 Lft. Of 15" Pipe with 2 Pipe End Sections Req'd.
12	205+54	PR-B	Exist. SS Manhole. Casting to be Adjusted to Grade
13	206+49	PR-B	Manhole Type "C-4" & 126 Lft. of 18" Pipe Req'd. Plug Ex. Pipe
14	206+80	PR-B	Inlet Type "B-15" & 44 Lft. of 15" Pipe Req'd.
15	206+80	PR-B	Inlet Type "C-15" & 18 Lft. of 15" Pipe Req'd.
16	206+80	PR-B	Manhole Type "C-4" Connect to Existing 36" Pipe
17	207+82	PR-B	Manhole Type "C-4" & 92 Lft. of 18" Pipe Req'd.
18	207+88	PR-B	Inlet Type "B-15" & 23 Lft. of 15" Pipe Req'd.
19	207+90	PR-B	* Manhole Type "D-100" Connect to Existing 36" Pipe
20	208+71	PR-B	Exist. Manhole to be Removed. Plug both Inlet & Outlet Pipes
21	208+74	PR-B	Manhole Type "C-4" Connect to Existing 18" Pipe
24	209+32	PR-B	Exist. SS Manhole. Casting to be Adjusted to Grade
29	209+60	PR-B	Exist. SS Manhole. Casting to be Adjusted to Grade
30	209+64	PR-B	Exist. STRM Manhole. Casting to be Adjusted to Grade
47	22+91	PR-A	Exist. STRM Manhole. Casting to be Adjusted to Grade



NOTES: ALL STATION/OFFSET CALLOUTS ARE CALLED OUT FROM LINE "PR-B" UNLESS OTHERWISE NOTED  
FOR CONSTRUCTION DETAILS OF ALL DRIVEWAYS, SEE THE DRIVEWAY DETAIL SHEET

PRELIMINARY

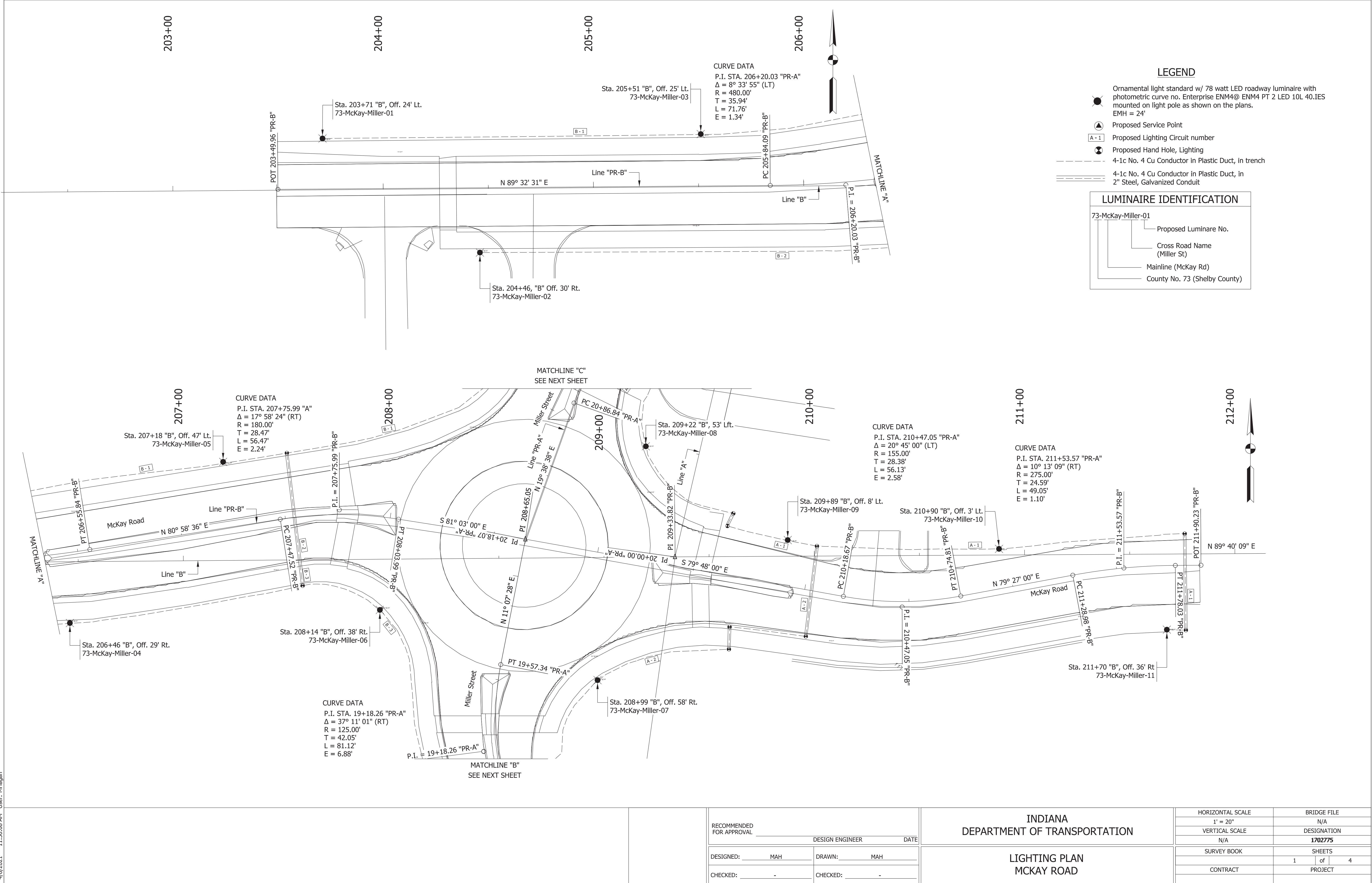
RECOMMENDED FOR APPROVAL _____	
DESIGN ENGINEER _____ DATE _____	
DESIGNED: _____	DRAWN: _____
CHECKED: _____	CHECKED: _____

INDIANA  
DEPARTMENT OF TRANSPORTATION

CONSTRUCTION DETAILS  
STA. 203+00.00 TO 208+65.09 LINE "PR-B"

HORIZONTAL SCALE 1" = 30' UNLESS NOTED	BRIDGE FILE N/A
VERTICAL SCALE N/A	DESIGNATION 1702775
	SHEET of 47
CONTRACT R-41302	PROJECT 1702775

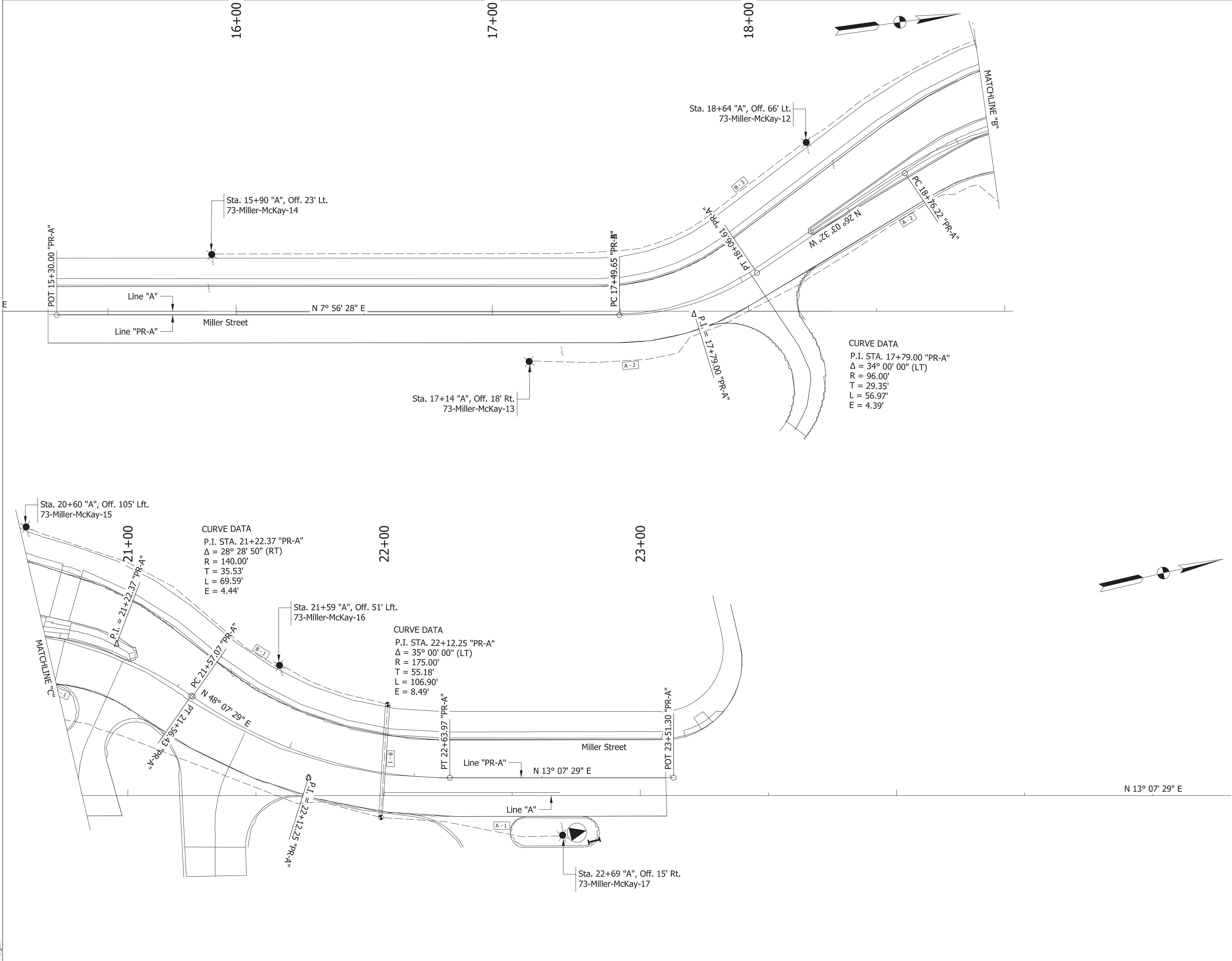




RECOMMENDED FOR APPROVAL _____	
DESIGNED: _____ MAH	DRAWN: _____ MAH
CHECKED: _____ -	CHECKED: _____ -

INDIANA DEPARTMENT OF TRANSPORTATION	
LIGHTING PLAN MCKAY ROAD	

HORIZONTAL SCALE	BRIDGE FILE	
1" = 20"	N/A	
VERTICAL SCALE	DESIGNATION	
N/A	1702775	
SURVEY BOOK	SHEETS	
	1	of 4
CONTRACT	PROJECT	



LEGEND

Ornamental light standard w/ 78 watt LED roadway luminaire with photometric curve no. Enterprise ENM4@ ENM4 PT 2 LED 10L 40.IES mounted on light pole as shown on the plans.  
EMH = 24'

Proposed Service Point

A - 1

Proposed Lighting Circuit number

Proposed Hand Hole, Lighting

4-1c No. 4 Cu Conductor in Plastic Duct, in trench

4-1c No. 4 Cu Conductor in Plastic Duct, in 2" Steel, Galvanized Conduit

LUMINAIRE IDENTIFICATION

73-McKay-Miller-01

Proposed Luminaire No.

Cross Road Name (Miller St)

Mainline (McKay Rd)

County No. 73 (Shelby County)

Model: Miller Street  
4/8/2021 11:50:09 AM User: M.Hagan

H:\Projects\2018\Engineering\18EN050 Shelbyville Lighting\CAD\Sheets\Lighting\18EN050\_Shelbyville Lighting2021\_REFERENCE.dgn

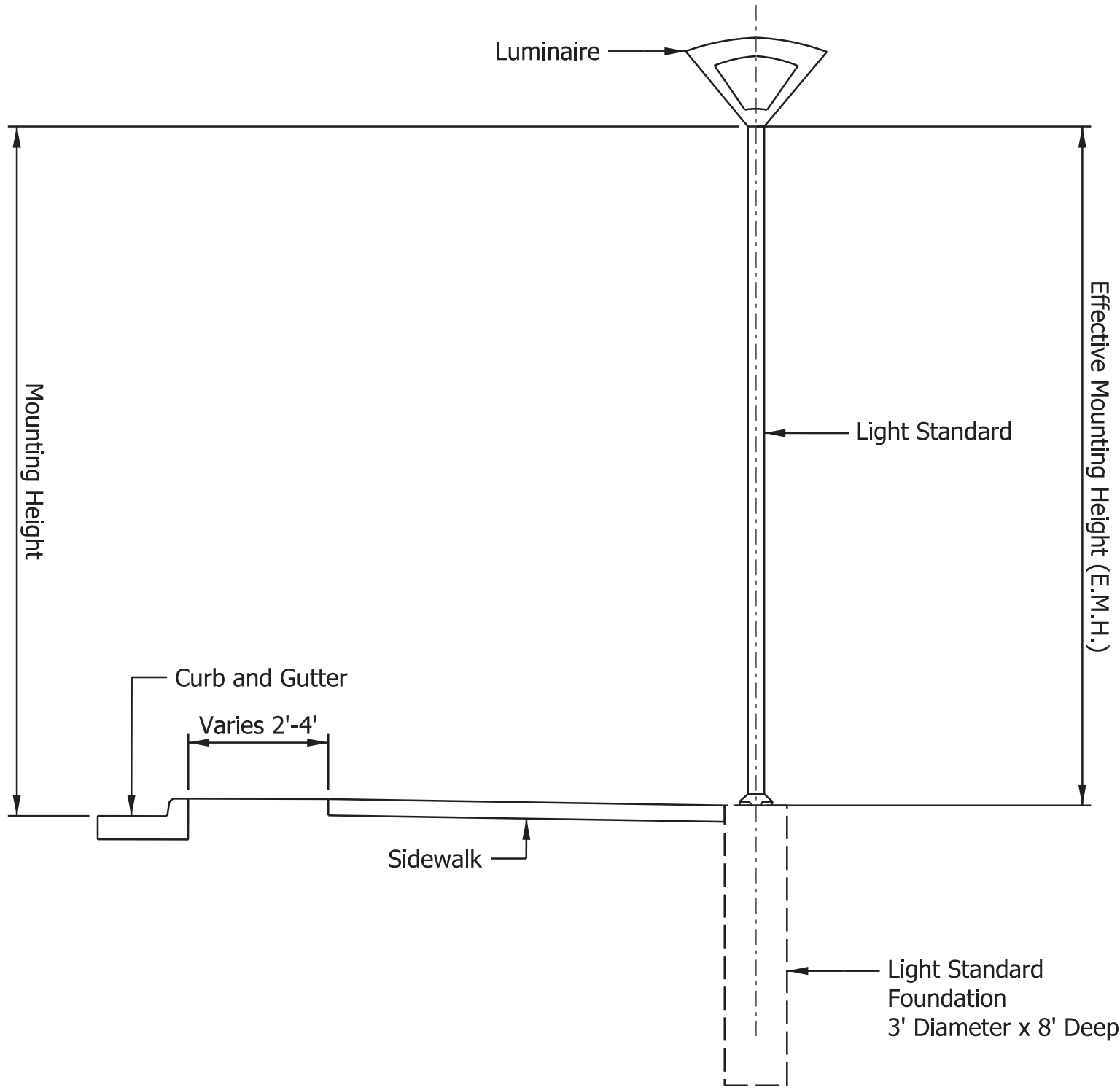
RECOMMENDED FOR APPROVAL _____	
DESIGN ENGINEER _____	DATE _____
DESIGNED: _____ MAH	DRAWN: _____ MAH
CHECKED: _____ -	CHECKED: _____ -

INDIANA DEPARTMENT OF TRANSPORTATION	
LIGHTING PLAN MILLER STREET	

HORIZONTAL SCALE		BRIDGE FILE	
1" = 20"		N/A	
VERTICAL SCALE		DESIGNATION	
N/A		1702775	
SURVEY BOOK		SHEETS	
		2 of 4	
CONTRACT		PROJECT	



Model: Lighting Tables  
4/8/2021 11:50:15 AM User: M.Pagan



TYPICAL LIGHT-POLE INSTALLATION - ROAD  
Not to Scale

SERVICE POINT AMP TABLE				
SERVICE POINT	MAIN BREAKER	BRANCH CIRCUIT	COMPUTED BRANCH CIRCUIT AMPERAGE	BRANCH CIRCUIT BREAKER
A	100 AMP	A-1	1.02 AMP	2-30 AMP
			0.68 AMP	
		A-2	0.34 AMP	2-30 AMP
			0.34 AMP	
B	100 AMP	B-1	0.68 AMP	2-30 AMP
			1.02 AMP	
		B-2	0.34 AMP	2-30 AMP
			0.34 AMP	
		B-3	0.34 AMP	2-30 AMP
			0.68 AMP	

DESIGN DATA		
Initial Lamp Lumen (LL) (Conventional)	10,045	lumens per luminaire
Average Maintained Illumination (EL)	1.0	fc
Lamp Lumen Depreciation Factor (LLD)	0.88	
Luminaire Dirt Depreciation Factor (LDD)	0.87	
Maintained Factor (LLD X LDD)	76%	
Uniformity Ratio	4:1	
Mounting Height (Conventional)	24	ft

RECOMMENDED FOR APPROVAL _____		DESIGN ENGINEER _____ DATE _____	
DESIGNED: _____ MAH		DRAWN: _____ MAH	
CHECKED: _____ -		CHECKED: _____ -	

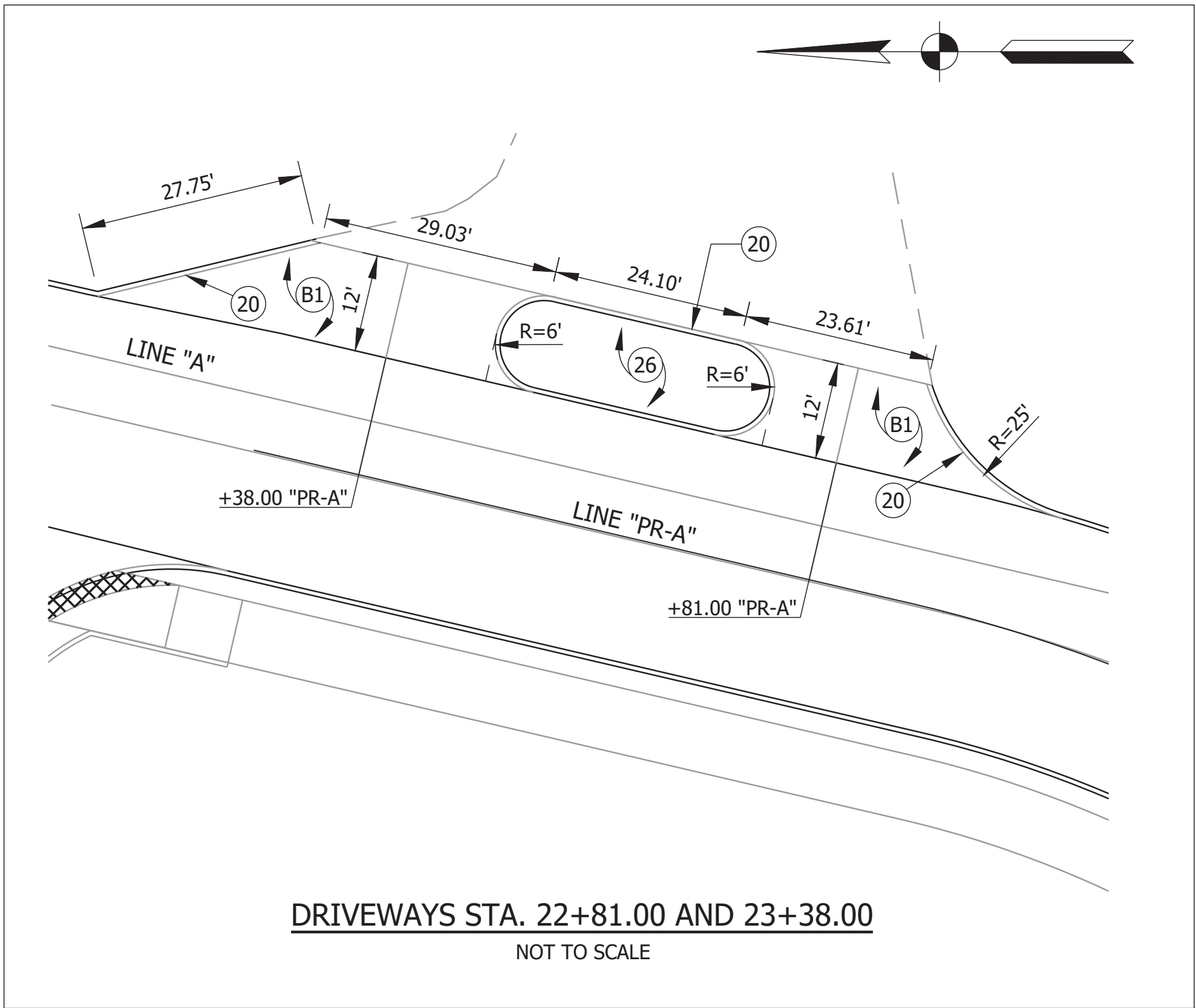
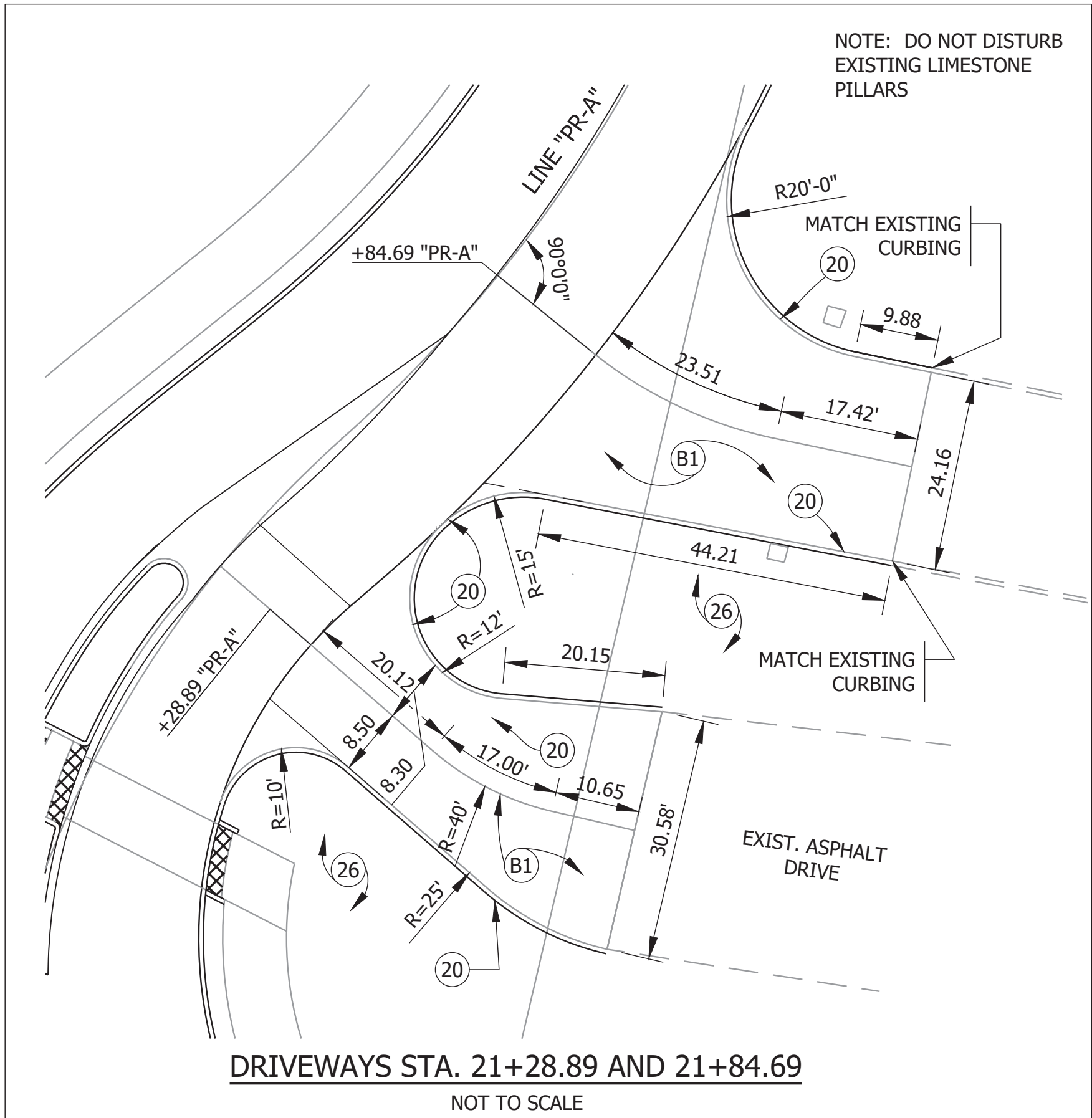
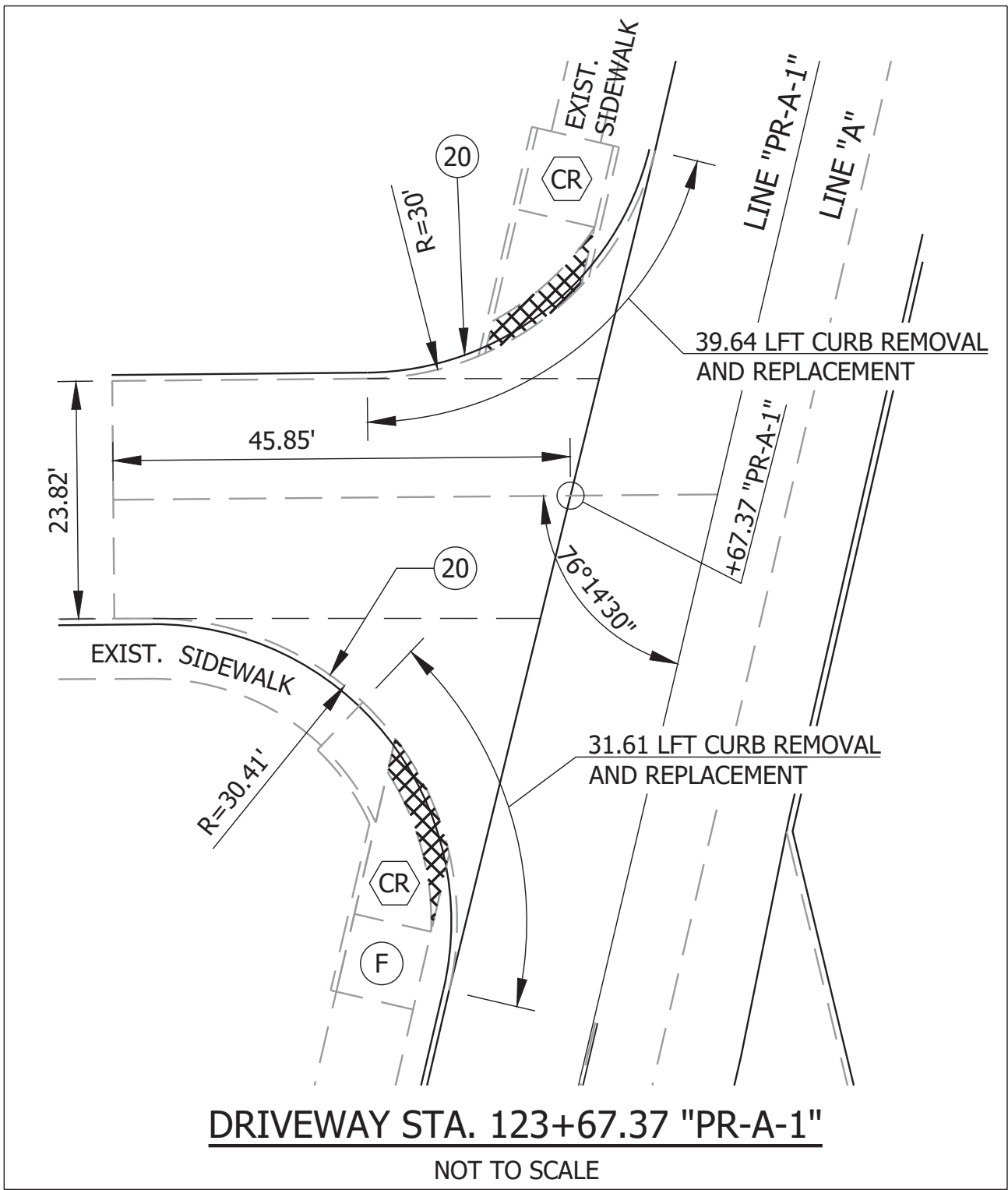
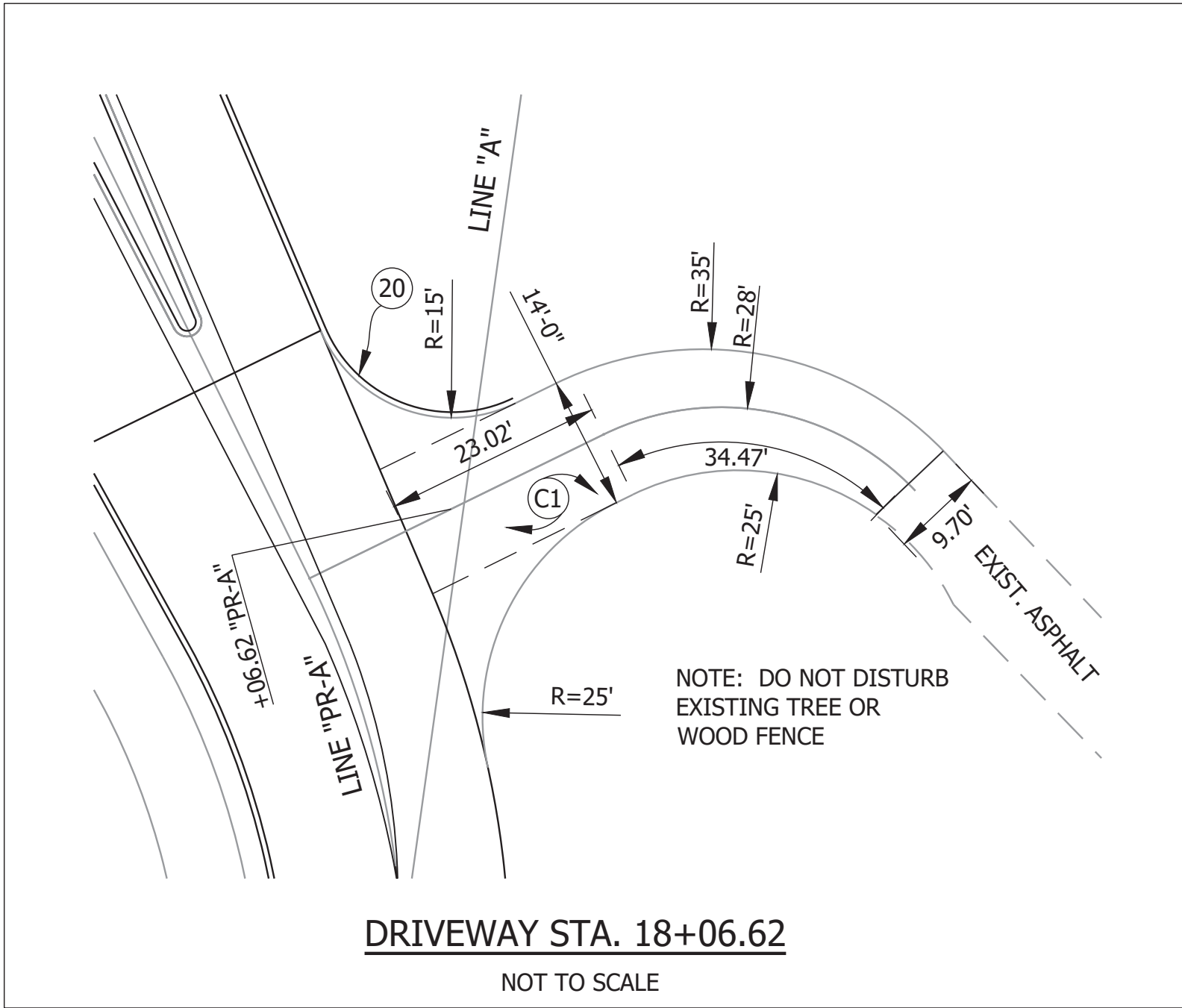
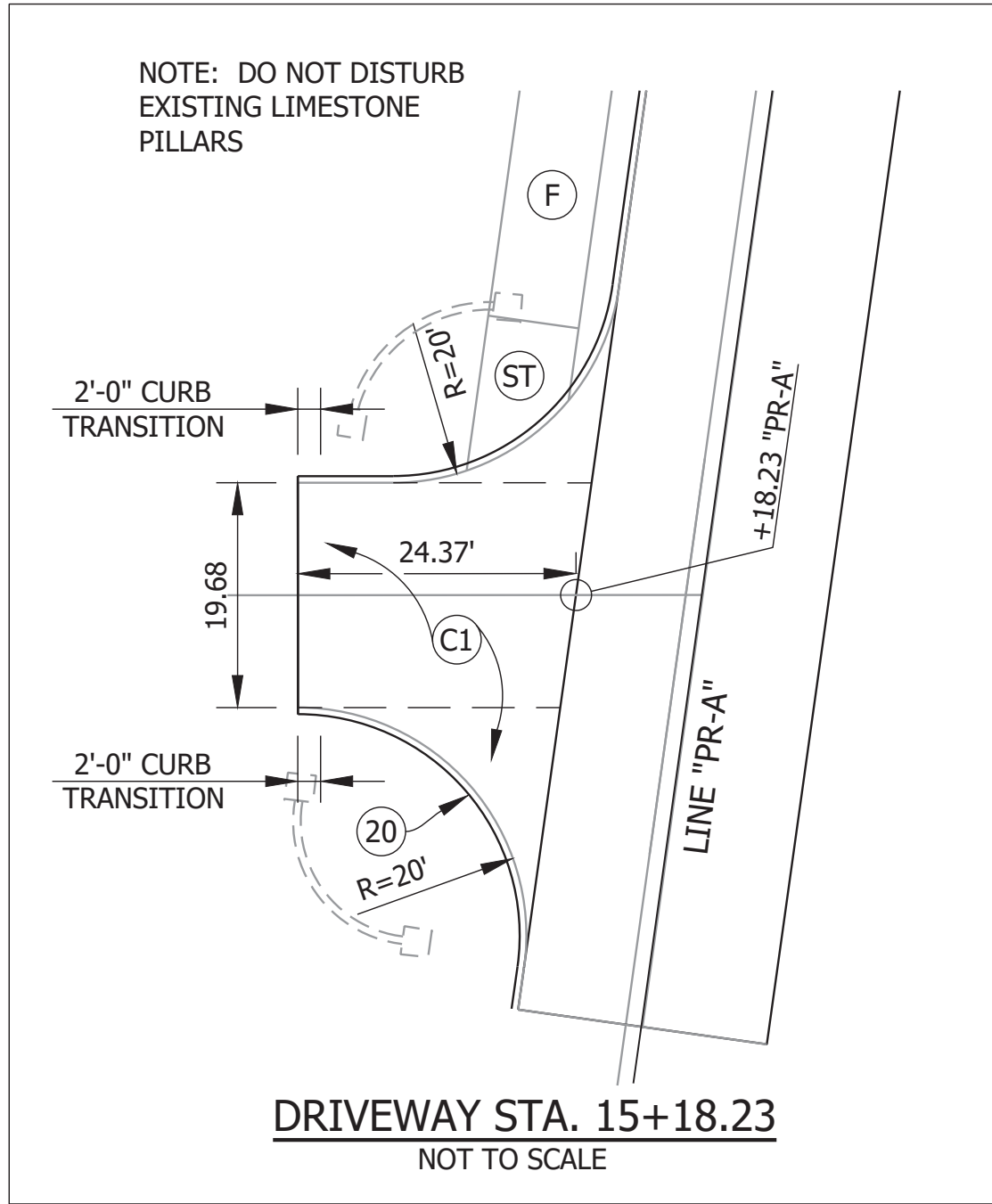
INDIANA DEPARTMENT OF TRANSPORTATION	
TABLES	

HORIZONTAL SCALE		BRIDGE FILE	
AS NOTED		N/A	
VERTICAL SCALE		DESIGNATION	
AS NOTED		1702775	
SURVEY BOOK		SHEETS	
		3	of 4
CONTRACT		PROJECT	









#### LEGEND

- (B1) PCCP FOR APPROACHES, 6" ON DENSE GRADED SUBBASE, 6" ON SUBGRADE TREATMENT TYPE 3
- (C1) HMA FOR APPROACHES, TYPE B, CONSISTING OF 165 LBS SYD HMA SURFACE, TYPE B, ON 275 LBS SYD HMA, INTERMEDIATE, TYPE B, ON 6" COMPACTED AGGREGATE, NO 53 BASE, ON SUBGRADE TREATMENT TYPE 3
- (F) HMA FOR SIDEWALKS, CONSISTING OF 140 LBS/SYS HMA, SURFACE, TYPE B ON 220 LBS/SYS HMA, INTERMEDIATE, TYPE B ON 6" COMPACTED AGGREGATE, NO. 53 BASE ON SUBGRADE TREATMENT, TYPE 2
- (ST) SIDEWALK TRANSITION
- (20) CONCRETE CURB
- (26) SODDING
- DETECTABLE WARNING SURFACE
- (CR) CURB RAMP

File Name: F:\4561-Shelbyville Roundabout\50 Plans\30 Sheet Drawings\20 Road Sheets\R-CONSTR-DTLS-01.dwg - MILLER DRIVES  
Modified / By: July 6, 2021 3:24:21 PM / sjohnson  
Plotted / By: July 6, 2021 3:36:57 PM / Stacey Johnson

NOTES: ALL STATION/OFFSET CALLOUTS ARE CALLED OUT FROM LINE "PR-B" UNLESS OTHERWISE NOTED

PRELIMINARY

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: AJD	DRAWN: SEJ	
CHECKED: ZRE	CHECKED: ZRE	

INDIANA  
DEPARTMENT OF TRANSPORTATION

CONSTRUCTION DETAILS  
SOUTH MILLER STREET DRIVES

HORIZONTAL SCALE	BRIDGE FILE
1" = 30' UNLESS NOTED	N/A
VERTICAL SCALE	DESIGNATION
N/A	1702775
	SHEET
	21 of 67
CONTRACT	PROJECT
R-41302	1702775

# **APPENDIX C**

## **Early Coordination**



# INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue  
Room N642  
Indianapolis, Indiana 46204

PHONE: (317) 233-6795

**Eric J. Holcomb, Governor**  
**Joe McGuinness, Commissioner**

February 26, 2020

{See Attached List}

## Sample Early Coordination Letter

Re: Intersection improvement Project  
Miller Street and McKay Road  
Designation Number 1702775  
Addison Township, City of Shelbyville, Shelby County, Indiana

Dear Agency:

The City of Shelbyville with oversight from the Indiana Department of Transportation (INDOT) propose to utilize federal funds to partially fund and proceed with the referenced project. This letter is part of the early coordination phase of the environmental review process. We request comments from your area of expertise regarding any possible environmental effects associated with this project. **Please use the Des. No. and description provided above in your reply.** We will incorporate your comments into a study of the project environmental effects.

The proposed project is located at the intersection of Miller Street and McKay Road. Specifically, the project is located in Section 7, Township 12 North, Range 7 East as illustrated on the *Shelbyville, Indiana* 7.5-minute United States Geological Survey (USGS) topographic quadrangle.

Miller Street is a north/south roadway classified as a Minor Arterial with a posted speed limit of 30 miles per hour. The posted speed limit is reduced to 20 miles per hour within the designated school zone. The cross-section of the north leg consists of one 15ft. travel lane in each direction, bordered by integral concrete curbs and an enclosed storm drainage system. A sidewalk is located along the west side of the north leg. The south leg consists of 11ft. travel lanes in each direction bordered by earthen shoulders with no curb or sidewalks. There are no dedicated turn lanes for any direction of travel on either leg of Miller Street.

McKay Road is an east/west roadway classified as a Minor Arterial with a posted speed limit of 30 miles per hour. The posted speed limit is reduced to 20 miles per hour within the designated school zone. The roadway cross-section consists of one 15ft. travel lane in each direction, bordered by integral concrete curbs and no shoulders. The pavement width is 30ft. from curb face to curb face. Sidewalks are located along the north side of the road in the northwest quadrant and along the south side within the southeast quadrant of the intersection. There are no dedicated turn lanes for any direction of travel on either leg of McKay Road. The intersection of Miller Street and McKay Road is a four-way stop, controlled by signage. There is no traffic signal.

The surrounding land is primarily developed with residential dwellings. The Shelbyville High School and Middle School campus is present in the northwest quadrant of the intersection. The northeast quadrant of the intersection consists of single-family residential parcels and the Heritage House assisted living facility is present in the southwest quadrant. The southeast quadrant is occupied with a large, single-family dwelling.

The primary purpose of the project is to provide an improved intersection that meets current and projected traffic capacity and provides upgraded pedestrian facilities. The project is needed due to the current and projected traffic volumes, and lack of adequate pedestrian facilities.

Traffic congestion is a concern during the peak travel times in the morning and afternoons due to the proximity of the Shelbyville school facilities located immediately west of the intersection. The high volume of school buses, student and staff traffic combined with local through traffic all utilizing the intersection concurrently, results in congestion and delays during peak travel times during the morning and late afternoons.



An additional need for the project is a result of the large number of students who walk to school and cross the intersection to access the school campus. Pedestrian safety is a primary concern because there are two quadrants of the intersection that lack sidewalks and cross walks entirely.

A secondary safety issue effects the vehicles traveling westbound from SR 9 to SR 44 via McKay Road. The east leg of McKay Road has a substantial grade of 6+% entering the intersection. During inclement weather conditions, the steep grade creates difficult conditions for westbound vehicles stopping and restarting on the grade.

The preferred alternate will upgrade the existing intersection to a single-lane roundabout. Additional items to be constructed as a part of the project include new sidewalks and cross walks, an enclosed drainage system with new storm inlets, curbs and gutter. All sidewalks and ramp systems will be constructed in compliance with ADA (Americans with Disabilities Act) design parameters.

Additional permanent and temporary right-of-way will be required to construct the project. As the project design advances, the exact amounts of right-of-way will be defined. The maintenance of traffic plan will close McKay Road and detour traffic while constructing the intersection proper and connecting the new approaches into McKay Road. The approaches connecting South Miller Street will be constructed near the existing roadway during this phase; however, the connections to the existing road will occur in Phase 2 of construction and will result in some partial closures and lane restrictions.

The construction letting date is scheduled for October 2021. Construction is anticipated to begin in early 2022. The construction schedule will be coordinated with the school system to reduce disruptions to school traffic and bus routes.

Based on the scope of work, this project will require a full Section 106 evaluation for above ground historic resources and a Phase I archeological survey. All reports will be reviewed and approved by INDOT-Cultural Resources Office prior to submittal to the State Historic Preservation Officer. The project area will also be surveyed for water resources such as streams and wetlands.

Please review the information herein and provide a written evaluation of any potential impacts upon resources under your jurisdiction. Please reply within 30 days of receipt of this letter or the environmental document will indicate that your agency has no comment. Your cooperation in expediting the development of this project is appreciated.

If you have any questions, you may contact Ms. Elayna Stoner, Project Manager, at 317.779.3506 or elaynas@MetricEnv.com, or write to her at 6971 Hillsdale Court, Indianapolis, Indiana 46250.

On behalf of the City of Shelbyville, we appreciate your interest in this project.

*Elayna Stoner*  
Elayna Stoner  
Metric Environmental, LLC  
Project Manager

cc: File No. 18-0147  
Mr. Alan Delaunay, Zimmer Construction

Attachments: Project Location Map  
USGS Topographic Map  
2013 Aerial Photograph  
Ground Level Photographs

**Project Maps and Attachments Provided with this Letter are Located in Appendix B of this Document**



**Early Coordination Recipients  
Des. No. 1702775  
Intersection improvement Project  
Miller Street and McKay Road, City of Shelbyville  
Addison Township, Shelby County, Indiana**

Federal Highway Administration  
[Robert.Dirks@dot.in.gov](mailto:Robert.Dirks@dot.in.gov)

U.S. Department of Housing and Urban  
Development  
[Paul.J.Lehmann@hud.gov](mailto:Paul.J.Lehmann@hud.gov)

State Conservationist  
Natural Resources Conservation Service  
[rick.neilson@in.usda.gov](mailto:rick.neilson@in.usda.gov)

US Fish and Wildlife Service  
[robin\\_mcwiliams@fws.gov](mailto:robin_mcwiliams@fws.gov)

Regional Environmental Coordinator  
Midwest Regional Office, National Park Service  
601 Riverfront Drive  
Omaha, Nebraska 68102

Indiana Department of Transportation  
Greenfield District Office  
[JCurry1@indot.in.gov](mailto:JCurry1@indot.in.gov)

Indiana Department of Transportation  
Office of Public Involvement  
[rclark@indot.in.gov](mailto:rclark@indot.in.gov)

Indiana Department of Transportation  
Office of Aviation  
[JCourtade@indot.in.gov](mailto:JCourtade@indot.in.gov)

Southeastern Indiana Regional Planning  
Commission  
[susan.craig@sirpc.org](mailto:susan.craig@sirpc.org)

Indiana Geological Survey  
<https://igs.indiana.edu/eAssessment>

Indiana Department of Natural Resources  
Division of Fish and Wildlife  
[environmentalreview@dnr.in.gov](mailto:environmentalreview@dnr.in.gov)

IDEM Proposed Roadway Construction Projects  
Electronic Coordination  
<http://www.in.gov/idem/5284.htm>

IDEM Wellhead Proximity Determinator  
[www.in.gov/idem/cleanwater/pages/wellhead](http://www.in.gov/idem/cleanwater/pages/wellhead)

Shelbyville Fire Department  
Chief, Tony Logan  
[tlogan@cityofshelbyvillein.com](mailto:tlogan@cityofshelbyvillein.com)  
Fire Station 3  
1350 W. McKay Road  
Shelbyville, IN 46176

Shelby County Commissioners  
[Kevin.nigh@co.shelby.in.us](mailto:Kevin.nigh@co.shelby.in.us)  
[Don.parker@co.shelby.in.us](mailto:Don.parker@co.shelby.in.us)  
[Chris.ross@co.shelby.in.us](mailto:Chris.ross@co.shelby.in.us)

Honorable Tom DeBaun  
Mayor, City of Shelbyville  
[mayor@cityofshelbyvillein.com](mailto:mayor@cityofshelbyvillein.com)

Shelbyville Police Department  
Chief of Police  
[mweidner@cityofshelbyvillein.com](mailto:mweidner@cityofshelbyvillein.com)

Shelbyville MS4 Coordinator  
[dbyers@cityofshelbyvillein.com](mailto:dbyers@cityofshelbyvillein.com)

Shelbyville Central Schools  
803 St Joseph Street  
Shelbyville, IN 46176

Shelbyville High School  
2003 South Miller Street  
Shelbyville, IN 46176

City of Shelbyville Public Utility Office  
44 West Washington Street  
Shelbyville, IN 46176

Shelbyville Parks and Recreation  
945 S. Tompkins Street  
Shelbyville, IN 46176

**State of Indiana**  
**DEPARTMENT OF NATURAL RESOURCES**  
**Division of Fish and Wildlife**  
**Early Coordination/Environmental Assessment**

---

**DNR #:** ER-22268

**Request Received:** February 26, 2020

**Requestor:** Metric Environmental  
Elayna Stoner  
6971 Hillside Court  
Indianapolis, IN 46250

**Project:** Miller Street and McKay Road intersection roundabout construction, City of Shelbyville;  
Des #1702775

**County/Site info:** Shelby

The Indiana Department of Natural Resources has reviewed the above referenced project per your request. Our agency offers the following comments for your information and in accordance with the National Environmental Policy Act of 1969.

If our agency has regulatory jurisdiction over the project, the recommendations contained in this letter may become requirements of any permit issued. If we do not have permitting authority, all recommendations are voluntary.

**Regulatory Assessment:** This proposal may require the formal approval of our agency pursuant to the Flood Control Act (IC 14-28-1) for any proposal to construct, excavate, or fill in or on the floodway of Fortune Ditch. Please submit more detailed plans to the Division of Water's Technical Services Section if you are unsure whether or not a permit will be required.

**Natural Heritage Database:** The Natural Heritage Program's data have been checked. To date, no plant or animal species listed as state or federally threatened, endangered, or rare have been reported to occur in the project vicinity.

**Fish & Wildlife Comments:** The measures below should be implemented to avoid, minimize, or compensate for impacts to fish, wildlife, and botanical resources:

1. Revegetate all bare and disturbed areas with a mixture of grasses (excluding all varieties of tall fescue) and legumes as soon as possible upon completion; low endophyte tall fescue may be used in the ditch bottom and side slopes only.
2. Minimize and contain within the project limits all tree and brush clearing.
3. Do not cut any trees suitable for Indiana bat or Northern Long-eared bat roosting (greater than 5 inches dbh, living or dead, with loose hanging bark, or with cracks, crevices, or cavities) from April 1 through September 30.
4. Appropriately designed measures for controlling erosion and sediment must be implemented to prevent sediment from entering the stream or leaving the construction site; maintain these measures until construction is complete and all disturbed areas are stabilized.
5. Plant five trees, at least 2 inches in diameter-at-breast height, for each tree which is removed that is ten inches or greater in diameter-at-breast height.

**Contact Staff:** Christie L. Stanifer, Environ. Coordinator, Fish & Wildlife  
Our agency appreciates this opportunity to be of service. Please contact the above staff member at (317) 232-4080 if we can be of further assistance.

*Christie L. Stanifer*

Christie L. Stanifer  
Environ. Coordinator  
Division of Fish and Wildlife

**Date:** March 26, 2020





## United States Department of the Interior

### FISH AND WILDLIFE SERVICE

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

Phone: (812) 334-4261 Fax: (812) 334-4273

<http://www.fws.gov/midwest/Endangered/section7/s7process/step1.html>



In Reply Refer To:

February 03, 2021

Consultation code: 03E12000-2021-I-0678

Event Code: 03E12000-2021-E-03298

Project Name: Intersection Improvement Project (Des No. 1702775)

Subject: Concurrence verification letter for the 'Intersection Improvement Project (Des No. 1702775)' project under the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

To whom it may concern:

The U.S. Fish and Wildlife Service (Service) has received your request to verify that the **Intersection Improvement Project (Des No. 1702775)** (Proposed Action) may rely on the concurrence provided in the February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat (PBO) to satisfy requirements under Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended; 16 U.S.C 1531 *et seq.*).

Based on the information you provided (Project Description shown below), you have determined that the Proposed Action is within the scope and adheres to the criteria of the PBO, including the adoption of applicable avoidance and minimization measures, and may affect, but is not likely to adversely affect (NLAA) the endangered Indiana bat (*Myotis sodalis*) and/or the threatened Northern long-eared bat (*Myotis septentrionalis*).

The Service has 14 calendar days to notify the lead Federal action agency or designated non-federal representative if we determine that the Proposed Action does not meet the criteria for a NLAA determination under the PBO. If we do not notify the lead Federal action agency or designated non-federal representative within that timeframe, you may proceed with the Proposed Action under the terms of the NLAA concurrence provided in the PBO. This verification period allows Service Field Offices to apply local knowledge to implementation of the PBO, as we may identify a small subset of actions having impacts that were unanticipated. In such instances, Service Field Offices may request additional information that is necessary to verify inclusion of the proposed action under the PBO.

**For Proposed Actions that include bridge/structure removal, replacement, and/or maintenance activities:** If your initial bridge/structure assessments failed to detect Indiana bats, but you later detect bats during construction, please submit the Post Assessment Discovery of Bats at Bridge/Structure Form (User Guide Appendix E) to this Service Office. In these instances, potential incidental take of Indiana bats may be exempted provided that the take is reported to the Service.

If the Proposed Action is modified, or new information reveals that it may affect the Indiana bat and/or Northern long-eared bat in a manner or to an extent not considered in the PBO, further review to conclude the requirements of ESA Section 7(a)(2) may be required. If the Proposed Action may affect any other federally-listed or proposed species, and/or any designated critical habitat, additional consultation between the lead Federal action agency and this Service Office is required. If the proposed action has the potential to take bald or golden eagles, additional coordination with the Service under the Bald and Golden Eagle Protection Act may also be required. In either of these circumstances, please contact this Service Office.

## **Project Description**

The following project name and description was collected in IPaC as part of the endangered species review process.

### ***Name***

Intersection Improvement Project (Des No. 1702775)

### ***Description***

The City of Shelbyville, with partial funding from Federal Highway Administration (FHWA) and oversight from the Indiana Department of Transportation (INDOT), intends to proceed with an intersection improvement project at Miller Street and McKay Avenue in Shelby County, Indiana.

The proposed project is located at the intersection of Miller Street and McKay Road. Specifically, the project is located in Section 7, Township 12 North, Range 7 East as illustrated on the Shelbyville, Indiana 7.5-minute United States Geological Survey (USGS) topographic quadrangle

The surrounding land is primarily developed with residential dwellings. The Shelbyville High School and Middle School campus is present in the northwest quadrant of the intersection. The northeast quadrant of the intersection consists of single-family residential parcels and the Heritage House assisted living facility is present in the southwest quadrant. The southeast quadrant is occupied with a large, single-family dwelling. There is suitable summer habitat near the project area.

There will be tree removal required to complete the proposed improvement. Approximately 3 to 4 ornamental red maple trees will be removed in the northwest quadrant of the intersection. The trees will be removed under a separate contract. The tree clearing will take place between November 2022 and March 2023. Construction of the project is anticipated to begin in early 2023 and last approximately one year.

The preferred alternate will upgrade the existing intersection to a single-lane roundabout. Additional items to be constructed as a part of the project include new sidewalks and cross walks, an enclosed drainage system with new storm inlets, curbs and gutter. A multi-use path will be constructed within the southeast quadrant of the new roundabout.

New, additional permanent and temporary right-of-way will be required to construct the project. At this time, it is estimated that approximately 1.72 acres of additional, permanent right-of-way will be required to construct the project. Approximately 0.10 acre of temporary right-of-way will be necessary for construction access and drive re-grading.

New, additional permanent right-of-way will be necessary to complete the project and new, permanent lighting will be installed along each leg of the roundabout for approximately 300 to 400 feet. Temporary lighting may be necessary during construction. This will be determined via the contractor's means and methods.

On June 9, 2020, the INDOT Greenfield District reviewed the U.S. Fish and Wildlife Service database and no endangered bat species have been documented in or within 0.5 mile of the project area.

## Determination Key Result

Based on your answers provided, this project(s) may affect, but is not likely to adversely affect the endangered Indiana bat and/or the threatened Northern long-eared bat, therefore, consultation with the U.S. Fish and Wildlife Service pursuant to Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended 16 U.S.C. 1531 *et seq.*) is required. However, also based on your answers provided, this project may rely on the concurrence provided in the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

## Qualification Interview

1. Is the project within the range of the Indiana bat<sup>[1]</sup>?

[1] See [Indiana bat species profile](#)

**Automatically answered**

Yes

2. Is the project within the range of the Northern long-eared bat<sup>[1]</sup>?

[1] See [Northern long-eared bat species profile](#)

**Automatically answered**

Yes

3. Which Federal Agency is the lead for the action?

*A) Federal Highway Administration (FHWA)*

4. Are *all* project activities limited to non-construction<sup>[1]</sup> activities only? (examples of non-construction activities include: bridge/abandoned structure assessments, surveys, planning and technical studies, property inspections, and property sales)

[1] Construction refers to activities involving ground disturbance, percussive noise, and/or lighting.

No

5. Does the project include *any* activities that are **greater than** 300 feet from existing road/rail surfaces<sup>[1]</sup>?

[1] Road surface is defined as the actively used [e.g. motorized vehicles] driving surface and shoulders [may be pavement, gravel, etc.] and rail surface is defined as the edge of the actively used rail ballast.

No

6. Does the project include *any* activities **within** 0.5 miles of a known Indiana bat and/or NLEB hibernaculum<sup>[1]</sup>?

[1] For the purpose of this consultation, a hibernaculum is a site, most often a cave or mine, where bats hibernate during the winter (see suitable habitat), but could also include bridges and structures if bats are found to be hibernating there during the winter.

No

7. Is the project located **within** a karst area?

No

8. Is there *any* suitable<sup>[1]</sup> summer habitat for Indiana Bat or NLEB **within** the project action area<sup>[2]</sup>? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] The action area is defined as all areas to be affected directly or indirectly by the Federal action and not merely the immediate area involved in the action (50 CFR Section 402.02). Further clarification is provided by the [national consultation FAQs](#).

Yes

9. Will the project remove *any* suitable summer habitat<sup>[1]</sup> and/or remove/trim any existing trees **within** suitable summer habitat?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

Yes

10. Will the project clear more than 20 acres of suitable habitat per 5-mile section of road/rail?

No

11. Have presence/probable absence (P/A) summer surveys<sup>[1][2]</sup> been conducted<sup>[3][4]</sup> **within** the suitable habitat located within your project action area?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] Presence/probable absence summer surveys conducted within the fall swarming/spring emergence home range of a documented Indiana bat hibernaculum (contact local Service Field Office for appropriate distance from hibernacula) that result in a negative finding requires additional consultation with the local Service Field Office to determine if clearing of forested habitat is appropriate and/or if seasonal clearing restrictions are needed to avoid and minimize potential adverse effects on fall swarming and spring emerging Indiana bats.

[3] For projects within the range of either the Indiana bat or NLEB in which suitable habitat is present, and no bat surveys have been conducted, the transportation agency will assume presence of the appropriate species. This assumption of presence should be based upon the presence of suitable habitat and the capability of bats to occupy it because of their mobility.

[4] Negative presence/probable absence survey results obtained using the [summer survey guidance](#) are valid for a minimum of two years from the completion of the survey unless new information (e.g., other nearby surveys) suggest otherwise.

No

12. Does the project include activities **within documented Indiana bat habitat**<sup>[1][2]</sup>?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry biangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

*No*

13. Will the removal or trimming of habitat or trees occur **within** suitable but **undocumented Indiana bat** roosting/foraging habitat or travel corridors?

*Yes*

14. What time of year will the removal or trimming of habitat or trees **within** suitable but **undocumented Indiana bat** roosting/foraging habitat or travel corridors occur<sup>[1]</sup>?

[1] Coordinate with the local Service Field Office for appropriate dates.

*B) During the inactive season*

15. Does the project include activities **within documented NLEB habitat**<sup>[1][2]</sup>?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry biangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

*No*

16. Will the removal or trimming of habitat or trees occur **within** suitable but **undocumented NLEB** roosting/foraging habitat or travel corridors?

*Yes*

17. What time of year will the removal or trimming of habitat or trees **within** suitable but **undocumented NLEB** roosting/foraging habitat or travel corridors occur?

*B) During the inactive season*

18. Will *any* tree trimming or removal occur **within** 100 feet of existing road/rail surfaces?

*Yes*

19. Will *any* tree trimming or removal occur **between** 100-300 feet of existing road/rail surfaces?

*No*

20. Are *all* trees that are being removed clearly demarcated?  
Yes
21. Will the removal of habitat or the removal/trimming of trees include installing new or replacing existing **permanent** lighting?  
Yes
22. Does the project include wetland or stream protection activities associated with compensatory wetland mitigation?  
No
23. Does the project include slash pile burning?  
No
24. Does the project include *any* bridge removal, replacement, and/or maintenance activities (e.g., any bridge repair, retrofit, maintenance, and/or rehabilitation work)?  
No
25. Does the project include the removal, replacement, and/or maintenance of *any* structure other than a bridge? (e.g., rest areas, offices, sheds, outbuildings, barns, parking garages, etc.)  
No
26. Will the project involve the use of **temporary** lighting *during* the active season?  
Yes
27. Is there *any* suitable habitat **within** 1,000 feet of the location(s) where **temporary** lighting will be used?  
Yes
28. Will the project install *any* new or replace any existing **permanent** lighting in addition to the lighting already indicated for habitat removal (including the removal or trimming of trees) or bridge/structure removal, replacement or maintenance activities?  
Yes
29. Is there *any* suitable habitat **within** 1,000 feet of the location(s) where **permanent** lighting (other than the lighting already indicated for habitat removal (including the removal or trimming of trees) or bridge/structure removal, replacement or maintenance activities) will be installed or replaced?  
Yes
30. Does the project include percussives or other activities (**not including tree removal/trimming or bridge/structure work**) that will increase noise levels above existing traffic/background levels?  
No



31. Are *all* project activities that are **not associated with** habitat removal, tree removal/trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives, limited to actions that DO NOT cause any additional stressors to the bat species?

Examples: lining roadways, unlighted signage , rail road crossing signals, signal lighting, and minor road repair such as asphalt fill of potholes, etc.

Yes

32. Will the project raise the road profile **above the tree canopy**?

No

33. Are the project activities that are not associated with habitat removal, tree removal/trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives consistent with a No Effect determination in this key?

**Automatically answered**

*Yes, other project activities are limited to actions that DO NOT cause any additional stressors to the bat species as described in the BA/BO*

34. Is the habitat removal portion of this project consistent with a Not Likely to Adversely Affect determination in this key?

**Automatically answered**

*Yes, because the tree removal/trimming that occurs outside of the Indiana bat's active season occurs greater than 0.5 miles from the nearest hibernaculum, is less than 100 feet from the existing road/rail surface, includes clear demarcation of the trees that are to be removed, and does not alter documented roosts and/or surrounding summer habitat within 0.25 miles of a documented roost.*

35. Is the habitat removal portion of this project consistent with a Not Likely to Adversely Affect determination in this key?

**Automatically answered**

*Yes, because the tree removal/trimming that occurs outside of the NLEB's active season occurs greater than 0.5 miles from the nearest hibernaculum, is less than 100 feet from the existing road/rail surface, includes clear demarcation of the trees that are to be removed, and does not alter documented roosts and/or surrounding summer habitat within 0.25 miles of a documented roost.*

36. **General AMM 1**

Will the project ensure *all* operators, employees, and contractors working in areas of known or presumed bat habitat are aware of *all* FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable Avoidance and Minimization Measures?

Yes

**37. Tree Removal AMM 1**

Can *all* phases/aspects of the project (e.g., temporary work areas, alignments) be modified, to the extent practicable, to avoid tree removal<sup>[1]</sup> in excess of what is required to implement the project safely?

Note: Tree Removal AMM 1 is a minimization measure, the full implementation of which may not always be practicable. Projects may still be NLAA as long as Tree Removal AMMs 2, 3, and 4 are implemented and LAA as long as Tree Removal AMMs 3, 5, 6, and 7 are implemented.

[1] The word “trees” as used in the AMMs refers to trees that are suitable habitat for each species within their range. See the USFWS’ current summer survey guidance for our latest definitions of suitable habitat.

Yes

**38. Tree Removal AMM 3**

Can tree removal be limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits)?

Yes

**39. Tree Removal AMM 4**

Can the project avoid cutting down/removal of *all* (1) **documented**<sup>[1]</sup> Indiana bat or NLEB roosts<sup>[2]</sup> (that are still suitable for roosting), (2) trees **within** 0.25 miles of roosts, and (3) documented foraging habitat any time of year?

[1] The word documented means habitat where bats have actually been captured and/or tracked.

[2] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry triangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

Yes

**40. Lighting AMM 2**

Does the lead agency use the BUG (Backlight, Uplight, and Glare) system developed by the Illuminating Engineering Society<sup>[1][2]</sup> to rate the amount of light emitted in unwanted directions?

[1] Refer to [Fundamentals of Lighting - BUG Ratings](#)

[2] Refer to [The BUG System—A New Way To Control Stray Light](#)

Yes

**41. Lighting AMM 2**

Will the **permanent** lighting used during removal of suitable habitat and/or the removal/trimming of trees within suitable habitat be designed to be as close to 0 for all three BUG ratings as possible, with a priority of "uplight" of 0 and "backlight" as low as practicable?

Yes

**42. Lighting AMM 1**

Will *all* **temporary** lighting be directed away from suitable habitat during the active season?

Yes

**43. Lighting AMM 2**

Does the lead agency use the BUG (Backlight, Uplight, and Glare) system developed by the Illuminating Engineering Society<sup>[1][2]</sup> to rate the amount of light emitted in unwanted directions?

[1] Refer to [Fundamentals of Lighting - BUG Ratings](#)

[2] Refer to [The BUG System—A New Way To Control Stray Light](#)

Yes

**44. Lighting AMM 2**

Will the **permanent** lighting (other than any lighting already indicated for tree clearing or bridge/structure removal, replacement or maintenance activities) be designed to be as close to 0 for all three BUG ratings as possible, with a priority of "uplight" of 0 and "backlight" as low as practicable?

Yes

## Project Questionnaire

1. Have you made a No Effect determination for *all* other species indicated on the FWS IPaC generated species list?

N/A

2. Have you made a May Affect determination for *any* other species on the FWS IPaC generated species list?

N/A

3. How many acres<sup>[1]</sup> of trees are proposed for removal between 0-100 feet of the existing road/rail surface?

[1] If described as number of trees, multiply by 0.09 to convert to acreage and enter that number.

0.36

## Avoidance And Minimization Measures (AMMs)

This determination key result includes the commitment to implement the following Avoidance and Minimization Measures (AMMs):

**TREE REMOVAL AMM 1**

Modify all phases/aspects of the project (e.g., temporary work areas, alignments) to avoid tree removal.

**LIGHTING AMM 1**

Direct temporary lighting away from suitable habitat during the active season.

**TREE REMOVAL AMM 2**

Apply time of year restrictions for tree removal when bats are not likely to be present, or limit tree removal to 10 or fewer trees per project at any time of year within 100 feet of existing road/rail surface and **outside of documented** roosting/foraging habitat or travel corridors; visual emergence survey must be conducted with no bats observed.

**LIGHTING AMM 2**

When installing new or replacing existing permanent lights, use downward-facing, full cut-off lens lights (with same intensity or less for replacement lighting); or for those transportation agencies using the BUG system developed by the Illuminating Engineering Society, be as close to 0 for all three ratings with a priority of "uplight" of 0 and "backlight" as low as practicable.

**TREE REMOVAL AMM 3**

Ensure tree removal is limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits).

**TREE REMOVAL AMM 4**

Do not remove **documented** Indiana bat or NLEB roosts that are still suitable for roosting, or trees within 0.25 miles of roosts, or **documented** foraging habitat any time of year.

**GENERAL AMM 1**

Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs.

## **Determination Key Description: FHWA, FRA, FTA Programmatic Consultation For Transportation Projects Affecting NLEB Or Indiana Bat**

This key was last updated in IPaC on December 29, 2020. Keys are subject to periodic revision.

This decision key is intended for projects/activities funded or authorized by the Federal Highway Administration (FHWA), Federal Railroad Administration (FRA), and/or Federal Transit Administration (FTA), which may require consultation with the U.S. Fish and Wildlife Service (Service) under Section 7 of the Endangered Species Act (ESA) for the endangered **Indiana bat** (*Myotis sodalis*) and the threatened **Northern long-eared bat** (NLEB) (*Myotis septentrionalis*).

This decision key should only be used to verify project applicability with the Service's [February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects](#). The programmatic biological opinion covers limited transportation activities that may affect either bat species, and addresses situations that are both likely and not likely to adversely affect either bat species. This decision key will assist in identifying the effect of a specific project/activity and applicability of the programmatic consultation. The programmatic biological opinion is not intended to cover all types of transportation actions. Activities outside the scope of the programmatic biological opinion, or that may affect ESA-listed species other than the Indiana bat or NLEB, or any designated critical habitat, may require additional ESA Section 7 consultation.



## United States Department of the Interior

### FISH AND WILDLIFE SERVICE

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

Phone: (812) 334-4261 Fax: (812) 334-4273

<http://www.fws.gov/midwest/Endangered/section7/s7process/step1.html>



In Reply Refer To:

February 02, 2021

Consultation Code: 03E12000-2021-SLI-0678

Event Code: 03E12000-2021-E-03284

Project Name: Intersection Improvement Project (Des No. 1702775)

Subject: List of threatened and endangered species that may occur in your proposed project location or may be affected by your proposed project

#### To Whom It May Concern:

The attached species list identifies any federally threatened, endangered, proposed and candidate species that may occur within the boundary of your proposed project or may be affected by your proposed project. The list also includes designated critical habitat if present within your proposed project area or affected by your project. This list is provided to you as the initial step of the consultation process required under section 7(c) of the Endangered Species Act, also referred to as Section 7 Consultation.

Section 7 of the Endangered Species Act of 1973 requires that actions authorized, funded, or carried out by Federal agencies not jeopardize federally threatened or endangered species or adversely modify designated critical habitat. To fulfill this mandate, Federal agencies (or their designated non-federal representative) must consult with the Service if they determine their project “may affect” listed species or critical habitat.

Under 50 CFR 402.12(e) (the regulations that implement Section 7 of the Endangered Species Act) the accuracy of this species list should be verified after 90 days. This verification can be completed formally or informally. You may verify the list by visiting the ECOS-IPaC website <http://ecos.fws.gov/ipac/> at regular intervals during project planning and implementation and completing the same process you used to receive the attached list. As an alternative, you may contact this Ecological Services Field Office for updates.

Please use the species list provided and visit the U.S. Fish and Wildlife Service’s Region 3 Section 7 Technical Assistance website at - <http://www.fws.gov/midwest/endangered/section7/s7process/index.html>. This website contains step-by-step instructions which will help you determine if your project will have an adverse effect on listed species and will help lead you through the Section 7 process.

For all **wind energy projects** and **projects that include installing towers that use guy wires or are over 200 feet in height**, please contact this field office directly for assistance, even if no federally listed plants, animals or critical habitat are present within your proposed project or may be affected by your proposed project.

Although no longer protected under the Endangered Species Act, be aware that bald eagles are protected under the Bald and Golden Eagle Protection Act (16 U.S.C. 668 *et seq.*) and Migratory Bird Treaty Act (16 U.S.C. 703 *et seq.*), as are golden eagles. Projects affecting these species may require measures to avoid harming eagles or may require a permit. If your project is near an eagle nest or winter roost area, see our Eagle Permits website at <http://www.fws.gov/midwest/midwestbird/EaglePermits/index.html> to help you determine if you can avoid impacting eagles or if a permit may be necessary.

We appreciate your concern for threatened and endangered species. Please include the Consultation Tracking Number in the header of this letter with any request for consultation or correspondence about your project that you submit to our office.

Attachment(s):

- Official Species List

## Official Species List

This list is provided pursuant to Section 7 of the Endangered Species Act, and fulfills the requirement for Federal agencies to "request of the Secretary of the Interior information whether any species which is listed or proposed to be listed may be present in the area of a proposed action".

This species list is provided by:

**Indiana Ecological Services Field Office**

620 South Walker Street  
Bloomington, IN 47403-2121  
(812) 334-4261



## Project Summary

Consultation Code: 03E12000-2021-SLI-0678

Event Code: 03E12000-2021-E-03284

Project Name: Intersection Improvement Project (Des No. 1702775)

Project Type: TRANSPORTATION

Project Description: The City of Shelbyville, with partial funding from Federal Highway Administration (FHWA) and oversight from the Indiana Department of Transportation (INDOT), intends to proceed with an intersection improvement project at Miller Street and McKay Avenue in Shelby County, Indiana.

The proposed project is located at the intersection of Miller Street and McKay Road. Specifically, the project is located in Section 7, Township 12 North, Range 7 East as illustrated on the Shelbyville, Indiana 7.5-minute United States Geological Survey (USGS) topographic quadrangle

The surrounding land is primarily developed with residential dwellings. The Shelbyville High School and Middle School campus is present in the northwest quadrant of the intersection. The northeast quadrant of the intersection consists of single-family residential parcels and the Heritage House assisted living facility is present in the southwest quadrant. The southeast quadrant is occupied with a large, single-family dwelling. There is suitable summer habitat near the project area.

There will be tree removal required to complete the proposed improvement. Approximately 3 to 4 ornamental red maple trees will be removed in the northwest quadrant of the intersection. The trees will be removed under a separate contract. The tree clearing will take place between November 2022 and March 2023. Construction of the project is anticipated to begin in early 2023 and last approximately one year.

The preferred alternate will upgrade the existing intersection to a single-lane roundabout. Additional items to be constructed as a part of the project include new sidewalks and cross walks, an enclosed drainage system with new storm inlets, curbs and gutter. A multi-use path will be constructed within the southeast quadrant of the new roundabout.

New, additional permanent and temporary right-of-way will be required to construct the project. At this time, it is estimated that approximately 1.72 acres of additional, permanent right-of-way will be required to construct the project. Approximately 0.10 acre of temporary right-of-way will be necessary for construction access and drive re-grading.

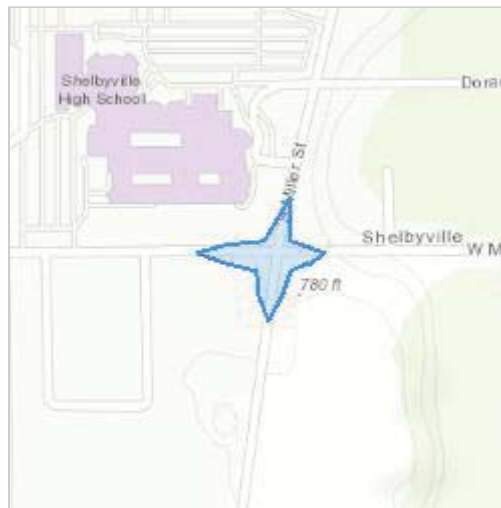
New, additional permanent right-of-way will be necessary to complete the

project and new, permanent lighting will be installed along each leg of the roundabout for approximately 300 to 400 feet. Temporary lighting may be necessary during construction. This will be determined via the contractor's means and methods.

On June 9, 2020, the INDOT Greenfield District reviewed the U.S. Fish and Wildlife Service database and no endangered bat species have been documented in or within 0.5 mile of the project area.

**Project Location:**

Approximate location of the project can be viewed in Google Maps: <https://www.google.com/maps/@39.50462845,-85.78717767500001,14z>



Counties: Shelby County, Indiana

## Endangered Species Act Species

There is a total of 2 threatened, endangered, or candidate species on this species list.

Species on this list should be considered in an effects analysis for your project and could include species that exist in another geographic area. For example, certain fish may appear on the species list because a project could affect downstream species. Note that 1 of these species should be considered only under certain conditions.

IPaC does not display listed species or critical habitats under the sole jurisdiction of NOAA Fisheries<sup>1</sup>, as USFWS does not have the authority to speak on behalf of NOAA and the Department of Commerce.

See the "Critical habitats" section below for those critical habitats that lie wholly or partially within your project area under this office's jurisdiction. Please contact the designated FWS office if you have questions.

- 
1. [NOAA Fisheries](#), also known as the National Marine Fisheries Service (NMFS), is an office of the National Oceanic and Atmospheric Administration within the Department of Commerce.

## Mammals

NAME	STATUS
Indiana Bat <i>Myotis sodalis</i> There is <b>final</b> critical habitat for this species. The location of the critical habitat is not available. Species profile: <a href="https://ecos.fws.gov/ecp/species/5949">https://ecos.fws.gov/ecp/species/5949</a>	Endangered
Northern Long-eared Bat <i>Myotis septentrionalis</i> No critical habitat has been designated for this species. This species only needs to be considered under the following conditions: <ul style="list-style-type: none"> <li>▪ Incidental take of the NLEB is not prohibited here. Federal agencies may consult using the 4(d) rule streamlined process. Transportation projects may consult using the programmatic process. See <a href="http://www.fws.gov/midwest/endangered/mammals/nleb/index.html">www.fws.gov/midwest/endangered/mammals/nleb/index.html</a></li> </ul> Species profile: <a href="https://ecos.fws.gov/ecp/species/9045">https://ecos.fws.gov/ecp/species/9045</a>	Threatened

## Critical habitats

THERE ARE NO CRITICAL HABITATS WITHIN YOUR PROJECT AREA UNDER THIS OFFICE'S JURISDICTION.

**From:** [Darrah, Taylor N](#)  
**To:** [Elayna Stoner](#)  
**Subject:** RE: USFWS Bat Database Review Request DES No 1702775  
**Date:** Tuesday, June 9, 2020 7:57:35 AM  
**Attachments:** [image001.png](#)  
[image002.png](#)  
[image003.png](#)  
[image004.png](#)  
[image005.png](#)  
[image006.png](#)

---

Elayna,

A review of the USFWS GIS database for Indiana bat and Northern long-eared bat roosting, hibernacula and capture sites was conducted for Des No. 1702775 on June 9, 2020. There are no documented sites within a half mile the project area. The USFWS Information for Planning and Conservation (IPaC) website must be consulted and a new project created to obtain an official species list and complete the questionnaire for the project to determine the applicability of the programmatic consultation. If needed, the IPaC generated documents must be forwarded to the USFWS for verification.

Thank you,

**Taylor Darrah**

***Environmental Section Manager***

Indiana Department of Transportation  
32 South Broadway  
Greenfield, IN 46140

**Office:** (317) 467-3915

**Cell:** (317) 526-6080 – **Please temporarily direct all calls to my cell phone**

**Email:** [TDarrah@indot.in.gov](mailto:TDarrah@indot.in.gov)



---

**From:** Elayna Stoner <elaynas@metricenv.com>  
**Sent:** Tuesday, June 09, 2020 12:08 AM  
**To:** Darrah, Taylor N <TDarrah@indot.IN.gov>  
**Subject:** USFWS Bat Database Review Request DES No 1702775

\*\*\*\* This is an EXTERNAL email. Exercise caution. DO NOT open attachments or click links from unknown senders or unexpected email. \*\*\*\*

---

Hello

## Organization and Project Information

**Project ID:**  
**Des. ID:** Designation Number 1702775.  
**Project Title:** Intersection improvement Project  
**Name of Organization:** Metric Environmental  
**Requested by:** Elayna Stoner

## Environmental Assessment Report

1. Geological Hazards:
  - Moderate liquefaction potential
  - 1% Annual Chance Flood Hazard
2. Mineral Resources:
  - Bedrock Resource: High Potential
  - Sand and Gravel Resource: High Potential
3. Active or abandoned mineral resources extraction sites:
  - None documented in the area

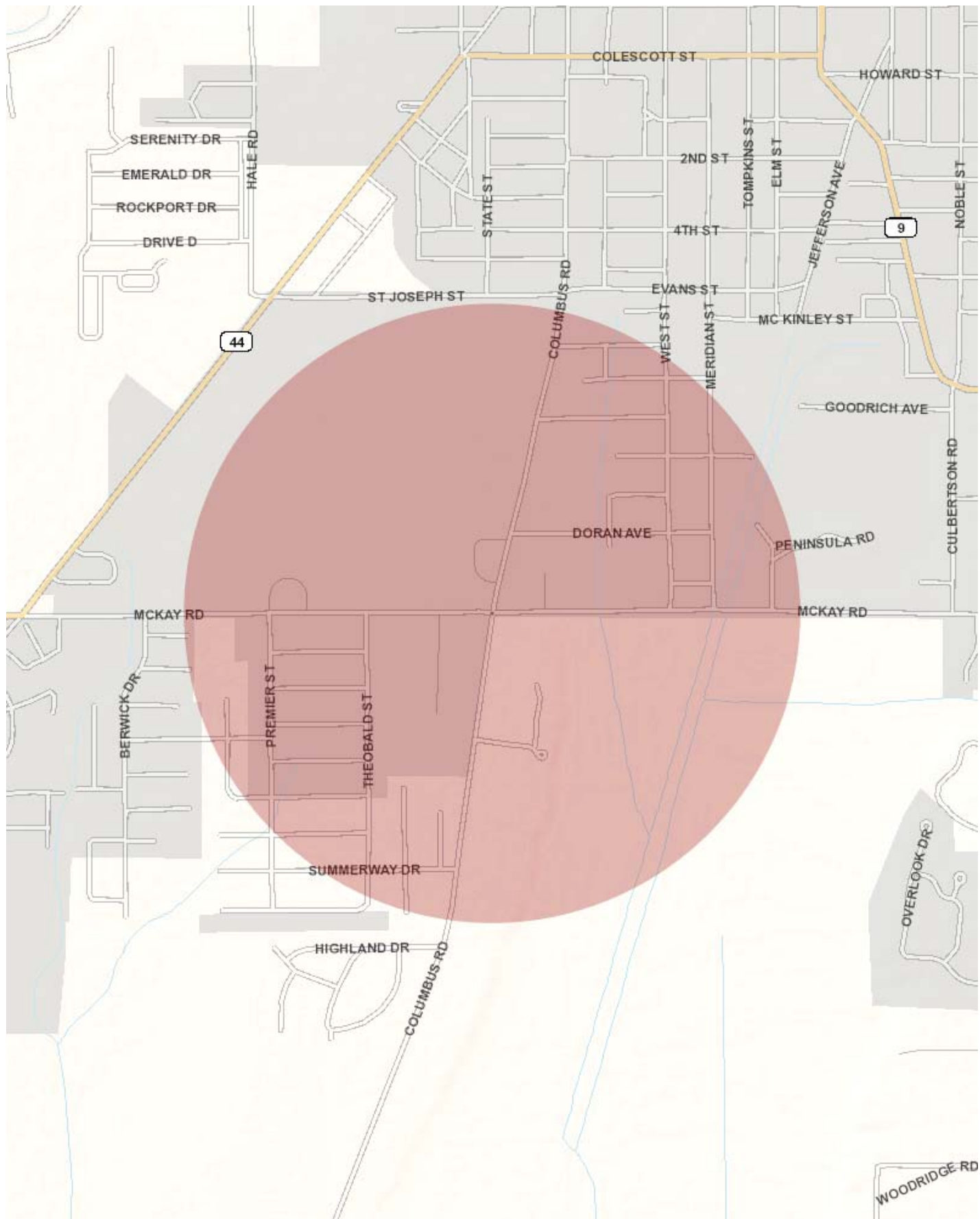
\*All map layers from Indiana Map ([maps.indiana.edu](https://maps.indiana.edu))

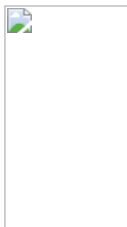
### **DISCLAIMER:**

This document was compiled by Indiana University, Indiana Geological Survey, using data believed to be accurate; however, a degree of error is inherent in all data. This product is distributed "AS-IS" without warranties of any kind, either expressed or implied, including but not limited to warranties of suitability to a particular purpose or use. No attempt has been made in either the design or production of these data and document to define the limits or jurisdiction of any federal, state, or local government. The data used to assemble this document are intended for use only at the published scale of the source data or smaller (see the metadata links below) and are for reference purposes only. They are not to be construed as a legal document or survey instrument. A detailed on-the-ground survey and historical analysis of a single site may differ from these data and this document.

This information was furnished by Indiana Geological Survey  
Address: 420 N. Walnut St., Bloomington, IN 47404  
Email: [IGSEnvir@indiana.edu](mailto:IGSEnvir@indiana.edu)  
Phone: 812 855-7428

Date: March 19, 2020





# Indiana Department of Environmental Management

*We Protect Hoosiers and Our Environment.*

100 North Senate Avenue - Indianapolis, IN 46204  
(800) 451-6027 - (317) 232-8603 - [www.idem.IN.gov](http://www.idem.IN.gov)

City of Shelbyville  
Tom DeBaun, Mayor  
44. W. Washington Street  
Shelbyville, IN 46176  
Date 3/19/2020

Metric Environmental  
Elayna Stoner  
6971 Hillsdale Court  
Indianapolis, IN 46250

To Engineers and Consultants Proposing Roadway Construction Projects:

RE: The City of Shelbyville with oversight from the Indiana Department of Transportation (INDOT) propose to utilize federal funds to partially fund and proceed with the referenced project. Intersection improvement Project at Miller Street and McKay Road (Designation Number 1702775). The proposed project is located at the intersection of Miller Street and McKay Road. Specifically, the project is located in Section 7, Township 12 North, Range 7 East (Addison Township) as illustrated on the Shelbyville, Indiana 7.5-minute United States Geological Survey (USGS) topographic quadrangle. The primary purpose of the project is to provide an improved intersection that meets current and projected traffic capacity and provides upgraded pedestrian facilities. The project is needed due to the current and projected traffic volumes, and lack of adequate pedestrian facilities. The preferred alternate will upgrade the existing intersection to a single-lane roundabout. Additional items to be constructed as a part of the project include new sidewalks and cross walks, an enclosed drainage system with new storm inlets, curbs and gutter. All sidewalks and ramp systems will be constructed in compliance with ADA (Americans with Disabilities Act) design parameters. Additional permanent and temporary right-of-way will be required to construct the project. As the project design advances, the exact amounts of right-of-way will be defined. The maintenance of traffic plan will close McKay Road and detour traffic while constructing the intersection proper and connecting the new approaches into McKay Road. The approaches connecting South Miller Street will be constructed near the existing roadway during this phase; however, the connections to the existing road will occur in Phase 2 of construction and will result in some partial closures and lane restrictions. The construction letting date is scheduled for October 2021. Construction is anticipated to begin in early 2022. The construction schedule will be coordinated with the school system to reduce disruptions to school traffic and bus routes.

This letter from the Indiana Department of Environmental Management (IDEM) serves as a standardized response to enquiries inviting IDEM comments on roadway construction, reconstruction, or other improvement projects within existing roadway corridors when the proposed scope of the project is beneath the threshold requiring a formal National Environmental Policy Act-mandated Environmental Assessment or Environmental Impact Statement. As the letter attempts to address all roadway-related environmental topics of potential concern, it is possible that not every topic addressed in the letter will be applicable to your particular roadway project.

For additional information on specific roadway-related topics of interest, please visit the appropriate Web pages cited below, many of which provide contact information for persons within the various program areas who can answer questions not fully addressed in this letter. Also please be mindful that some environmental requirements may be subject to change and so each person intending to include a copy of this letter in their project documentation packet is advised to download the most recently revised version of the letter; found at: <http://www.in.gov/idem/5283.htm> (<http://www.in.gov/idem/5283.htm>).

To ensure that all environmentally-related issues are adequately addressed, IDEM recommends that you read this letter in its entirety, and consider each of the following issues as you move forward with the planning of your proposed roadway construction, reconstruction, or improvement project:

## WATER AND BIOTIC QUALITY

1. Section 404 of the Clean Water Act requires that you obtain a permit from the U.S. Army Corps of Engineers (USACE) before discharging dredged or fill materials into any wetlands or other waters, such as rivers, lakes, streams, and ditches. Other activities regulated include the relocation, channelization, widening, or other such alteration of a stream, and the mechanical clearing (use of heavy construction equipment) of wetlands. Thus, as a project owner or sponsor, it is your responsibility to ensure that no wetlands are disturbed without the proper permit. Although you may initially refer to the U.S. Fish and Wildlife Service National Wetland Inventory maps as a means of identifying potential areas of concern, please be mindful that those maps do not depict jurisdictional wetlands regulated by the USACE or the Department of Environmental Management. A valid jurisdictional wetlands determination can only be made by the USACE, using the 1987 Wetland Delineation Manual.

USACE recommends that you have a consultant check to determine whether your project will abut, or lie within, a wetland area. To view a list of consultants that have requested to be included on a list posted by the USACE on their Web site, see USACE Permits and Public Notices (<http://www.lrl.usace.army.mil/orf/default.asp>) and then click on "Information" from the menu on the right-hand side of that page. Their "Consultant List" is the fourth entry down on the "Information" page. Please note that the USACE posts all consultants that request to appear on the list, and that inclusion of any particular consultant on the list does not represent an endorsement of that consultant by the USACE, or by IDEM.



Much of northern Indiana (Newton, Lake, Porter, LaPorte, St. Joseph, Elkhart, LaGrange, Steuben, and Dekalb counties; large portions of Jasper, Starke, Marshall, Noble, Allen, and Adams counties; and lesser portions of Benton, White, Pulaski, Kosciusko, and Wells counties) is served by the USACE District Office in Detroit (313-226-6812). The central and southern portions of the state (large portions of Benton, White, Pulaski, Kosciusko, and Wells counties; smaller portions of Jasper, Starke, Marshall, Noble, Allen, and Adams counties; and all other Indiana counties located in north-central, central, and southern Indiana) are served by the USACE Louisville District Office (502-315-6733).

Additional information on contacting these U.S. Army Corps of Engineers (USACE) District Offices, government agencies with jurisdiction over wetlands, and other water quality issues, can be found at <http://www.in.gov/idem/4396.htm> (<http://www.in.gov/idem/4396.htm>). IDEM recommends that impacts to wetlands and other water resources be avoided to the fullest extent.

2. In the event a Section 404 wetlands permit is required from the USACE, you also must obtain a Section 401 Water Quality Certification from the IDEM Office of Water Quality Wetlands Program. To learn more about the Wetlands Program, visit: <http://www.in.gov/idem/4384.htm> (<http://www.in.gov/idem/4384.htm>).
3. If the USACE determines that a wetland or other water body is isolated and not subject to Clean Water Act regulation, it is still regulated by the state of Indiana. A State Isolated Wetland permit from IDEM's Office of Water Quality (OWQ) is required for any activity that results in the discharge of dredged or fill materials into isolated wetlands. To learn more about isolated wetlands, contact the OWQ Wetlands Program at 317-233-8488.
4. If your project will involve over a 0.5 acre of wetland impact, stream relocation, or other large-scale alterations to water bodies such as the creation of a dam or a water diversion, you should seek additional input from the OWQ Wetlands Program staff. Consult the Web at: <http://www.in.gov/idem/4384.htm> (<http://www.in.gov/idem/4384.htm>) for the appropriate staff contact to further discuss your project.
5. Work within the one-hundred year floodway of a given water body is regulated by the Department of Natural Resources, Division of Water. The Division issues permits for activities regulated under the follow statutes:
  - IC 14-26-2 Lakes Preservation Act 312 IAC 11
  - IC 14-26-5 Lowering of Ten Acre Lakes Act No related code
  - IC 14-28-1 Flood Control Act 310 IAC 6-1
  - IC 14-29-1 Navigable Waterways Act 312 IAC 6
  - IC 14-29-3 Sand and Gravel Permits Act 312 IAC 6
  - IC 14-29-4 Construction of Channels Act No related code

For information on these Indiana (statutory) Code and Indiana Administrative Code citations, see the DNR Web site at: <http://www.in.gov/dnr/water/9451.htm> (<http://www.in.gov/dnr/water/9451.htm>). Contact the DNR Division of Water at 317-232-4160 for further information.

The physical disturbance of the stream and riparian vegetation, especially large trees overhanging any affected water bodies should be limited to only that which is absolutely necessary to complete the project. The shade provided by the large overhanging trees helps maintain proper stream temperatures and dissolved oxygen for aquatic life.

6. For projects involving construction activity (which includes clearing, grading, excavation and other land disturbing activities) that result in the disturbance of one (1), or more, acres of total land area, contact the Office of Water Quality – Watershed Planning Branch (317/233-1864) regarding the need for a Rule 5 Storm Water Runoff Permit. Visit the following Web page
  - <http://www.in.gov/idem/4902.htm> (<http://www.in.gov/idem/4902.htm>)

To obtain, and operate under, a Rule 5 permit you will first need to develop a Construction Plan (<http://www.in.gov/idem/4917.htm#constreq>) (<http://www.in.gov/idem/4917.htm#constreq>), and as described in 327 IAC 15-5-6.5 (<http://www.in.gov/legislative/iac/T03270/A00150> [PDF] (<http://www.in.gov/legislative/iac/T03270/A00150.PDF>), pages 16 through 19). Before you may apply for a Rule 5 Permit, or begin construction, you must submit your Construction Plan to your county Soil and Water Conservation District (SWCD) (<http://www.in.gov/isda/soil/contacts/map.html> (<http://www.in.gov/isda/soil/contacts/map.html>)).

Upon receipt of the construction plan, personnel of the SWCD or the Indiana Department of Environmental Management will review the plan to determine if it meets the requirements of 327 IAC 15-5. Plans that are deemed deficient will require re-submittal. If the plan is sufficient you will be notified and instructed to submit the verification to IDEM as part of the Rule 5 Notice of Intent (NOI) submittal. Once construction begins, staff of the SWCD or Indiana Department of Environmental Management will perform inspections of activities at the site for compliance with the regulation.

Please be mindful that approximately 149 Municipal Separate Storm Sewer System (MS4) areas are now being established by various local governmental entities throughout the state as part of the implementation of Phase II federal storm water requirements. All of these MS4 areas will eventually take responsibility for Construction Plan review, inspection, and enforcement. As these MS4 areas obtain program approval from IDEM, they will be added to a list of MS4 areas posted on the IDEM Website at: <http://www.in.gov/idem/4900.htm> (<http://www.in.gov/idem/4900.htm>).

If your project is located in an IDEM-approved MS4 area, please contact the local MS4 program about meeting their storm water requirements. Once the MS4 approves the plan, the NOI can be submitted to IDEM.



Regardless of the size of your project, or which agency you work with to meet storm water requirements, IDEM recommends that appropriate structures and techniques be utilized both during the construction phase, and after completion of the project, to minimize the impacts associated with storm water runoff. The use of appropriate planning and site development and appropriate storm water quality measures are recommended to prevent soil from leaving the construction site during active land disturbance and for post construction water quality concerns. Information and assistance regarding storm water related to construction activities are available from the Soil and Water Conservation District (SWCD) offices in each county or from IDEM.

7. For projects involving impacts to fish and botanical resources, contact the Department of Natural Resources - Division of Fish and Wildlife (317/232-4080) for addition project input.
8. For projects involving water main construction, water main extensions, and new public water supplies, contact the Office of Water Quality - Drinking Water Branch (317-308-3299) regarding the need for permits.
9. For projects involving effluent discharges to waters of the State of Indiana, contact the Office of Water Quality - Permits Branch (317-233-0468) regarding the need for a National Pollutant Discharge Elimination System (NPDES) permit.
10. For projects involving the construction of wastewater facilities and sewer lines, contact the Office of Water Quality - Permits Branch (317-232-8675) regarding the need for permits.

## AIR QUALITY

The above-noted project should be designed to minimize any impact on ambient air quality in, or near, the project area. The project must comply with all federal and state air pollution regulations. Consideration should be given to the following:

1. Regarding open burning, and disposing of organic debris generated by land clearing activities; some types of open burning are allowed (<http://www.in.gov/idem/4148.htm>) under specific conditions. You also can seek an open burning variance from IDEM.

However, IDEM generally recommends that you take vegetative wastes to a registered yard waste composting facility or that the waste be chipped or shredded with composting on site (you must register with IDEM if more than 2,000 pounds is to be composted; contact 317/232-0066). The finished compost can then be used as a mulch or soil amendment. You also may bury any vegetative wastes (such as leaves, twigs, branches, limbs, tree trunks and stumps) onsite, although burying large quantities of such material can lead to subsidence problems, later on.

Reasonable precautions must be taken to minimize fugitive dust emissions from construction and demolition activities. For example, wetting the area with water, constructing wind barriers, or treating dusty areas with chemical stabilizers (such as calcium chloride or several other commercial products). Dirt tracked onto paved roads from unpaved areas should be minimized.

Additionally, if construction or demolition is conducted in a wooded area where blackbirds have roosted or abandoned buildings or building sections in which pigeons or bats have roosted for 3-5 years precautionary measures should be taken to avoid an outbreak of histoplasmosis. This disease is caused by the fungus *Histoplasma capsulatum*, which stems from bird or bat droppings that have accumulated in one area for 3-5 years. The spores from this fungus become airborne when the area is disturbed and can cause infections over an entire community downwind of the site. The area should be wetted down prior to cleanup or demolition of the project site. For more detailed information on histoplasmosis prevention and control, please contact the Acute Disease Control Division of the Indiana State Department of Health at (317) 233-7272.

2. The U.S. EPA and the Surgeon General recommend that people not have long-term exposure to radon at levels above 4 pCi/L. (For a county-by-county map of predicted radon levels in Indiana, visit: <http://www.in.gov/idem/4145.htm>.)

The U.S. EPA further recommends that all homes (and apartments within three stories of ground level) be tested for radon. If in-home radon levels are determined to be 4 pCi/L, or higher, EPA recommends a follow-up test. If the second test confirms that radon levels are 4 pCi/L, or higher, EPA recommends the installation of radon-reduction measures. (For a list of qualified radon testers and radon mitigation (or reduction) specialists visit: [http://www.in.gov/isdh/regsvcs/radhealth/pdfs/radon\\_testers\\_mitigators\\_list.pdf](http://www.in.gov/isdh/regsvcs/radhealth/pdfs/radon_testers_mitigators_list.pdf).) It also is recommended that radon reduction measures be built into all new homes, particularly in areas like Indiana that have moderate to high predicted radon levels.

To learn more about radon, radon risks, and ways to reduce exposure visit: <http://www.in.gov/isdh/regsvcs/radhealth/radon.htm> (<http://www.in.gov/isdh/regsvcs/radhealth/radon.htm>), <http://www.in.gov/idem/4145.htm> (<http://www.in.gov/idem/4145.htm>), or <http://www.epa.gov/radon/index.html> (<http://www.epa.gov/radon/index.html>).

3. With respect to asbestos removal: all facilities slated for renovation or demolition (except residential buildings that have (4) four or fewer dwelling units and which will not be used for commercial purposes) must be inspected by an Indiana-licensed asbestos inspector prior to the commencement of any renovation or demolition activities. If regulated asbestos-containing material (RACM) that may become airborne is found, any subsequent demolition, renovation, or asbestos removal activities must be performed in accordance with the proper notification and emission control requirements.

If no asbestos is found where a renovation activity will occur, or if the renovation involves removal of less than 260 linear feet of RACM off of pipes, less than 160 square feet of RACM off of other facility components, or less than 35 cubic feet of RACM off of all facility components, the owner or operator of the project does not need to notify IDEM before beginning the renovation activity.

For questions on asbestos demolition and renovation activities, you can also call IDEM's Lead/Asbestos section at 1-888-574-8150.

However, in all cases where a demolition activity will occur (even if no asbestos is found), the owner or operator must still notify IDEM 10 working days prior to the demolition, using the form found at <http://www.in.gov/icpr/webfile/formsdiv/44593.pdf> (<http://www.in.gov/icpr/webfile/formsdiv/44593.pdf>).

Anyone submitting a renovation/demolition notification form will be billed a notification fee based upon the amount of friable asbestos containing material to be removed or demolished. Projects that involve the removal of more than 2,600 linear feet of friable asbestos containing materials on pipes, or 1,600 square feet or 400 cubic feet of friable asbestos containing material on other facility components, will be billed a fee of \$150 per project; projects below these amounts will be billed a fee of \$50 per project. All notification remitters will be billed on a quarterly basis.

For more information about IDEM policy regarding asbestos removal and disposal, visit: <http://www.in.gov/idem/4983.htm> (<http://www.in.gov/idem/4983.htm>).

4. With respect to lead-based paint removal: IDEM encourages all efforts to minimize human exposure to lead-based paint chips and dust. IDEM is particularly concerned that young children exposed to lead can suffer from learning disabilities. Although lead-based paint abatement efforts are not mandatory, any abatement that is conducted within housing built before January 1, 1978, or a child-occupied facility is required to comply with all lead-based paint work practice standards, licensing and notification requirements. For more information about lead-based paint removal visit: <http://www.in.gov/isdh/19131.htm> (<http://www.in.gov/isdh/19131.htm>).
5. Ensure that asphalt paving plants are permitted and operate properly. The use of cutback asphalt, or asphalt emulsion containing more than seven percent (7%) oil distillate, is prohibited during the months April through October. See 326 IAC 8-5-2, Asphalt Paving Rule (<http://www.ai.org/legislative/iac/T03260/A00080.PDF>) (<http://www.ai.org/legislative/iac/T03260/A00080.PDF>)).
6. If your project involves the construction of a new source of air emissions or the modification of an existing source of air emissions or air pollution control equipment, it will need to be reviewed by the IDEM Office of Air Quality (OAQ). A registration or permit may be required under 326 IAC 2 (View at: [www.ai.org/legislative/iac/t03260/a00020.pdf](http://www.ai.org/legislative/iac/t03260/a00020.pdf)) (<http://www.ai.org/legislative/iac/t03260/a00020.pdf>)). New sources that use or emit hazardous air pollutants may be subject to Section 112 of the Clean Air Act and corresponding state air regulations governing hazardous air pollutants.
7. For more information on air permits visit: <http://www.in.gov/idem/4223.htm> (<http://www.in.gov/idem/4223.htm>), or to initiate the IDEM air permitting process, please contact the Office of Air Quality Permit Reviewer of the Day at (317) 233-0178 or [OAMPROD@dem.state.in.us](mailto:OAMPROD@dem.state.in.us).

## LAND QUALITY

In order to maintain compliance with all applicable laws regarding contamination and/or proper waste disposal, IDEM recommends that:

1. If the site is found to contain any areas used to dispose of solid or hazardous waste, you need to contact the Office of Land Quality (OLQ) at 317-308-3103.
2. All solid wastes generated by the project, or removed from the project site, need to be taken to a properly permitted solid waste processing or disposal facility. For more information, visit <http://www.in.gov/idem/4998.htm> (<http://www.in.gov/idem/4998.htm>).
3. If any contaminated soils are discovered during this project, they may be subject to disposal as hazardous waste. Please contact the OLQ at 317-308-3103 to obtain information on proper disposal procedures.
4. If PCBs are found at this site, please contact the Industrial Waste Section of OLQ at 317-308-3103 for information regarding management of any PCB wastes from this site.
5. If there are any asbestos disposal issues related to this site, please contact the Industrial Waste Section of OLQ at 317-308-3103 for information regarding the management of asbestos wastes (Asbestos removal is addressed above, under Air Quality).
6. If the project involves the installation or removal of an underground storage tank, or involves contamination from an underground storage tank, you must contact the IDEM Underground Storage Tank program at 317/308-3039. See: <http://www.in.gov/idem/4999.htm> (<http://www.in.gov/idem/4999.htm>).

## FINAL REMARKS

Should you need to obtain any environmental permits in association with this proposed project, please be mindful that IC 13-15-8 requires that you notify all adjoining property owners and/or occupants within ten days your submittal of each permit application. However, if you are seeking multiple permits, you can still meet the notification requirement with a single notice if all required permit applications are submitted with the same ten day period.

Should the scope of the proposed project be expanded to the extent that a National Environmental Policy Act Environmental Assessment (EA) or Environmental Impact Statement (EIS) is required, IDEM will actively participate in any early interagency coordination review of the project.

Meanwhile, please note that this letter does not constitute a permit, license, endorsement or any other form of approval on the part of the Indiana Department of Environmental Management regarding any project for which a copy of this letter is used. Also note that it is the responsibility of the project engineer or consultant using this letter to ensure that the most current draft of this document, which is located at

<http://www.in.gov/ide/5284.htm> (<http://www.in.gov/ide/5284.htm>), is used.

---

## Signature(s) of the Applicant

I acknowledge that the following proposed roadway project will be financed in part, or in whole, by public monies.

## Project Description

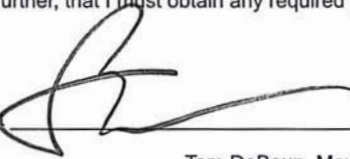
The City of Shelbyville with oversight from the Indiana Department of Transportation (INDOT) propose to utilize federal funds to partially fund and proceed with the referenced project. Intersection improvement Project at Miller Street and McKay Road (Designation Number 1702775). The proposed project is located at the intersection of Miller Street and McKay Road. Specifically, the project is located in Section 7, Township 12 North, Range 7 East (Addison Township) as illustrated on the Shelbyville, Indiana 7.5-minute United States Geological Survey (USGS) topographic quadrangle. The primary purpose of the project is to provide an improved intersection that meets current and projected traffic capacity and provides upgraded pedestrian facilities. The project is needed due to the current and projected traffic volumes, and lack of adequate pedestrian facilities. The preferred alternate will upgrade the existing intersection to a single-lane roundabout. Additional items to be constructed as a part of the project include new sidewalks and cross walks, an enclosed drainage system with new storm inlets, curbs and gutter. All sidewalks and ramp systems will be constructed in compliance with ADA (Americans with Disabilities Act) design parameters. Additional permanent and temporary right-of-way will be required to construct the project. As the project design advances, the exact amounts of right-of-way will be defined. The maintenance of traffic plan will close McKay Road and detour traffic while constructing the intersection proper and connecting the new approaches into McKay Road. The approaches connecting South Miller Street will be constructed near the existing roadway during this phase; however, the connections to the existing road will occur in Phase 2 of construction and will result in some partial closures and lane restrictions. The construction letting date is scheduled for October 2021. Construction is anticipated to begin in early 2022. The construction schedule will be coordinated with the school system to reduce disruptions to school traffic and bus routes.

With my signature, I do hereby affirm that I have read the letter from the Indiana Department of Environment that appears directly above. In addition, I understand that in order to complete that project in which I am interested, with a minimum of impact to the environment, I must consider all the issues addressed in the aforementioned letter, and further, that I must obtain any required permits.

Date: 04/05/2021

Signature of the INDOT

Project Engineer or Other Responsible Agent



Tom DeBaun, Mayor

Date: 3/19/2020

Signature of the

For Hire Consultant Elayna Stoner

Elayna Stoner

August 20, 2020

Elayna Stoner  
Metric Environmental  
6971 Hillsdale Court  
Indianapolis, Indiana 46250

Dear Ms. Stoner:

The proposed project to make intersection improvements at Miller Street and McKay Road in Addison Township, City of Shelbyville, Shelby County, Indiana (Des No 1702775), as referred to in your letter received July 7, 2020, will not cause a conversion of prime farmland.

If you need additional information, please contact John Allen at 317-295-5859.

Sincerely,

**RICHARD**  
**NEILSON**

Digitally signed by  
RICHARD NEILSON  
Date: 2020.08.21  
11:09:58 -04'00'

RICK NEILSON  
State Soil Scientist



**APPENDIX D**  
**Section 106 of the National Historic  
Preservation Act**

**FEDERAL HIGHWAY ADMINISTRATION'S  
SECTION 4(F) COMPLIANCE REQUIREMENTS (for historic properties) AND  
SECTION 106 FINDINGS AND DETERMINATIONS  
AREA OF POTENTIAL EFFECTS  
ELIGIBILITY DETERMINATIONS  
EFFECT FINDING  
MILLER STREET/MCKAY ROAD INTERSECTION IMPROVEMENT PROJECT  
CITY OF SHELBYVILLE, ADDISON TOWNSHIP, SHELBY COUNTY, INDIANA  
DES. NO.: 1702775**

**AREA OF POTENTIAL EFFECTS**

**(Pursuant to 36 CFR Section 800.4(a)(1))**

The Area of Potential Effects (APE) extends approximately 500 feet beyond the project end points in each direction and was widened in areas with greater viewshed to the project area. A map of the APE can be found in Appendix A.

**ELIGIBILITY DETERMINATIONS**

**(Pursuant to 36 CFR 800.4(c)(2))**

The APE contains no properties listed, or eligible for listing, in the National Register of Historic Places (NRHP).

**EFFECT FINDING**

INDOT, acting on FHWA's behalf, has determined a "No Historic Properties Affected" finding is appropriate for this undertaking. INDOT respectfully requests the Indiana State Historic Preservation Officer provide written concurrence with the Section 106 determination of effect.

**SECTION 4(F) COMPLIANCE REQUIREMENTS (for historic properties)**

This undertaking will not convert property from any Section 4(f) historic property to a transportation use; the INDOT, acting on FHWA's behalf, has determined the appropriate Section 106 finding is "No Historic Properties Affected"; therefore no Section 4(f) evaluation is required.

*Anuradha V. Kumar*

---

Anuradha V. Kumar, for FHWA  
Manager  
INDOT Cultural Resources

02/24/2021

---

Approved Date



**FEDERAL HIGHWAY ADMINISTRATION  
DOCUMENTATION OF SECTION 106 FINDING OF  
NO HISTORIC PROPERTIES AFFECTED  
SUBMITTED TO THE STATE HISTORIC PRESERVATION OFFICER  
PURSUANT TO 36 CFR SECTION 800.4(d)(1)  
MILLER STREET/MCKAY ROAD INTERSECTION IMPROVEMENT PROJECT  
CITY OF SHELBYVILLE, ADDISON TOWNSHIP, SHELBY COUNTY, INDIANA  
DES. NO.: 1702775**

**1. DESCRIPTION OF THE UNDERTAKING**

**A) Project Description**

The City of Shelbyville, with funding from the Federal Highway Administration and administrative oversight from the Indiana Department of Transportation (INDOT), proposes to proceed with the Miller Street/McKay Road Intersection Improvement Project. The proposed undertaking is located in southwest Shelbyville approximately 4,550 feet west of State Route (SR) 9 and 4,000 feet east of South (S) Miller Avenue in Addison Township in central Shelby County, Indiana. The project area can be found on the Shelbyville, Indiana USGS Topographic Quadrangle, in Section 7, Township 12 North, Range 7 East. See Appendix A for maps of the project location.

The primary purpose of the project is to provide an improved intersection at Miller Street and McKay Road that meets current and projected traffic capacity and provides upgraded pedestrian facilities. The project is needed due to the current and projected traffic volumes, and lack of adequate pedestrian facilities. Traffic congestion is a concern during the peak travel times in the morning and afternoons due to the proximity of Shelbyville school facilities located immediately west of the intersection. The high volume of school bus, student, and staff traffic, combined with local through traffic all utilizing the intersection concurrently, results in congestion and delays during peak travel times during the morning and late afternoons.

An additional need for the project is a result of the large number of students who walk to school and cross the intersection to access the school campus. Pedestrian safety is a primary concern because there are two quadrants of the intersection that lack sidewalks and cross walks. A secondary safety issue affects the vehicles traveling westbound from State Road (SR) 9 to SR 44 via McKay Road. The east leg of McKay Road has a substantial grade of over six percent entering the intersection. During inclement weather conditions the steep grade creates difficult conditions for westbound vehicles stopping and restarting on the grade.

The preferred alternative would reconfigure the existing four-way stop intersection into a single-lane roundabout. Additional items to be constructed as a part of the project include new sidewalks and cross

walks, an enclosed drainage system with new storm inlets, curbs, and gutter. All sidewalks and ramp systems will be constructed in compliance with Americans with Disabilities Act (ADA) design parameters. An additional 1.72 acres of permanent right-of-way and 0.10 acre of temporary right-of-way will be needed. See Appendix B for detailed project plans.

The maintenance of traffic plan will close McKay Road and detour traffic while constructing the intersection proper and connecting the new approaches into McKay Road. The approaches connecting South Miller Street will be constructed near the existing roadway during this phase; however, the connections to the existing road will occur in Phase 2 of construction and will result in some partial closures and lane restrictions. Traffic will be detoured via other nearby roads, including Miller Avenue, S West Street, and S Harrison Street for north-south traffic, and Doran Avenue, St Joseph Street, and W CR 250S for east-west traffic. The numerous alternate routes in the vicinity should adequately handle any diverted traffic from the project area. Therefore, it is believed the detour routes would not experience appreciable impacts from this project to warrant including the traffic diversion routes in the APE.

The construction letting date is scheduled for October 2021. Construction is anticipated to begin in early 2022. The construction schedule will be coordinated with the school system to reduce disruptions to school traffic and bus routes.

Federal-aid highway construction projects qualify as “undertakings” as defined in CFR 800.16(y) and are subject to a Section 106 review as per Section 106 of the National Historic Preservation Act of 1966. Federal-aid funds will be used for planning and/or construction of the proposed rest area improvements; thus, a Section 106 review is applicable.

The Area of Potential Effects (APE) includes all locations where the project may result in disturbance of the ground; all locations from which elements of the project may be visible or audible; all locations where activity may result in changes in traffic patterns, land use, or public access; and all areas where there may be direct or indirect effects due to elements of the project. The archaeological APE was defined as the project footprint. The above-ground structures APE was defined as extending approximately 500 feet beyond the project end points in each direction and was widened in areas with greater viewshed to the project area. Aerial maps of the APE are located in Appendix A and project site photographs are located in Appendix C.

## **2. EFFORTS TO IDENTIFY HISTORIC PROPERTIES**

### **A) Historic Property Report**

To determine the presence of historic properties within the project’s APE, a short-format historic property report (HPR) was prepared by Metric Environmental, LLC (Hudziak, 6/18/2020). Hudziak is a

Qualified Professional (QP) and meets the Secretary of the Interior's Professional Qualification Standards pursuant to 36 CFR Section 800.4(b).

Hudziak reviewed the Indiana Register of Historic Sites and Structures and the National Register of Historic Places (NRHP) and found no listed properties within the APE.

Hudziak also reviewed the 1992 *Shelby County Historic Sites and Structures Inventory Report*, the Indiana Division of Historic Preservation and Archaeology (DHPA) online State Historic Architectural and Archaeological Research Database (SHAARD), and the Indiana Historic Buildings, Bridges, and Cemeteries Map (IHBBC) and found that no "Notable" or "Outstanding" rated properties were located within the APE.

Additionally, on April 9, 2020, Hudziak conducted a field survey for the project.

The HPR included the project's APE and recommended that no properties within the APE are eligible for NRHP listing.

The abstract and conclusion sections of the HPR are presented in Appendix D. The INDOT Cultural Resources Office (INDOT-CRO) determined that the HPR was suitable for distribution to consulting parties on July 8, 2020.

#### B) Archaeological Survey

A Phase Ia Archaeological Survey Report was prepared by Metric Environmental, LLC (Snell, 12/22/20). Snell is a Qualified Professional (QP) archaeologist who meets the Secretary of the Interior's Professional Qualification Standards pursuant to 36 CFR Section 800.4(b). A literature review of the SHAARD database indicated that there are no previously recorded archaeological sites within 1.0 mile of the project.

The unmarked Shanks Cemetery (Indiana Cemetery Record No. CR-73-8) was reported as possibly being in or adjacent to the archaeological APE, defined as the project footprint. The record for this resource describes its location as being 200 yards southeast of the Miller Street and McKay Road intersection, but that no visible sign of the cemetery remained; No ranking for the resource was provided. In October 2020, the INDOT-CRO conducted a geophysical survey within the southeast quadrant of the intersection of Miller Street and McKay Road. The geophysical survey did not identify any evidence of the cemetery and concluded its location is outside of and to the southeast of the project footprint. A summary of the INDOT-CRO geophysical survey is presented in Appendix E; the full geophysical survey report was included as an appendix with the Phase Ia Archaeological Survey Report (Snell, 12/22/20).

On November 6, 2020, Snell and other Metric staff conducted field work that included a visual inspection, pedestrian survey, soil cores, and the excavation of shovel test probes. No archaeological resources were identified as a result of the investigation.

Snell prepared and submitted the Phase Ia Archaeological Survey Report to the INDOT's CRO for review. The report recommended the project be allowed to proceed with no additional work. On December 22,

2020, the CRO released the report for submittal to the State Historic Preservation Office (SHPO) and consulting parties for review and comment. The Phase I Archaeological Survey Report's summary and recommendations are presented in Appendix F.

### C) Consultation

#### 1. Consulting Party Invitation

Per 36 CFR 800.2(c)(1) the Federal agency fulfilling the Section 106 requirements is statutorily obligated to involve stakeholders in consultation. Per Section 101 (b)(3) of the National Historic Preservation Act of 1966, the SHPO is responsible for consulting on Federal undertakings that may affect historic properties.

In addition to the SHPO, the parties listed below were invited to participate as consulting parties for this undertaking.

<b>Invited Consulting Party</b>	<b>Accepted/Declined Invitation</b>
Shelby County Historic Society	No Response—Declined
Joseph Boggs Society of Historic Preservation	No Response—Declined
Shelbyville Mayor	No Response—Declined
Department Head: Genealogy and History, Shelby County Public Library	Accepted
Eastern Shawnee Tribe of Oklahoma	No Response
Peoria Tribe of Indians of Oklahoma	No Response
Indiana Landmarks, Central Regional Office	Accepted
Indianapolis Metropolitan Planning Organization	No Response—Declined
Shelby County Commissioners	No Response—Declined
Shelbyville City Council President	No Response—Declined
Miami Tribe of Oklahoma	Accepted
Pokagon Band of Potawatomi Indians	No Response

A hard copy of the Early Coordination Letter (ECL) was sent to the SHPO on January 23, 2020, and the other consulting parties received it via email. The SHPO was requested to identify the need to include additional consulting parties. The parties were requested to indicate whether they agreed or did not agree to participate as a consulting party within thirty (30) days of receipt of the invitation. It was noted that if the invited consulting party did not reply, they would not be considered a consulting party and would not receive further information about the undertaking unless the scope changed.

In an email dated January 24, 2020, the Head of Genealogy and History, Shelby County Public Library, accepted the invitation to participate as a consulting party, and, in emails dated January 25 and 27, 2020, discussed and shared additional information concerning Shanks Cemetery (CR-73-8), which was reported as possibly being in or adjacent to the archaeological APE.

In a letter dated February 5, 2020, the SHPO acknowledged receipt of the ECL and noted they were not aware of any further stakeholders who should be invited to be consulting parties.

In a letter dated February 10, 2020, the Indiana Landmarks, Central Regional Office, accepted the invitation to participate as a consulting party. They requested that further research be undertaken to learn more about the unmarked Shanks Cemetery (Cr-73-8) and to determine if the site would be eligible for the NRHP.

In a letter dated February 20, 2020, the Miami Tribe of Oklahoma accepted the invitation to participate as a consulting party and stated that if any Native American artifacts are discovered during the project their office should be notified immediately.

No other replies were received.

All consulting parties' correspondence is in Appendix G.

## 2. Consultation Regarding the Area of Potential Effects and National Register of Historic Places Eligibility

On July 8 and 9, 2020, a request to review the HPR was sent to the SHPO and consulting parties. On July 20, 2020, a revised request to review the HPR was sent to the SHPO and the consulting parties because the initial request did not include the required attachments. The SHPO was provided a digital copy of the report, and the other consulting parties were notified of the availability of the report for review at IN SCOPE (<http://erms.indot.in.gov/Section106Documents/>). Recipients of this review request were asked to provide comments within 30 days; however, Tribal consulting parties could enter the process at any time and were encouraged to respond to the notification with any comments or concerns at their earliest convenience.

In a letter dated July 20, 2020, the SHPO acknowledged receipt of the HPR. The SHPO agreed with the HPR's proposed APE and recommendations that there are no historic properties listed or eligible for inclusion in the NRHP with the project's APE. In an email dated September 1, 2020, the Head of Genealogy and History, Shelby County Public Library, requested to remain a consulting party for the project. No other comments regarding the HPR were received.

Requests to review the Phase Ia Archaeological Survey Report were sent December 22, 2020, to the SHPO and the consulting parties. The SHPO was provided a digital copy of the report, and the other consulting parties were notified of the availability of the report for review at IN SCOPE (<http://erms.indot.in.gov/Section106Documents/>). Recipients of this review request were asked to provide comments within 30 days; however, Tribal consulting parties could enter the process at any time and were encouraged to respond to the notification with any comments or concerns at their earliest convenience.

In an email dated January 4, 2021, the Head of Genealogy and History, Shelby County Public Library, stated that although the Shanks Cemetery was not found within the proposed work area, they requested to continue being a consulting party for the project.

In a letter dated January 21, 2021, the SHPO concurred with the opinion of the archaeologist that no further archaeological investigations are necessary, with the condition that if the boundaries of the project be altered to include additional areas in the southeastern quadrant of the intersection, then additional archaeological investigations and/or archaeological monitoring may be required. Submission of such information to the SHPO for review and comment would then be required. The SHPO recommended INDOT to put forth a finding. No other comments regarding the archaeological report were received.

All consulting parties' correspondence is in Appendix G.

### **3. BASIS FOR FINDING**

The SHPO concurred with the proposed APEs and NRHP eligibility recommendations of both the project historian and the archaeologist. No other consulting parties provided comment on the proposed APEs and NRHP eligibility recommendations of both the project historian and archaeologist.

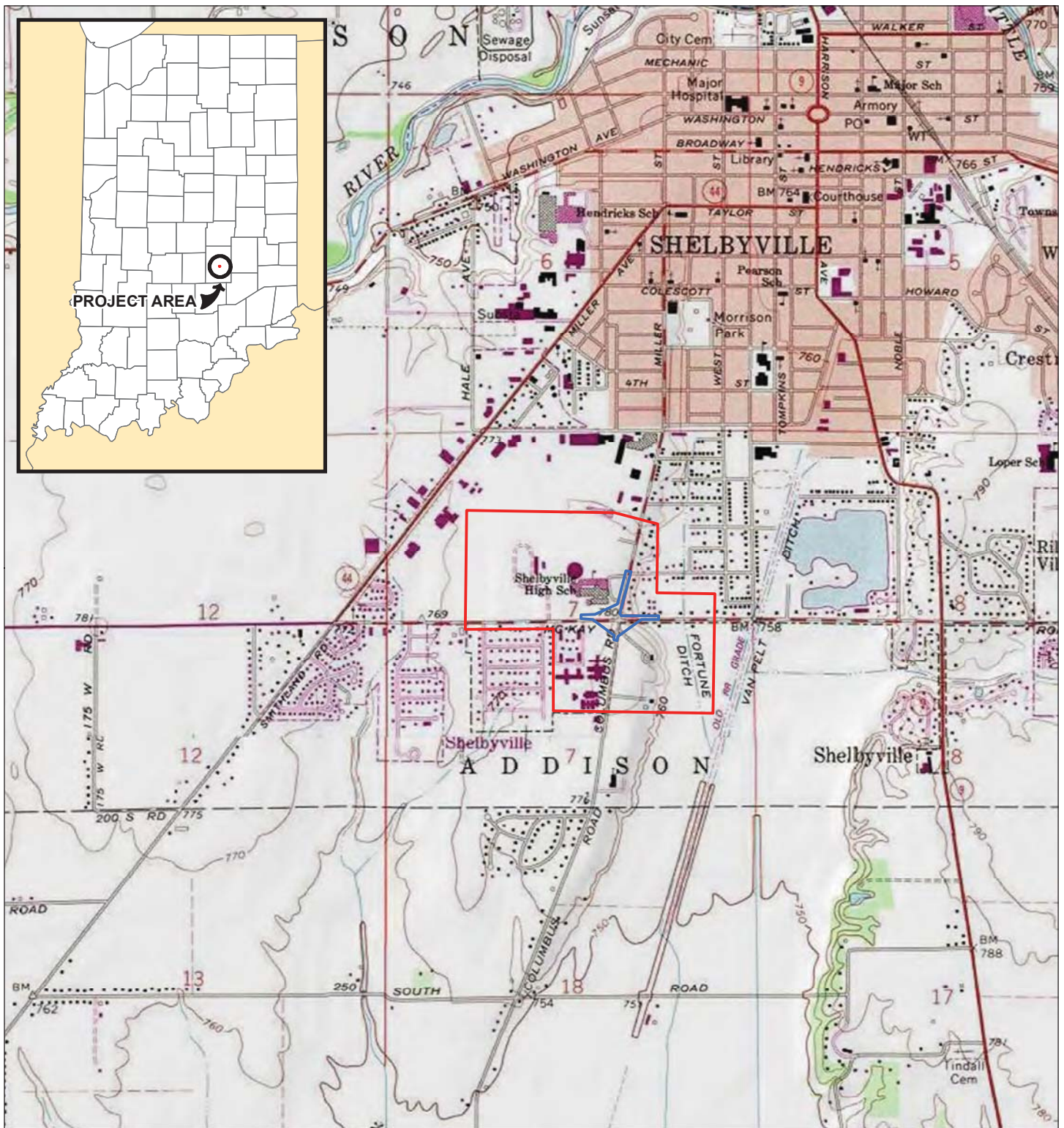
Because no historic properties are present within the APE, a finding of "no historic properties affected" has been made for this undertaking.

INDOT's Findings, made on behalf of the FHWA, and supporting 800.4(d)(1)) documentation are hereby provided to the SHPO and other consulting parties for a final 30-day comment period. Views of the public are being concurrently sought through publication of the Findings in a locally available, widely circulated newspaper. This document will be revised if necessary if public comment warrants it.

### **APPENDICES**

- A. Project Location Maps and APE
- B. Project Information and Plans
- C. Project Site Photographs and Key Maps
- D. Historic Property Report Summary and Conclusion
- E. Geophysical Survey Report Summary
- F. Archaeology Report Summary and Recommendations
- G. Consulting Parties' Correspondence





- APE
- Survey Area

Figure 1. Project area on portions of the 1994 Shelbyville, IN, and 1988 Lewis Creek, IN, 7.5 Minute Quadrangle  
 Miller Street and McKay Road  
 Intersection Improvement  
 Shelbyville, Shelby County, Indiana  
 Des. No. 1702775  
 Metric Project No. 18-0147  
 Map Date: 08/12/2019

All Locations Approximate  
 1994 and 1988 Basemaps



Meters  
 0 240 480  
 1 cm = 240 m







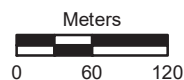
- APE
- Survey Area
- Arc Loc

Figure 2. Project area on an aerial photograph  
Miller Street and McKay Road  
Intersection Improvement  
Shelbyville, Shelby County, Indiana  
Des. No. 1702775  
Metric Project No. 18-0147  
Map Date: 06/04/2020

All Locations Approximate  
2013 Basemap  
Service Layer Credits: Source: Esri, DigitalGlobe, GeoEye, Earthstar  
Geographics, CNES/Airbus DS, USDA, USGS, AeroGRID, IGN, and  
the GIS User Community



1 cm = 60 m







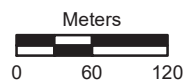
- |  |   |
|--|---|
| <ul style="list-style-type: none"> <li><span style="border: 2px solid red; display: inline-block; width: 20px; height: 10px; margin-right: 5px;"></span> APE</li> <li><span style="border: 2px solid blue; display: inline-block; width: 20px; height: 10px; margin-right: 5px;"></span> Survey Area</li> <li><span style="color: orange; font-size: 1.2em;">➤</span> Photograph Location</li> </ul> | <ul style="list-style-type: none"> <li><span style="background-color: yellow; border: 1px solid black; display: inline-block; width: 20px; height: 10px; margin-right: 5px;"></span> AL001</li> <li><span style="background-color: yellow; border: 1px solid black; display: inline-block; width: 20px; height: 10px; margin-right: 5px;"></span> AL002</li> <li><span style="background-color: yellow; border: 1px solid black; display: inline-block; width: 20px; height: 10px; margin-right: 5px;"></span> AL003</li> <li><span style="background-color: yellow; border: 1px solid black; display: inline-block; width: 20px; height: 10px; margin-right: 5px;"></span> AL004</li> <li><span style="background-color: yellow; border: 1px solid black; display: inline-block; width: 20px; height: 10px; margin-right: 5px;"></span> AL005</li> <li><span style="background-color: yellow; border: 1px solid black; display: inline-block; width: 20px; height: 10px; margin-right: 5px;"></span> AL006</li> <li><span style="background-color: yellow; border: 1px solid black; display: inline-block; width: 20px; height: 10px; margin-right: 5px;"></span> AL007</li> <li><span style="background-color: yellow; border: 1px solid black; display: inline-block; width: 20px; height: 10px; margin-right: 5px;"></span> AL008</li> <li><span style="background-color: yellow; border: 1px solid black; display: inline-block; width: 20px; height: 10px; margin-right: 5px;"></span> AL009</li> </ul> |
|--|---|

Figure 1. Photograph Locations on an aerial photograph  
Miller Street and McKay Road Intersection Improvement  
Shelbyville, Shelby County, Indiana  
Des. No. 1702775  
Metric Project No. 18-0147  
Map Date: 06/05/2020

All Locations Approximate  
2013 Basemap  
Service Layer Credits: Source: Esri, DigitalGlobe, GeoEye, Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AeroGRID, IGN, and the GIS User Community



1 cm = 60 m





## PROJECT SITE PHOTOGRAPHS



Photo 1: View along Miller St looking south toward the project area from the north end of the APE



Photo 2: Typical ranch style found in the APE; 1820 S Miller St facing NE (Non-contributing, or NC)



Photo 3: Typical ranch style found in the APE; 2002 S Miller St facing SE (AL-01 rated Contributing, or C)



Photo 4: 771 Doran Ave facing SE (AL-02 rated C)





Photo 5: 2024 S Miller St facing NE (AL-03 rated C)



Photo 6: 2024 1/2 S Miller St facing SE (AL-04 rated C)





Photo 7: 2040 S Miller St facing SE (AL-05 rated C)



Photo 8: 2054 S Miller St facing NE (AL-06 rated C)





Photo 9: 704 W McKay Rd facing NE (rated NC)



Photo 10: View along W McKay Rd facing west toward the project area from the east end of the APE



A - Photo 11: View along S Miller St facing SW from its intersection with W McKay Rd



Photo 12: View along W McKay Rd facing west from its intersection with S Miller St





Photo 13: View along S Miller St facing north toward the project area from the south end of the APE



Photo 14: Typical commercial building on the west side of S Miller Rd; 2315 S Miller St facing SW (rated NC)





Photo 15: South High Gardens Condominiums Complex, 901-997 W McKay Rd, facing SW (rated NC)



Photo 16: 1039 W McKay Rd facing south (AL-07 rated C)



Photo 17: 1101 W McKay Rd facing SW (AL-08 rated C)



Photo 18: View along W McKay Rd facing east toward the project area from the west end of the APE





Photo 19: East (main) entrance of Shelbyville High School (AL-09 rated C)



Photo 20: East (main) entrance of Shelbyville High School facing NW (AL-09 rated C)



Photo 21: West (rear) entrance of Shelbyville High School facing NE (AL-09 rated C)



Photo 22: Religious building, 1815 S Miller St, facing SW (rated NC)



# HISTORIC PROPERTY SHORT REPORT

## MILLER STREET/MCKAY ROAD INTERSECTION IMPROVEMENT PROJECT,

DES. No. 1702775

CITY OF SHELBYVILLE, ADDISON TOWNSHIP,  
SHELBY COUNTY, INDIANA

PREPARED FOR:

ZIMMER CONSTRUCTION ENGINEERING, LLC

LEAD AGENCY:

FEDERAL HIGHWAY ADMINISTRATION

Prepared by:



**Complex Environment. Creative Solutions.**

6971 Hillsdale Court  
Indianapolis, IN 46256  
Telephone: 317.400.1633  
[www.metricenv.com](http://www.metricenv.com)

*Candace Hudziak*

Candace Hudziak, M.A.

Principal Investigator

[candaceh@metricenv.com](mailto:candaceh@metricenv.com)

June 18, 2020

## **MANAGEMENT SUMMARY**

This report documents the identification and evaluation efforts for properties included in the Area of Potential Effects (APE) for the Miller Street/McKay Road Intersection Improvement Project. Above-ground resources located within the project APE were identified and evaluated in accordance with Section 106, National Historic Preservation Act (NHPA) of 1966, as amended, and the regulations implementing Section 106 (36 CFR Part 800).

As a result of the NHPA, as amended, and CFR Part 800, federal agencies are required to take into account the impact of federal undertakings upon historic properties in the area of the undertaking. Historic properties include buildings, structures, sites, objects, and/or districts that are eligible for or listed on the National Register of Historic Places (NRHP). As this project is receiving funding from the Federal Highway Administration (FHWA), it is subject to a Section 106 review.

The APE contains no properties listed on the NRHP. The APE contains no properties that are recommended eligible for listing in the NRHP.

## **5.0 CONCLUSION**

The APE contains no properties listed on the NRHP.

As a result of identification and evaluation efforts for this project, no properties are recommended eligible for listing on the NRHP.



*\*Note: The complete geophysical survey report was included as an appendix with the Phase Ia Archaeological Report (Snell 12/22/20)*

**Geophysical Survey of a Reported Unmarked Cemetery for Miller Street at McKay Road  
Intersection Improvement, Des. No. 1702775, Shelby County, Indiana**

**by**

**Matt Coon**

**INDOT Cultural Resources Office  
100 N Senate Avenue, N642  
Indianapolis, IN 46204  
317-233-2083**

**October 22, 2020**

the grave shaft fill and the intact surrounding soil, and the associated localized disruption in the natural stratigraphy within the shaft, may be detectable by GPR (Conyers 2006c). Grave shafts are sometimes observable in radar data as V-shaped features resulting from the intersections of two semiparabolas corresponding to the edges of the shaft, or as shallow U-shaped features possibly resulting from slumping of the fill.

Some graves simply may not exhibit a detectable geophysical signature, even if other nearby graves are easily detected. Detection of graves may be further hampered by bioturbation from roots and animals, which may make them less distinct, and living root systems associated with trees and other vegetation may tend to mask the signature of underlying grave shafts. Disturbance of the upper portion of the shaft and fill by plowing may also reduce the detectability of graves by near surface methods.

## SURVEY SETTING

The survey area is located in the southeast quadrant of the intersection of Miller Street and McKay Road (Figure 6 and Figure 7). The west end of the survey area is low and flat, and the area slopes upward to the east and southeast, with the highest elevations around the southeast corner (Figure 8-Figure 10). The east end of the survey area slopes downward toward a roadcut and sidewalk running along the south side of McKay Road. The steepness of the slope increases toward the east end of the area (Figure 11). A fence shown in the aerial photo has been removed, but a political campaign sign attached to metal fence posts was present in the southwest corner of the survey area. Utility poles with metal guy wires were present near the west end, and overhead power lines crossed the survey area.

Soils in the western portion of the survey area are classified as somewhat poorly drained Crosby silt loam, 0 to 2 percent slopes (CrA), which developed in a parent material of silt over loamy glacial till on till plains (United States Department of Agriculture, Natural Resources Conservation Service [USDA, NRCS] 2020) (Figure 6). Soils at the eastern end of the area are classified as well drained Hennepin loam, 25 to 50 percent slopes (HeF), a loamy soil which formed in glacial till on side slopes (USDA, NRCS 2020).

Soils such as this are expected to provide good results for a resistance survey, given adequate soil moisture. Due to the relatively low clay content they are moderately well suited for GPR survey, although the GPR Suitability Index for the survey area is mapped as Low (4) with depth of penetration for a 200 MHz antenna estimated to range from 0.25-2.0 m (Doolittle et al. 2007; USDA-NRCS 2009).

Data were collected within a 10 m by 50 m area covering most of the proposed R/W (Figure 6). A small amount of R/W tapers to the east of the survey area but consisted of steep slope and was therefore not surveyed (Figure 11). The 2005 aerial photograph shows an area of apparently recent disturbance at the northwest corner of the survey area (Figure 7). The steep slope is seen as an area of brown ground vegetation toward the eastern end and northeast corner of the survey area (Figure 7).

Assuming that the current topography has not changed significantly since the 19<sup>th</sup> century, the most likely cemetery location would be on the high rise to the southeast of the survey area. The low, somewhat

poorly drained western end and the sloping east end of the survey area would appear to have a low probability for the present of historical graves, with the possible exception of the extreme southeastern corner of the survey area.

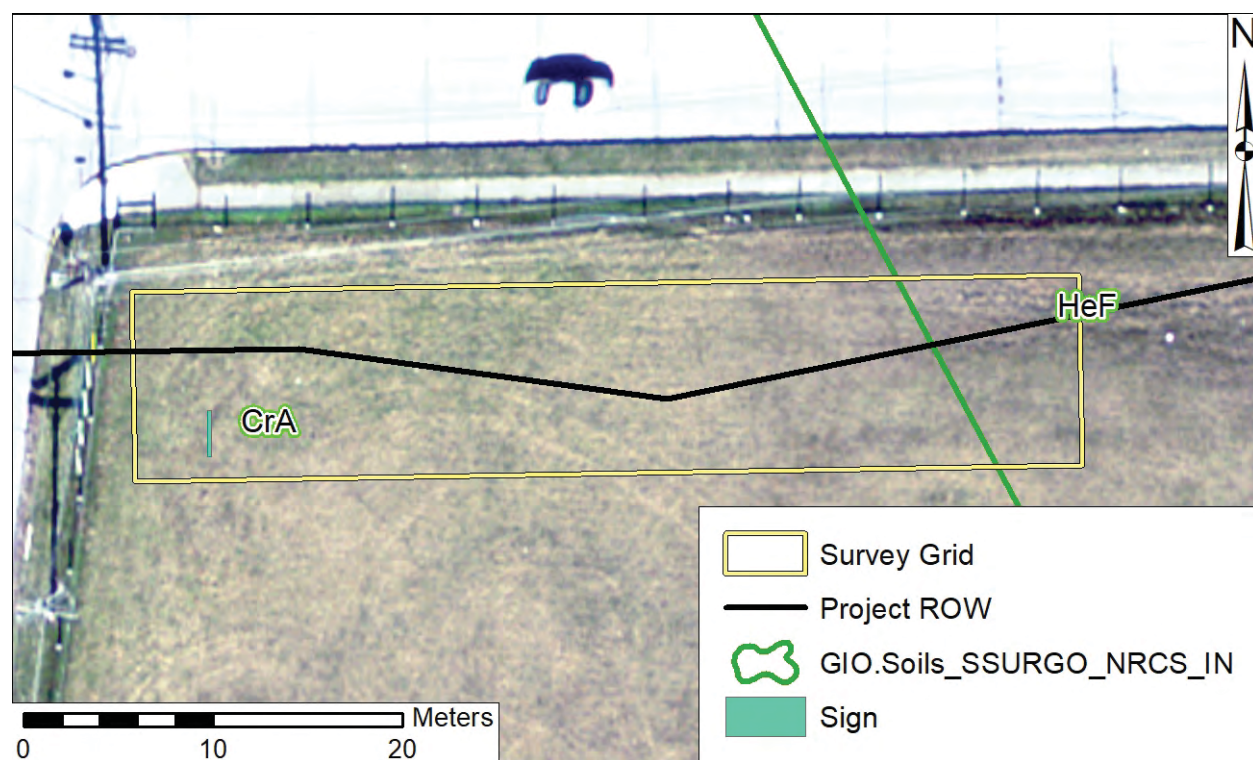


Figure 6. Portion of a 2012 aerial photograph showing the geophysical survey area, project R/W limits, and soil classifications.

## CONCLUSION

In summary, none of the methods employed during the survey revealed clear evidence for the presence of graves within the survey area. Potentially grave-like features that were initially identified appeared on closer inspection to be related to geological background variation or to unidentified modern disturbances. Based upon the topography and soils, the most likely location for a historic cemetery would be on the relative level, well drained high ground located outside of and to the southeast of the survey area.

As a general rule, interpretation of geophysical data is somewhat subjective. These interpretations are meant to guide or inform more traditional archaeological investigations, not to provide firm conclusions about the survey area or to demonstrate an absence of graves outside of the identified areas of interest. It is possible that graves are present that could not be detected by the geophysical instruments used within the survey area under the particular conditions prevailing at the time that the data were collected. In addition to investigating geophysical anomalies, it is always advisable for archaeological testing to sample other portions of a site, when possible.



# ARCHAEOLOGICAL SHORT REPORT

## PHASE IA ARCHAEOLOGICAL SURVEY FOR THE MILLER STREET/MCKAY ROAD ROUNDABOUT PROJECT, DES. No. 1702775 CITY OF SHELBYVILLE, ADDISON TOWNSHIP, SHELBY COUNTY, INDIANA

PREPARED FOR:

**ZIMMER CONSTRUCTION ENGINEERING, LLC**

LEAD AGENCY:

**FEDERAL HIGHWAY ADMINISTRATION**

Prepared by:



**Complex Environment. Creative Solutions.**

6971 Hillsdale Court  
Indianapolis, IN 46256  
Telephone: 317.400.1633  
[www.metricenv.com](http://www.metricenv.com)

A handwritten signature in black ink that reads "Samuel P. Snell".

---

Samuel P. Snell, MS, RPA  
Archaeological Principal Investigator  
[sams@metricenv.com](mailto:sams@metricenv.com)  
December 22, 2020



# INDIANA ARCHAEOLOGICAL SHORT REPORT

State Form 54566 (R / 9-20)

## INDIANA DEPARTMENT OF NATURAL RESOURCES DIVISION OF HISTORIC PRESERVATION AND ARCHAEOLOGY

402 West Washington Street, Room W274  
Indianapolis, Indiana 46204-2739  
Telephone Number: (317) 232-1646  
Fax Number: (317) 232-0693  
E-mail: [dhpa@dnr.IN.gov](mailto:dhpa@dnr.IN.gov)

Where applicable, the use of this form is recommended but not required by the Division of Historic Preservation and Archaeology (DHPA).

Name(s) of author(s) Samuel P. Snell, MS, RPA, and Megan Copenhaver		Date (month, day, year) December 22, 2020
Title of project Phase IA Archaeological Survey for the Miller Street/McKay Road Roundabout Project (Des. No. 1702775), City of Shelbyville, Addison Township, Shelby County, Indiana		
This document is being used to report on the results of: <input type="checkbox"/> Records check only <input checked="" type="checkbox"/> Records check and Phase Ia archaeological reconnaissance <input type="checkbox"/> An addendum to a previous archaeological report. For an addendum, provide the following information.		
Name(s) of author(s) of previous report		
Title of previous report		
Date of previous report (month, day, year)	DHPA number 24936	

PROJECT OVERVIEW			
Description of project The City of Shelbyville, with funding from the Federal Highway Administration and administrative oversight from the Indiana Department of Transportation (INDOT), intends to proceed with the construction of a roundabout at the intersection of Miller Street and McKay Road in the City of Shelbyville, Addison Township, Shelby County, Indiana.  The project proposes to upgrade the existing intersection to a single-lane roundabout. Additional items to be constructed as a part of the project include new sidewalks and cross walks, an enclosed drainage system with new storm inlets, curbs, and gutter. Additional permanent and temporary right-of-way will be required to construct the project. It is anticipated that 0.69 hectares (ha) (1.72 acres [ac]) of permanent right-of-way and 0.04 ha (0.10 ac) of temporary right-of-way will be needed.  The project extends 134 m (439.6 ft) north and 150 m (492.1 ft) south along Miller Street and 175 m (574.1 ft) west and 83 m (272.3 ft) east along McKay Road. The project encompasses a total of 1.4 ha (3.4 ac).			
INDOT designation number(s) 1702775	Project number 18-00147	DHPA number 24936	DHPA plan number
Prepared for: (Company / Institution / Agency) Zimmer Construction Engineering, LLC			
Name of contact Alan DeLaunay			
Address (number and street, city, state, and ZIP code) 4212 Southport Trace Drive, Indianapolis, Indiana 46237			
Telephone number (317)697-3486		E-mail address admarauder@sbcglobal.net	
Name of principal investigator Samuel P. Snell, MS, RPA			
Name of company / institution Metric Environmental			
Address (number and street, city, state, and ZIP code) 6971 Hillsdale Court, Indianapolis, IN 46250			
Telephone number (317)912-3466		E-mail address sams@metricenv.com	
Signature of principal investigator (Required)			Date (month, day, year)

PROJECT LOCATION						
County Shelby		USGS 7.5' series topographic quadrangle Shelbyville			Civil township Addison	
Legal Location						
Grid alignment						
1/4	1/4	1/4	1/4	Section	Township	Range

Describe disturbances. Attach photographs documenting disturbances.

#### Buried utilities, driveways, and construction

Actual area surveyed (hectares)

1.4

Actual area surveyed (acres)

3.4

Explain results of fieldwork.

The project is located at the intersection of Miller Street and McKay Road on the south side of the City of Shelbyville and encompasses a total of 1.4 ha (3.4 ac) (Figures 1 through 18). The project was surveyed as four individual survey areas corresponding to the four intersection quadrants. The survey areas were designated Areas 1, 2, 3, and 4 (Figures 5 through 18).

Area 1, the southwest quadrant of the intersection, consisted of a level grass yard (Figures 3 and 5 through 8). Area 1 was disturbed at the corner by utilities and sidewalk construction. There were marked utilities along the edge of Miller Street and McKay Road (Figures 3 and 5 through 6). A shallow drainage ditch was also present, running toward McKay Road at the western end of the area (Figure 8). A total of 19 STPs were excavated. All the STPs had a similar profile of dark grayish brown (10YR 4/2) to brown (10YR 4/3) clay loam extending 19 to 40 cm (7.5 to 5.7 in) below ground surface and underlain by a light yellowish brown (10YR 6/4) to yellowish brown (10YR 5/4) clay loam.

Area 2, the southeast quadrant of the intersection, was a mostly level grass covered field with the eastern end sloping down (Figures 3, and 9 through 11). Shanks Cemetery (CR-73-8) was recorded as being located within this quadrant. The Indiana Cemetery Survey Form (Money and Holmes n.d.) [Appendix B] and Find A Grave (2020) records the location of the cemetery being 200 yards southeast of the intersection and that the headstones were removed in 1928. The Cemetery Registry from the SHAARD Database, states that the exact location is not known. In October 2020, and as part of this project, the INDOT-CRO completed a geophysical survey of the area (Appendix A). The survey included both resistance and magnetic survey as well as ground penetrating radar. None of the methods employed during the survey revealed clear evidence for the presence of graves within the survey area. Potentially grave-like features that were initially identified appeared, on closer inspection, to be related to geological background variation or to unidentified modern disturbances. Based upon the topography and soils, the most likely location for a historic cemetery would be on the relatively level, well-drained, high ground located outside of and to the southeast of the survey area. Four STPs were excavated within the geophysical survey area and three STPs were excavated along Miller Street. All the STPs had a similar profile of dark grayish brown (10YR 4/2) to brown (10YR 4/3) clay loam extending 20 to 37 cm (7.9-14.6 in) below ground surface and underlain by a light yellowish brown (10YR 6/4) to yellowish brown (10YR 5/4) clay loam. No subsurface stains or cultural artifacts were encountered.

The likelihood that Shanks Cemetery (CR-73-8) is located within the southeast quadrant is considered to be very low to none. Based on this even minimal potential, the majority of the construction work has been shifted to the other quadrants of the intersection. Along Miller Street, the project will not impact the southeast quadrant except at the existing driveway. Along McKay Road, the project extends approximately 11 m (36 ft) south of the curb of McKay Road. In the unlikely event that archaeological deposits or human remains are encountered during the construction phase of the project, all work will cease and archaeologists from the Indiana Department of Natural Resource-Division of Historic Preservation and Archaeology and INDOT-CRO the Indiana Department of Transportation-Cultural Resources Section (INDOT-CRO) will be notified.

Area 3, the northeast quadrant of the intersection, consisted of a parking lot for an apartment complex along McKay Road and the yards along Miller Street (Figures 3, and 12 through 14). The majority of this area was disturbed by either a driveway or by buried utilities (Figure 3). One STP was excavated at the intersection in an undisturbed portion of a yard. The soil profile consisted of dark grayish brown (10YR 4/2) clay loam extending 34 cm (13.4 in) below ground surface and underlain by a light yellowish brown (10YR 6/4) clay loam.

Area 4, the northwest quadrant of the intersection, consisted of the grass yard of the Shelbyville High School (Figures 3, and 15 through 18). The corner was disturbed by intersection improvements, the installation of the high school sign, and electric power extending to the sign. Along McKay Road, there was a sidewalk and two areas of buried utilities. Twelve STPs were excavated in this area along three transects. Nine STPs contained similar profiles of dark grayish brown (10YR 4/2) to brown (10YR 4/3) clay loam extending 12 to 30 cm (4.7 to 11.8 in) below ground surface and underlain by a light yellowish brown (10YR 6/4) to yellowish brown (10YR 5/4) clay loam. Transect 2, STP 2 and Transect 3, STPs 4 and 5 were all disturbed and correspond to bare earth areas on the 2005 and 2006 Google Earth imagery. Transect 3, STP 5 contained gravel and rock and could not be fully excavated. The other two had similar profiles of mixed dark grayish brown (10YR 4/2) to brown (10YR 4/3) clay loam and yellowish brown (10YR 5/4) clay loam with gravels extending 24 to 30 cm (9.4 to 11.8 in) below ground surface and underlain by a light yellowish brown (10YR 6/4) to yellowish brown (10YR 5/4) clay loam. The north end of the area was disturbed by the school driveway and a sidewalk.

No archaeological resources were identified as a result of the survey and no further work is recommended for the project.

#### RECOMMENDATIONS

Records check (Check all that apply.)

- ☐ No archaeological investigation is recommended before the project is allowed to proceed because the records check has determined that the project area does not have the potential to contain archaeological resources.
- ☐ A Phase Ia archaeological reconnaissance is recommended.
- ☐ A cemetery development plan may be required under Indiana Code 14-21-1-26.5 because project ground disturbance will be within 100 feet of a cemetery.

Phase Ia archaeological reconnaissance (Check all that apply.)

- ☒ It is recommended that the project be allowed to proceed as planned because the Phase Ia archaeological reconnaissance has located no archaeological sites within the project area and/or previously recorded sites that were investigated warrant no additional investigation.
- ☐ It is recommended that Phase Ic archaeological subsurface reconnaissance be conducted before the project is allowed to proceed. The Phase Ia archaeological reconnaissance has determined that the project area includes landforms which have the potential to contain buried archaeological deposits.

Other recommendations / commitments

NA

**Pursuant to IC-14-21-1, if any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and 29) requires that the discovery must be reported to the Department of Natural Resources within two (2) business days. In that event, please call (317) 232-1646.**

REQUIRED ATTACHMENTS

- ☒ Figure showing project location within Indiana
- ☒ USGS topographic map showing the project area (1:24,000 scale)
- ☒ Aerial photograph showing the project area, land use and survey methods
- ☒ Photographs of the project area, including, if applicable, photographs documenting disturbances
- ☐ Project plans (if available)

Other attachments

Appendix A: INDOT-CRO Geophysical Survey of a Reported Unmarked Cemetery for Miller Street at McKay Road Intersection Improvement, Des. No. 1702775, Shelby County, Indiana.

References cited (See short report instructions for required references to be consulted.)

Beers, J. H., & Company

1880 Atlas of Shelby Co. Indiana. Unigraphic, Inc., Evansville, Indiana.

Coon, Matt

2007 An Archaeological Assessment for Pavement Resurfacing on SR 9 from South of SR 44 to Conrail Railroad (INDOT Des. No. 0600168) in Shelbyville, Indiana. Indiana Department of Transportation, Indianapolis. Prepared for the Indiana Department of Transportation.

2020 Geophysical Survey of a Reported Unmarked Cemetery for Miller Street at McKay Road Intersection Improvement, Des. No. 1702775, Shelby County, Indiana. Indiana Department of Transportation, Indianapolis. Prepared for Metric Environmental, LLC., Indianapolis.

Find A Grave

2020 Find A Grave, <https://www.findagrave.com/cemetery/2259686> accessed January 9, 2020.

Google Earth

2019 Google Earth Pro stand alone software accessed November 11, 2020.

Indiana Department of Natural Resources

2019 Indiana Historic Buildings, Bridges, and Cemeteries Map (<https://indnr.maps.com/home/item.html?id=1593429c17c34942a0d1d3fac03c4a80>) accessed November 11, 2020.

Indiana Geological & Water Survey

2020 Indiana Historical Aerial Photo Index, <https://igws.indiana.edu/IHAPI/Map/> accessed November 11, 2020.

Indiana Geographic Information Council

2020 Indiana Map, <https://maps.indiana.edu/> accessed November 11, 2020.

Indiana Highway Survey Commission





## INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue  
Room N642  
Indianapolis, Indiana 46204

PHONE: (317) 234-5168

**Eric Holcomb, Governor**  
**Joe McGuinness, Commissioner**

January 23, 2020

This letter was sent to the listed parties.

RE: Miller Street and McKay Road Intersection Improvement Des. No. 1702775, Shelbyville, Addison Township, Shelby County, Indiana.

Dear Consulting Party (see attached list),

The City of Shelbyville, with funding from the Federal Highway Administration and administrative oversight from the Indiana Department of Transportation (INDOT), proposes to proceed with the Miller Street and McKay Road Intersection Improvement Des. No. 1702775, Shelbyville, Addison Township, Shelby County, Indiana. Metric Environmental, LLC., is under contract with Zimmer Construction Engineering, LLC. on behalf of the City of Shelbyville, to advance the environmental documentation for the referenced project.

This letter is part of the early coordination phase of the environmental review process requesting comments associated with this project. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

The proposed undertaking is at the intersection of Miller Street and McKay Road in Shelby County, Indiana. It is within the City of Shelbyville, Addison Township, Shelbyville, IN, 7.5' USGS Topographic Quadrangle, in Section 7, Township 12N, Range 7E. The existing intersection is a 4-way stop controlled intersection.

The primary purpose of the project is to provide an improved intersection that meets current and projected traffic capacity and provides upgraded pedestrian facilities. The project is needed due to the increased traffic volumes along three of the four legs of the intersection. Traffic safety is a concern during the peak travel periods in the morning and afternoons due to the proximity of the Shelbyville high school, middle school and elementary school, which are all located within a three-block radius of the intersection. This results in a large volume of school buses utilizing the intersection concurrently with local traffic, which results in congestion and delays. Compounding the traffic concerns is the large number of students who walk to school and cross the intersection to access the adjacent sidewalks. Pedestrian safety is a primary concern because there are no designated crosswalks, and two quadrants of the intersection lack sidewalks and crosswalks entirely. A secondary safety issue affects the vehicles traveling west from SR 9 to SR 44 via McKay Road. This leg has a substantial grade of 6+% entering the intersection. Vehicles that stop in rain, snow and ice have difficulty restarting when positioned on the grade.

The preliminary preferred alternate will upgrade the existing intersection to a single-lane roundabout. Additional items to be constructed as part of the project include an enclosed storm drainage system with new storm inlets. New sidewalks and crosswalks will be constructed as well as new curb and gutter throughout the intersection. All sidewalk ramps will be constructed in accordance with Americans with Disabilities Act (ADA) requirements. Additional permanent and temporary right-of-way will be required to construct the project. The exact amount is not yet known and will be developed as the design advances.

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic and archaeological properties. In accordance with 36 CFR 800.2 (c), you are hereby requested to be a consulting party to participate in the Section 106 process. Entities that have been invited to participate in the Section 106 consultation process for this project are identified in the attached list. Per 36 CFR 800.3(f), we hereby request that the Indiana State Historic Preservation Officer (SHPO) notify this office if the SHPO staff is aware of any other parties that may be entitled to be consulting parties or should be contacted as potential consulting parties for the project.

The Section 106 process involves efforts to identify historic properties potentially affected by the undertaking, assess its effects and seek ways to avoid, minimize or mitigate any adverse effects on historic properties. For more information regarding the protection of historic resources, please see the Advisory Council on Historic Preservation's guide: *Protecting Historic Properties: A Citizen's Guide to Section 106 Review* available online at <https://www.achp.gov/sites/default/files/documents/2017-01/CitizenGuide.pdf>.

The Area of Potential Effects (APE) is the area in which the proposed project may cause alterations in the character or use of historic resources. At this time, no cultural resource investigations have occurred; however, the results of cultural resource identification and evaluation efforts, both above-ground and archaeological, will be forthcoming. Consulting parties will receive notification when these reports are completed.

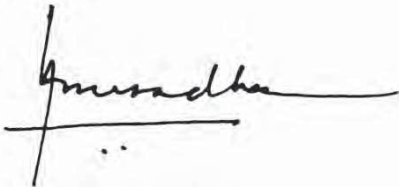
Please review the information and comment within thirty (30) calendar days of receipt. If you indicate that you do not desire to be a consulting party, or if you do not respond, you will not be included on the list of consulting parties for this project. If we do not receive your response in the time allotted, the project will proceed consistent with the proposed design and you will not receive further information about the project unless the design changes.

For questions concerning specific project details, you may contact Samuel P. Snell of Metric Environmental, LLC., at 317-912-3499 or [sams@Metricenv.com](mailto:sams@Metricenv.com). All future responses regarding the proposed project should be forwarded to Metric Environmental, LLC. at the following address:

Samuel P. Snell  
Archaeological Principal Investigator  
Metric Environmental, LLC.  
6971 Hillsdale Court  
Indianapolis, Indiana 46250  
[sams@metricenv.com](mailto:sams@metricenv.com)

Tribal contacts may contact Shaun Miller at [smiller@indot.in.gov](mailto:smiller@indot.in.gov) or 317-233-6795 or Michelle Allen at FHWA at [michelle.allen@dot.gov](mailto:michelle.allen@dot.gov) or 317-226-7344.

Sincerely,



Anuradha V. Kumar, Manager  
Cultural Resources Office  
Environmental Services

Enclosures:  
Location Map  
Aerial Map

Distribution List:

State Historic Preservation Officer	Indiana Landmarks, Central Regional Office
Shelby County Historical Society	Indianapolis Metropolitan Planning Organization
Joseph Boggs Society for Historical Preservation	Shelby County Commissioners
Shelbyville Mayor	Shelbyville City Council President
Department Head: Genealogy and History, Shelby County Public Library	
Eastern Shawnee Tribe of Oklahoma	Miami Tribe of Oklahoma
Peoria Tribe of Indians of Oklahoma	Pokagon Band of Potawatomi Indians

**From:** Sam Snell [mailto:sams@metricenv.com]

**Sent:** Thursday, January 23, 2020 1:16 PM

**To:** mdollase@indianalandmarks.org; BoggsSociety@gmail.com; BoggsSociety@gmail.com; director@grovermuseum.org; Anna.gremling@indy.gov; Kevin.nigh@co.shelby.in.us; Don.parker@co.shelby.in.us; Chris.ross@co.shelby.in.us; mayor@cityofshelbyvillein.com; rrolley@cityofshelbyvillein.com; Donna Dennison

**Cc:** Ross, Anthony

**Subject:** FHWA Project: Des. No. 1702775; Miller Street and McKay Road Intersection Improvement, Shelbyville, Addison Township, Shelby County, Indiana

**Des. No.: 1702775**

**Project Description:** Intersection Improvement at Miller Street and McKay Road

**Location:**

The City of Shelbyville, with funding from the Federal Highway Administration and administrative oversight from the Indiana Department of Transportation, proposes to proceed with the Miller Street and McKay Road Intersection Improvement Des. No. 1702775, Shelbyville, Addison Township, Shelby County, Indiana.

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic properties. The following agencies/individuals are being invited to become consulting parties:

State Historic Preservation Officer

Shelby County Historical Society

Shelby County Commissioners

Shelbyville Mayor

Department Head: Genealogy and History, Shelby County Public Library

Eastern Shawnee Tribe of Oklahoma

Peoria Tribe of Indians of Oklahoma

Indiana Landmarks, Central Regional Office

Indianapolis Metropolitan Planning Organization

Joseph Boggs Society for Historical Preservation

Shelbyville City Council President

Miami Tribe of Oklahoma

Pokagon Band of Potawatomi Indians

This letter is part of the early coordination phase of the environmental review process requesting comments associated with this project. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

Please review the attached letter, which is also located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with your comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comments. If we do not receive a response from an invited consulting party within the time allotted, the project will proceed consistent with the proposed design. **Therefore, if we do not receive a response within thirty (30) days, your agency or organization will not receive any further information on the project unless the scope of work changes.**

Tribal contacts may contact Shaun Miller at [smiller@indot.in.gov](mailto:smiller@indot.in.gov) or 317-233-6795 or Michelle Allen at FHWA at [michelle.allen@dot.gov](mailto:michelle.allen@dot.gov) or 317-226-7344.

Thank you in advance for your input,

**Samuel P. Snell, MS, RPA**  
Archaeological Principal Investigator



Phone: 317.912.3499 Email: [sams@metricenv.com](mailto:sams@metricenv.com)

6971 Hillsdale Court, Indianapolis, IN 46250



[www.metricenv.com](http://www.metricenv.com)

**Complex Environment. Creative Solutions.**

Certified DBE/MBE/SBE

INDIANAPOLIS | GARY | CINCINNATI

**please consider the environment before printing this e-mail**

"Notice: If you are not the intended recipient of this E-mail, be aware that any disclosure, copying, distribution, or use of this e-mail and/or any attachments is prohibited. If you have received this e-mail in error, please notify the sender immediately and delete this copy and any attachments hereto from your system. Thank you for your anticipated cooperation."



From: [Ross, Anthony](#)  
To: [thpo@estoo.net](#); "[dhunter@miamination.com](#)"; [lpappenfort@peoriatribes.com](#); [Matthew Bussler \(Matthew.Bussler@pokagonband-nsn.gov\)](#)  
Cc: [Sam Snell](#); [Miller, Shaun \(INDOT\)](#); [Allen, Michelle \(FHWA\)](#); [Branigin, Susan](#); [Kumar, Anuradha](#); [Matt House](#); [Blake, Mark](#)  
Subject: FHWA Project: Des. No. 1702775; Miller Street and McKay Road Intersection Improvement, Shelbyville, Addison Township, Shelby County, Indiana  
Date: Thursday, January 23, 2020 1:41:02 PM  
Attachments: [image001.png](#)  
[image002.png](#)  
[image003.png](#)  
[image004.png](#)  
[image006.png](#)  
[MillerStreetMcKayRoad\\_Des1702775\\_EarlyCoordination\\_2020-01-23.pdf](#)

---

**External Message:** *This message originated outside of Metric Environmental.  
Do not click links or open attachments unless you recognize the sender and know the content is safe.*

**Des. No.: 1702775**

**Project Description:** Intersection Improvement at Miller Street and McKay Road

**Location:** Shelbyville, Shelby County, Indiana

The City of Shelbyville, with funding from the Federal Highway Administration and administrative oversight from the Indiana Department of Transportation, proposes to proceed with the Miller Street and McKay Road Intersection Improvement Des. No. 1702775, Shelbyville, Addison Township, Shelby County, Indiana.

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic properties. The following agencies/individuals are being invited to become consulting parties:

State Historic Preservation Officer	Indiana Landmarks, Central Regional Office
Shelby County Historical Society	Indianapolis Metropolitan Planning Organization
Shelby County Commissioners	Joseph Boggs Society for Historical Preservation
Shelbyville Mayor	Shelbyville City Council President
Department Head: Genealogy and History, Shelby County Public Library	
Eastern Shawnee Tribe of Oklahoma	Miami Tribe of Oklahoma
Peoria Tribe of Indians of Oklahoma	Pokagon Band of Potawatomi Indians

This letter is part of the early coordination phase of the environmental review process requesting comments associated with this project. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

Please review the attached letter, which is also located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with your comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comments. If we do not receive a response from an invited consulting party within the time allotted, the project will proceed consistent with the proposed design. **Therefore, if we do not receive a response within thirty (30) days, your agency or organization will not receive any further information on the project unless the scope of work changes.**

Tribal contacts may contact Shaun Miller at [smiller@indot.in.gov](mailto:smiller@indot.in.gov) or 317-233-6795 or Michelle Allen at FHWA at [michelle.allen@dot.gov](mailto:michelle.allen@dot.gov) or 317-226-7344.

Thank you in advance for your input,

From: [Donna Dennison](#)  
To: [Sam Snell](#)  
Cc: [mdollase@indianalandmarks.org](#); [BoggsSociety@gmail.com](#); [BoggsSociety@gmail.com](#);  
[director@grovermuseum.org](#); [Anna.gremling@indy.gov](#); [Kevin.nigh@co.shelby.in.us](#);  
[Don.parker@co.shelby.in.us](#); [Chris.ross@co.shelby.in.us](#); [mayor@cityofshelbyvillein.com](#);  
[rnolley@cityofshelbyvillein.com](#); [Matt House](#)  
Subject: RE: FHWA Project: Des. No. 1702775; Miller Street and McKay Road Intersection Improvement, Shelbyville,  
Addison Township, Shelby County, Indiana  
Date: Friday, January 24, 2020 11:52:33 AM  
Attachments: [image001.png](#)  
[image002.png](#)  
[image003.png](#)  
[image004.png](#)  
[image005.png](#)

---

Hello, I am the Head of Genealogy at the Shelby County Public Library, I am also representing the Shelby County/Indiana Genealogical Society with this letter. Though firsthand accounts of several of our older residents it is known that there is a cemetery at the corner of Miller and McKay on the East side of the road. We have done extensive research and the Cemetery is listed on the DNRs listing of Cemeteries. It is call the Shank Cemetery. Upon further research we have found at least 12 bodies that we know are there. One man, Jacob Shank, was the original owner of the property in acquiring the deed in 1832. He was one of the founding fathers of our town, a town sheriff, built many of our buildings including our first jail and courthouse, he was on the first library board, was a State Representatives along with many other documented accomplishment in our city. We would like to see and are working toward getting this site declared a Pioneer Cemetery not a roundabout.

I have a very large packet of information we have compiled about the Cemetery and the Shank family that I would be very willing to share if you would like to see it. I look forward to working with you on this project.

From: Sam Snell <mailto:sams@metricenv.com>  
Sent: Friday, January 24, 2020 12:44 PM  
To: Donna Dennison  
Subject: RE: FHWA Project: Des. No. 1702775; Miller Street and McKay Road Intersection Improvement, Shelbyville, Addison Township, Shelby County, Indiana

Hello Ms. Dennison,

Thank you for replying to our Early Coordination email. Matt House had informed us of the Cemetery location and mentioned that you have met people that remember seeing the cemetery. I have done a cursory review of some historic maps, SHAARD, and some historic aerials. The Find-a-Grave website mentioned it being 200 yards southeast of the intersection. We appreciate that opportunity to look at the information you have put together. If your information is electronic and too large to email, we are able to set up a file transfer site so you can share the information with us.

As the Section 106 process proceeds we will keep in contact with you.

Thank you once again for accepting our invitation to be a Consulting Party for this project.

Sam

**Samuel P. Snell, MS, RPA**

Archaeological Principal Investigator

**Metric Environmental, LLC**

Phone: 317.912.3499

Email: [sams@metricenv.com](mailto:sams@metricenv.com)

**From:** [Donna Dennison](#)  
**To:** [Sam Snell](#)  
**Cc:** [mdollase@indianalandmarks.org](mailto:mdollase@indianalandmarks.org); [BoggsSociety@gmail.com](mailto:BoggsSociety@gmail.com); [BoggsSociety@gmail.com](mailto:BoggsSociety@gmail.com); [director@grovermuseum.org](mailto:director@grovermuseum.org); [Anna.gremling@indy.gov](mailto:Anna.gremling@indy.gov); [Kevin.nigh@co.shelby.in.us](mailto:Kevin.nigh@co.shelby.in.us); [Don.parker@co.shelby.in.us](mailto:Don.parker@co.shelby.in.us); [Chris.ross@co.shelby.in.us](mailto:Chris.ross@co.shelby.in.us); [mayor@cityofshelbyvillein.com](mailto:mayor@cityofshelbyvillein.com); [rnolley@cityofshelbyvillein.com](mailto:rnolley@cityofshelbyvillein.com); [Matt House](#)  
**Subject:** RE: FHWA Project: Des. No. 1702775; Miller Street and McKay Road Intersection Improvement, Shelbyville, Addison Township, Shelby County, Indiana  
**Date:** Saturday, January 25, 2020 11:53:32 AM  
**Attachments:** image001.png  
image002.png  
image003.png  
image004.png  
image005.png  
Shank Cemetery.pdf

---

Thank you Sam, I have attached maps and basic info about Mr. Shank for you. We have a lot more information here than this but thought we would start with the basics. I have contacted Jennie Regan-Dinius with the DNR to see what we can do to preserve the cemetery while also helping with the progress of the roundabout. Also the cemetery is on private property and we have talked to the present landowners and he stated that he had no idea it was there. Matt and I worked together on another project and I contacted him because I didn't want them to start until they were fully aware of what they were going to find there. He's been great!

The Addison Times is fully aware of what is going on, Christian and I talk a couple of times a week. I have requested that we keep this quiet for now. My goal for going to Matt was not to raise a ruckus or public outcry but to save a final resting place of one of Shelbyville's founding fathers and his family. I'm hoping by working together we can do both.



From: Sam Snell <sams@metricenv.com>  
Date: 1/27/20 7:52 AM (GMT-05:00)  
To: Donna Dennison <ddennison@sscpl.lib.in.us>  
Cc: Matt House <mhouse@cityofshelbyvillein.com>, Luella Beth Hillen <bethh@metricenv.com>, Elayna Stoner <elaynas@metricenv.com>  
Subject: RE: FHWA Project: Des. No. 1702775; Miller Street and McKay Road Intersection Improvement, Shelbyville, Addison Township, Shelby County, Indiana

Good morning Donna,

Thank you for the great information. I had been curious if the current landowner had any ideas about the location of the cemetery.

I have reworked the email addresses in this chain so that the rest of the contacts aren't bombarded with emails. I have kept Matt and I have added Luella Beth Hillen; our Director of NEPA Services, and Elayna Stoner; our project manager who is also going to be completing the Environmental Document for this project. I have passed on your previous emails so they are up to speed as well.

Thank you once again for the information and we will stay in touch as our process continues.

Sam

**Samuel P. Snell, MS, RPA**

Archaeological Principal Investigator

**Metric Environmental, LLC**

Phone: 317.912.3499

Email: [sams@metricenv.com](mailto:sams@metricenv.com)



Division of Historic Preservation & Archaeology · 402 W. Washington Street, W274 · Indianapolis, IN 46204-2739  
Phone 317-232-1646 · Fax 317-232-0693 · dhpa@dnr.IN.gov · www.IN.gov/dnr/historic



February 5, 2020

Samuel P. Snell  
Archaeological Principal Investigator  
Metric Environmental, LLC  
6971 Hillsdale Court  
Indianapolis, Indiana 46250

Federal Agency: Indiana Department of Transportation ("INDOT"),  
on behalf of Federal Highway Administration, Indiana Division ("FHWA")

Re: Early coordination letter regarding Miller Street and McKay Road intersection improvement project in  
Shelbyville, Addison Township, Shelby County, Indiana (Des. No. 1702775; DHPA No. 24936)

Dear Mr. Snell:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), 36 C.F.R. Part 800, and the "Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana," the staff of the Indiana State Historic Preservation Officer ("Indiana SHPO" or "INDNR-DHPA") has reviewed your January 23, 2020 submission, which included INDOT's early coordination letter, all of which we received on January 27, 2020 for the aforementioned project.

We are not aware of any parties who should be invited to participate in the Section 106 consultation on this federal undertaking, beyond those whom INDOT already has invited. In your next regular correspondence on this project, please advise us as to which of the invited consulting parties has accepted the invitation.

We look forward to reviewing the proposed area of potential effects and the reports on investigations of above-ground cultural resources and archaeological resources that the early coordination letter indicated will be forthcoming.

The Indiana SHPO staff's archaeological reviewer for this project is Wade T. Tharp, and the structures reviewer is Danielle Kauffmann. In all future correspondence about the Miller Street and McKay Road intersection improvement project in Shelbyville, Addison Township, Shelby County, Indiana (Des. No. 1702775), please refer to DHPA No. 24936.

Very truly yours,

Beth K. McCord  
Deputy State Historic Preservation Officer

BKM:DMK:dmk

emc: Robert Dirks, PE, FHWA  
Anuradha Kumar, INDOT  
Shaun Miller, INDOT  
Susan Branigin, INDOT  
Shirley Clark, INDOT  
Anthony Ross, INDOT  
Samuel P. Snell, Metric Environmental, LLC  
Wade T. Tharp, INDNR-DHPA  
Danielle Kauffmann, INDNR-DHPA



INDIANA LANDMARKS

*Central Regional Office*

1201 Central Avenue, Indianapolis, IN 46202

317 639 4534 / 800 450 4534 / [www.indianalandmarks.org](http://www.indianalandmarks.org)

February 10, 2020

Samuel P. Snell  
Archaeological Principal Investigator  
Metric Environmental, LLC  
6971 Hillsdale Court  
Indianapolis, IN 46250

RE: Des. No. 1702775 – Miller Street & McKay Road Intersection Improvements, Shelby, Addison Township, Shelby County, Indiana

Dear Mr. Snell:

Thank you for the opportunity to comment on the above undertaking. Indiana Landmarks agrees to be a consulting party for this project. We are interested in learning more about the project as it progresses.

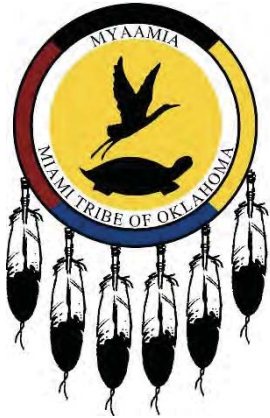
Per email correspondence that we received from Donna Dennison (Head of Genealogy at the Shelby County Public Library) on January 24-25, 2020, we are aware that there is a small cemetery on the east side of Miller Street & McKay Road, most likely within the Area of Potential Effects (APE). It is our understanding that the cemetery land was formerly owned by Jacob Shank, a prominent early Shelbyville citizen. Mr. Shank is also purportedly buried at the site. We request that further research be undertaken to learn more about the cemetery's prominence and history, and to determine if the site would be eligible for inclusion in the National Register of Historic Places.

Furthermore, as the project moves forward, we are interested to learn how appropriate parties will seek to avoid, minimize, or mitigate adverse effects to this historic site.

We appreciate your consideration and will look forward to remaining involved in the Section 106 process for this project.

Sincerely,

Joshua Biggs  
Community Preservation Specialist



## Miami Tribe of Oklahoma

3410 P St. NW, Miami, OK 74354 • P.O. Box 1326, Miami, OK 74355  
Ph: (918) 541-1300 • Fax: (918) 542-7260  
[www.miamination.com](http://www.miamination.com)



Via email: [smiller@indot.in.gov](mailto:smiller@indot.in.gov)

February 20, 2020

Shaun Miller  
Archaeological Team Lead, Cultural Resources Office  
Indiana DOT  
575 North Pennsylvania Street  
Indianapolis, IN 46204

Re: Des. No. 1702775; Miller Street and McKay Road Intersection Improvement, Shelbyville, Addison Township, Shelby County, Indiana – Comments of the Miami Tribe of Oklahoma

Dear Mr. Miller:

Aya, kikwehsitoole – I show you respect. My name is Diane Hunter, and I am the Tribal Historic Preservation Officer for the Federally Recognized Miami Tribe of Oklahoma. In this capacity, I am the Miami Tribe's point of contact for all Section 106 issues.

The Miami Tribe offers no objection to the above-referenced project at this time, as we are not currently aware of existing documentation directly linking a specific Miami cultural or historic site to the project site. However, as this project is within the aboriginal homelands of the Miami Tribe, if any human remains or Native American cultural items falling under the Native American Graves Protection and Repatriation Act (NAGPRA) or archaeological evidence is discovered during any phase of this project, the Miami Tribe requests immediate consultation with the entity of jurisdiction for the location of discovery. In such a case, please contact me at 918-541-8966 or by email at [dhunter@miamination.com](mailto:dhunter@miamination.com) to initiate consultation.

The Miami Tribe accepts the invitation to serve as a consulting party to the proposed project. In my capacity as Tribal Historic Preservation Officer I am the point of contact for consultation.

Respectfully,

Diane Hunter  
Tribal Historic Preservation Officer





# INDIANA DEPARTMENT OF TRANSPORTATION

North State Avenue  
Room N  
Indianapolis, Indiana

NE: -

**Eric Holcomb, Governor**  
**Joe McGuinness, Commissioner**

July 8, 2020

This letter was sent to the listed parties.

RE: Miller Street/McKay Road Intersection Improvement Project  
City of Shelbyville, Shelby County, Indiana  
Des. No. 1702775

Dear Consulting Party,

The City of Shelbyville, with funding from the Federal Highway Administration and administrative oversight from the Indiana Department of Transportation (INDOT), proposes to proceed with the Miller Street and McKay Road Intersection Improvement Project (Des. No. 1702775).

This letter is part of the Section 106 review process for this project. Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic and archaeological properties. We are requesting comments from you regarding the possible effects of this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

A Section 106 early coordination letter was distributed on January 23, 2020. On February 10, 2020 Joshua Biggs of Indiana Landmarks replied that their organization would like to participate as a consulting party. Mr. Biggs also stated that, via correspondence with Donna Dennison, they have been made aware of the possible burial location for Jacob Shank within the APE, and are interested in what further research will reveal regarding the former Shanks Cemetery (please refer to the attachments for a copy of this letter).

The proposed undertaking is located in southwest Shelbyville, Addison Township, in central Shelby County, Indiana. The project area can be found on the *Shelbyville Indiana* USGS Topographic Quadrangle, in Section 7, Township 12 North, Range 7 East.

Miller Street is a north/south roadway classified as a Minor Arterial with one 15ft. travel lane in each direction. A sidewalk is located on the west side of the street. McKay Road is an east/west roadway classified as a Minor Arterial with one 15ft. travel lane in each direction. A sidewalk is located on the north side of the street. Neither road has a dedicated turn lane in any direction for travel. The intersection is controlled by a four-way stop sign.

The primary purpose of the project is to provide an improved intersection that meets current and projected traffic capacity and provides upgraded pedestrian facilities. The project is needed due to the current and projected traffic volumes, and lack of adequate pedestrian facilities.

Traffic congestion is a concern during the peak travel times in the morning and afternoons due to the proximity of the Shelbyville school facilities located immediately west of the intersection. The high volume of school buses, student and staff traffic combined with local through traffic all utilizing the intersection concurrently, results in congestion and delays during peak travel times during the morning and late afternoons.

An additional need for the project is a result of the large number of students who walk to school and cross the intersection to access the school campus. Pedestrian safety is a primary concern because there are two quadrants of the intersection that lack sidewalks and cross walks entirely.

A secondary safety issue effects the vehicles traveling westbound from SR 9 to SR 44 via McKay Road. The east leg of McKay Road has a substantial grade of 6+% entering the intersection. During inclement weather conditions, the steep grade creates difficult conditions for westbound vehicles stopping and restarting on the grade.

The preferred alternate will upgrade the existing intersection to a single-lane roundabout. Additional items to be constructed as a part of the project include new sidewalks and cross walks, an enclosed drainage system with new storm inlets, curbs and gutter. All sidewalks and ramp systems will be constructed in compliance with ADA (Americans with Disabilities Act) design parameters.

Additional permanent and temporary right-of-way will be required to construct the project. It is anticipated that 1.72 acres of permanent right-of-way and 0.10 acre of temporary right-of-way will be needed. The maintenance of traffic plan will close McKay Road and detour traffic while constructing the intersection proper and connecting the new approaches into McKay Road. The approaches connecting South Miller Street will be constructed near the existing roadway during this phase; however, the connections to the existing road will occur in Phase 2 of construction and will result in some partial closures and lane restrictions.

Zimmer Construction Engineering, LLC is under contract with the City of Shelbyville to advance the environmental documentation for the referenced project. Metric Environmental, LLC (above-ground and archaeology) has been subcontracted to complete the Section 106 documentation for the project.

In accordance with 36 CFR 800.2 (c), you were invited to become a consulting party as part of the Section 106 process, or you are hereby invited to become a consulting party as part of the Section 106 process. Entities that have previously accepted consulting party status--as well as additional entities that are currently being invited to become consulting parties--are identified in the attached list.

The Section 106 process involves efforts to identify historic properties potentially affected by the undertaking, to assess the undertaking's effects and to seek ways to avoid, minimize, or mitigate any adverse effects on historic properties. For more information regarding the protection of historic resources, please see the Advisory Council on Historic Preservation's guide: *Protecting Historic Properties: Citizen's Guide to Section 106* available online at <https://www.achp.gov/sites/default/files/documents/2017-01/CitizenGuide.pdf>.

The Area of Potential Effects (APE) is the area in which the proposed project may cause alterations in the character or use of historic resources. The APE contains no resources listed in the National Register of Historic Places (NRHP).

A historian who meets the Secretary of the Interior's Professional Qualification Standards identified and evaluated above-ground resources within the APE for potential eligibility for the NRHP. As a result of the historic property identification and evaluation efforts, no resources are recommended as eligible for listing in the NRHP.

An archaeologist who meets the Secretary of the Interior's Professional Qualification Standards is conducting a survey of archaeological resources within the APE for potential eligibility for listing in the NRHP. A report of that investigation is forthcoming and will be distributed to the appropriate consulting parties for review at a later date.

The Historic Property Report and is available for review in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE). You are invited to review these documents and to respond with comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome

your related opinions and other input to be considered in the preparation of the environmental document. If you prefer a hard-copy of this material, please respond to this email with your request within seven (7) days.

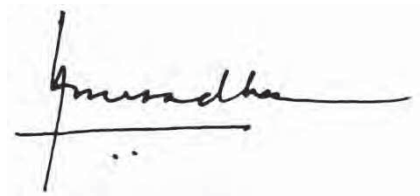
Please review the information and comment within thirty (30) calendar days of receipt. If you indicate that you do not desire to be a consulting party or if you have not previously accepted consulting party status and you do not respond to this letter, you will not be included on the list of consulting parties for this project and will not receive further information about the project unless the design changes.

For questions concerning specific project details, you may contact Elayna Stoner at Metric Environmental at 317-605-2736 or [elaynas@Metricenv.com](mailto:elaynas@Metricenv.com). All future responses regarding the proposed project should be forwarded to Metric Environmental at the following address:

Ms. Elayna Stoner  
Metric Environmental  
6971 Hillsdale Court  
Indianapolis, Indiana 46250

Tribal contacts may contact Shaun Miller at [smiller@indot.in.gov](mailto:smiller@indot.in.gov) or 317-233-6795 or Michelle Allen at FHWA at [michelle.allen@dot.gov](mailto:michelle.allen@dot.gov) or 317-226-7344.

Sincerely,



Anuradha V. Kumar, Manager  
Cultural Resources Office  
Environmental Services

cc: File No. 18-0147

Distribution List (in addition to the IN SHPO):

Miami Tribe of Oklahoma

Donna Dennison  
Dept Head for Genealogy and History  
Shelby County Public Library  
57 Broadway St  
Shelbyville, IN 49176  
[ddennison@sscpl.lib.in.us](mailto:ddennison@sscpl.lib.in.us)

Joshua Biggs  
Community Preservation Specialist  
Indiana Landmarks, Central Regional Office  
1201 Central Avenue  
Indianapolis, IN 46202  
[jbiggs@indianalandmarks.org](mailto:jbiggs@indianalandmarks.org)

From: [Candace Hudziak](#)  
To: [Kumar, Anuradha](#); [Branigin, Susan](#); [Ross, Anthony](#); [Joshua Biggs](#); [ddennison@sscpl.lib.in.us](#); [dkauffmann@dnr.in.gov](#)  
Cc: [Miller, Shaun \(INDOT\)](#); [Jella Beth Hillen](#); [Elayna Stoner](#); [Sam Snell](#); [wtharp1@dnr.in.gov](#)  
Subject: FHWA Project: Des. No. 1702775; Miller Street McKay Road Intersection Improvement Project, City of Shelbyville, Shelby County, Indiana  
Date: Wednesday, July 1, 2020 3:12:00 PM  
Attachments: [MillerMcKayProject\\_Des1702775\\_HPRDist\\_etter\\_2020-07-01.pdf](#)

---

**Des. No.: 1702775**

**Project Description: Miller Street/McKay Road Intersection Improvement Project**

**Location: City of Shelbyville, Addison Township, Shelby County, Indiana**

The City of Shelbyville, with funding from the Federal Highway Administration and administrative oversight from the Indiana Department of Transportation, proposes to proceed with an intersection improvement project at Miller Street and McKay Road (Des. No. 1702775). The Section 106 Early Coordination Letter for this project was originally distributed on January 23, 2020.

As part of Section 106 of the National Historic Preservation Act, a Historic Property Report has been prepared and is ready for review and comment by consulting parties.

Please review this documentation located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with any comments that you may have. If a hard copy of the materials is needed, please respond to this email with your request as soon as you can.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. Tribal consulting parties may enter the process at any time and are encouraged to respond to this notification with any comments or concerns at their earliest convenience. Tribal contacts may contact Shaun Miller at [smiller@indot.in.gov](mailto:smiller@indot.in.gov) or 317-233-6795 or Michelle Allen at FHWA at [michelle.allen@dot.gov](mailto:michelle.allen@dot.gov) or 317-226-7344.

Thank you in advance for your input,

**Candy Hudziak**

Senior Project Manager/Senior Scientist

**Metric Environmental, LLC**

Certified DBE/MBE/SBE Company

Phone: 317.400.1633

Mobile: 317.443.4123

Email: [candaceh@metricenv.com](mailto:candaceh@metricenv.com)



From: [Ross, Anthony](#)  
To: [Candace Hudjak](#)  
Subject: FW: FHWA Project: Des. No. 1702775; Miller Street McKay Road Intersection Improvement Project, City of Shelbyville, Shelby County, Indiana  
Date: Thursday, July 9, 2020 8:56:4 AM  
Attachments: [MillerMcKayProject\\_Des1702775\\_HPRDist\\_etter\\_2020-07-09.pdf](#)

---

**External Message:** *This message originated outside of Metric Environmental.  
Do not click links or open attachments unless you recognize the sender and know the content is safe.*

---

**From:** Miller, Shaun (INDOT) <[smiller@indot.IN.gov](mailto:smiller@indot.IN.gov)>  
**Sent:** Thursday, July 09, 2020 8:53 AM  
**To:** Diane Hunter <[dhunter@miamination.com](mailto:dhunter@miamination.com)>  
**Cc:** Ross, Anthony <[ARoss3@indot.IN.gov](mailto:ARoss3@indot.IN.gov)>  
**Subject:** FW: FHWA Project: Des. No. 1702775; Miller Street/McKay Road Intersection Improvement Project, City of Shelbyville, Shelby County, Indiana

**Des. No.: 1702775**

**Project Description: Miller Street/McKay Road Intersection Improvement Project**

**Location: City of Shelbyville, Addison Township, Shelby County, Indiana**

The City of Shelbyville, with funding from the Federal Highway Administration and administrative oversight from the Indiana Department of Transportation, proposes to proceed with an intersection improvement project at Miller Street and McKay Road (Des. No. 1702775). The Section 106 Early Coordination Letter for this project was originally distributed on January 23, 2020.

As part of Section 106 of the National Historic Preservation Act, a Historic Property Report has been prepared and is ready for review and comment by consulting parties.

Please review this documentation located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with any comments that you may have. If a hard copy of the materials is needed, please respond to this email with your request as soon as you can.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. Tribal consulting parties may enter the process at any time and are encouraged to respond to this notification with any comments or concerns at their earliest convenience. Tribal contacts may contact Shaun Miller at [smiller@indot.in.gov](mailto:smiller@indot.in.gov) or 317-233-6795 or Michelle Allen at FHWA at [michelle.allen@dot.gov](mailto:michelle.allen@dot.gov) or 317-226-7344.

Thank you in advance for your input,

Shaun Miller  
INDOT, Cultural Resources Office  
Archaeology Team Lead  
(317)233-6795

From: [Candace Hudziak](#)  
To: [Candace Hudziak](#)  
Subject: FW: FHWA Project: Des. No. 1702775; Miller Street McKay Road Intersection Improvement Project, City of Shelbyville, Shelby County, Indiana  
Date: Monday, July 20, 2020 10:5 :00 AM  
Attachments: [MillerMcKayProject\\_Des1702775\\_HPRDist etter 2020-07-0 .pdf](#)

---

**From:** Candace Hudziak

**Sent:** Monday, July 20, 2020 10:56 AM

**To:** Kumar, Anuradha <[akumar@indot.IN.gov](mailto:akumar@indot.IN.gov)>; Branigin, Susan <[SBranchin@indot.IN.gov](mailto:SBranchin@indot.IN.gov)>; Ross, Anthony <[ARoss3@indot.IN.gov](mailto:ARoss3@indot.IN.gov)>; Joshua Biggs <[jbiggs@indianalandmarks.org](mailto:jbiggs@indianalandmarks.org)>; ddennison@sscpl.lib.in.us; dkauffmann@dnr.in.gov

**Cc:** Miller, Shaun (INDOT) <[smiller@indot.IN.gov](mailto:smiller@indot.IN.gov)>; Luella Beth Hillen <[bethh@metricenv.com](mailto:bethh@metricenv.com)>; Elayna Stoner <[elaynas@metricenv.com](mailto:elaynas@metricenv.com)>; Sam Snell <[sams@metricenv.com](mailto:sams@metricenv.com)>; wtharp1@dnr.in.gov

**Subject:** RE: FHWA Project: Des. No. 1702775; Miller Street/McKay Road Intersection Improvement Project, City of Shelbyville, Shelby County, Indiana

Good morning,

Attached please find the revised distribution letter that was sent with the Historic Property Report, which you were notified was available for review on IN SCOPE on July 8, 2020. The original distribution letter failed to include the attachments. This version of the letter (attached, and available via IN SCOPE at <http://erms.indot.in.gov/Section106Documents/>) includes the attachments. My apologies for any confusion this may have caused. Please feel free to contact me with any questions regarding this document.

Thank you,

Candy Hudziak

---

**From:** Candace Hudziak

**Sent:** Wednesday, July 8, 2020 3:12 PM

**To:** Kumar, Anuradha <[akumar@indot.IN.gov](mailto:akumar@indot.IN.gov)>; Branigin, Susan <[SBranchin@indot.IN.gov](mailto:SBranchin@indot.IN.gov)>; Ross, Anthony <[ARoss3@indot.IN.gov](mailto:ARoss3@indot.IN.gov)>; Joshua Biggs <[jbiggs@indianalandmarks.org](mailto:jbiggs@indianalandmarks.org)>; ddennison@sscpl.lib.in.us; dkauffmann@dnr.in.gov

**Cc:** Miller, Shaun (INDOT) <[smiller@indot.IN.gov](mailto:smiller@indot.IN.gov)>; Luella Beth Hillen <[bethh@metricenv.com](mailto:bethh@metricenv.com)>; Elayna Stoner <[elaynas@metricenv.com](mailto:elaynas@metricenv.com)>; Sam Snell <[sams@metricenv.com](mailto:sams@metricenv.com)>; wtharp1@dnr.in.gov

**Subject:** FHWA Project: Des. No. 1702775; Miller Street/McKay Road Intersection Improvement Project, City of Shelbyville, Shelby County, Indiana

**Des. No.: 1702775**

**Project Description:** Miller Street/McKay Road Intersection Improvement Project

**Location:** City of Shelbyville, Addison Township, Shelby County, Indiana

The City of Shelbyville, with funding from the Federal Highway Administration and

administrative oversight from the Indiana Department of Transportation, proposes to proceed with an intersection improvement project at Miller Street and McKay Road (Des. No. 1702775). The Section 106 Early Coordination Letter for this project was originally distributed on January 23, 2020.

As part of Section 106 of the National Historic Preservation Act, a Historic Property Report has been prepared and is ready for review and comment by consulting parties.

Please review this documentation located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with any comments that you may have. If a hard copy of the materials is needed, please respond to this email with your request as soon as you can.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. Tribal consulting parties may enter the process at any time and are encouraged to respond to this notification with any comments or concerns at their earliest convenience. Tribal contacts may contact Shaun Miller at [smiller@indot.in.gov](mailto:smiller@indot.in.gov) or 317-233-6795 or Michelle Allen at FHWA at [michelle.allen@dot.gov](mailto:michelle.allen@dot.gov) or 317-226-7344.

Thank you in advance for your input,

**Candy Hudziak**

Senior Project Manager/Senior Scientist

**Metric Environmental, LLC**

Certified DBE/MBE/SBE Company

Phone: 317.400.1633

Mobile: 317.443.4123

Email: [candaceh@metricenv.com](mailto:candaceh@metricenv.com)



Division of Historic Preservation & Archaeology · 402 W. Washington Street, W274 · Indianapolis, IN 46204-2739  
Phone 317-232-1646 · Fax 317-232-0693 · [dhpa@dnr.IN.gov](mailto:dhpa@dnr.IN.gov) · [www.IN.gov/dnr/historic](http://www.IN.gov/dnr/historic)



July 20, 2020

Elayna Stoner  
Metric Environmental  
6971 Hillsdale Court  
Indianapolis, Indiana 46250

Federal Agency: Indiana Department of Transportation (“INDOT”),  
on behalf of Federal Highway Administration, Indiana Division (“FHWA”)

Re: Historic property short report (Hudziak, 6/18/2020) regarding the Miller Street and McKay Road  
intersection improvement project in Shelbyville, Addison Township, Shelby County, Indiana  
(Des. No. 1702775; DHPA No. 24936)

Dear Ms. Stoner:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), 36 C.F.R. Part 800, and the “Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana,” the staff of the Indiana State Historic Preservation Officer (“Indiana SHPO” or “INDNR-DHPA”) has reviewed your July 8, 2020 submission, which enclosed the historic property short report (“HPSR”; Hudziak, 6/18/2020) for the aforementioned project in Shelby County.

The proposed area of potential effects (“APE”) appears to be of adequate size to encompass the geographic area in which direct and indirect effects of a project of this nature could occur.

For the purposes of the Section 106 review of this federal undertaking, we agree with the conclusions of the HPSR that there are no historic properties listed or eligible for inclusion in the National Register of Historic Places within the project’s APE. We note that the three enclosures listed in INDOT’s July 8, 2020 cover letter were not included in the hard copy submission, nor the copy uploaded to IN SCOPE.

As indicated in INDOT’s letter, we note that a report of archaeological investigations will be forthcoming. We also note that the HPSR indicates this archaeological report will include information on any potential research regarding the Shanks Cemetery. We look forward to reviewing and commenting on that report.

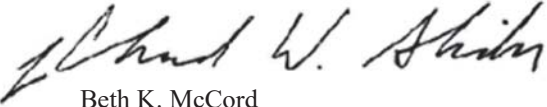
If any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29) requires that the discovery be reported to INDNR-DHPA within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29 does not obviate the need to adhere to applicable federal statutes and regulations, including but not limited to 36 C.F.R. Part 800.

The Indiana SHPO staff’s archaeological reviewer for this project is Wade T. Tharp, and the structures reviewer is Danielle Kauffmann. However, if you have a question about the Section 106 process, please contact initially the INDOT Cultural Resources staff members who are assigned to this project.



In all future correspondence about the Miller Street and McKay Road intersection improvement project in Shelbyville, Shelby County (Des. No. 1702775), please continue to refer to DHPA No. 24936.

Very truly yours,

A handwritten signature in black ink, appearing to read "Beth K. McCord". The signature is fluid and cursive, with the first name "Beth" being more prominent.

Beth K. McCord  
Deputy State Historic Preservation Officer

BKM:DMK:dmk

emc: Robert Dirks, PE, FHWA  
Anuradha Kumar, INDOT  
Shaun Miller, INDOT  
Anthony Ross, INDOT  
Susan Branigin, INDOT  
Elayna Stoner, Metric Environmental  
Candance Hudziak, Metric Environmental  
Donna Dennison, Shelby County Public Library  
Joshua Biggs, Indiana Landmarks, Central Regional Office  
Danielle Kauffmann, INDNR-DHPA  
Wade T. Tharp, INDNR-DHPA

**From:** Donna Dennison <ddennison@sscpl.lib.in.us>

**Sent:** Tuesday, September 1, 2020 8:18 AM

**To:** Candace Hudziak <candaceh@metricenv.com>

**Subject:** RE: FHWA Project: Des. No. 1702775; Miller Street/McKay Road Intersection Improvement Project, City of Shelbyville, Shelby County, Indiana

Good Morning, I, Donna Dennison, Head of Genealogy and History at the Shelby County Public Library and Vice President of the Shelby County Genealogical Society would like to remain a consultant for this project. I just found this email in my "Junk" and I hope I am not too late for this. The Shank Cemetery, and founding father of Shelbyville Jacob Shank is on the corner of this property and while I am very supportive of this project I do not want to see another pioneer Cemetery paved over. Thank you for including me in this project.

*Donna J Dennison  
Head of Genealogy and History  
Shelby County Public Library  
52 W Broadway St.  
Shelbyville IN 46176  
317-392-8144 ext 302*

From: [Candace Hudziak](#)  
To: [Donna Dennison](#)  
Cc: [Ross, Anthony](#); [Sam Snell](#); [Miller, Shaun \(INDOT\)](#); [Elayna Stoner](#)  
Subject: RE: FHWA Project: Des. No. 1702775; Miller Street McKay Road Intersection Improvement Project, City of Shelbyville, Shelby County, Indiana  
Date: Tuesday, September 1, 2020 :03:00 AM

---

Hello Ms. Dennison.

Thank you for your interest, and no, it's not too late to participate. Your comments below have been included in the Section 106 documentation for this project, and you'll be included in all formal correspondence and notifications going forward.

Take care,

**Candy Hudziak**  
Senior Project Manager

**Metric Environmental, LLC**  
Certified DBE/MBE/SBE Company  
Phone: 317.400.1633  
Mobile: 317.443.4123  
Email: [candaceh@metricenv.com](mailto:candaceh@metricenv.com)

From: [Sam Snell](#)  
To: [Kumar, Anuradha](#); [Ross, Anthony](#); [Branigin, Susan](#); [Branigin, Susan](#); [Donna Dennison](#); [jbiggs@indianalandmarks.org](#); [Kauffmann, Danielle M](#)  
Cc: [Coon, Matthew](#)  
Subject: FHWA Project: Des. No. 1702775; Miller Street McKay Road Intersection Improvement Project, City of Shelbyville, Shelby County, Indiana  
Date: Tuesday, December 22, 2020 12:52:00 PM  
Attachments: [image001.png](#)  
[image002.png](#)  
[image003.png](#)  
[image004.png](#)  
[image005.png](#)  
[MillerMcKayProject\\_Des1702775\\_ArchDist\\_etter\\_2020-12-22.pdf](#)

---

**Des. No.: 1702775**

**Project Description: Miller Street/McKay Road Intersection Improvement Project**  
**Location: City of Shelbyville, Addison Township, Shelby County, Indiana**

The City of Shelbyville, with funding from the Federal Highway Administration and administrative oversight from the Indiana Department of Transportation, proposes to proceed with an intersection improvement project at Miller Street and McKay Road (Des. No. 1702775). The Section 106 Early Coordination Letter for this project was originally distributed on January 23, 2020.

As part of Section 106 of the National Historic Preservation Act, an Archaeology Report has been prepared and is ready for review and comment by consulting parties.

Please review this documentation located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with any comments that you may have. If a hard copy of the materials is needed, please respond to this email with your request as soon as you can.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. Tribal consulting parties may enter the process at any time and are encouraged to respond to this notification with any comments or concerns at their earliest convenience. Tribal contacts may contact Shaun Miller at [smiller@indot.in.gov](mailto:smiller@indot.in.gov) or 317-233-6795 or Michelle Allen at FHWA at [michelle.allen@dot.gov](mailto:michelle.allen@dot.gov) or 317-226-7344.

Thank you in advance for your input,

**Samuel P. Snell, MS, RPA**  
Senior Project Manager/  
Archaeological Principal Investigator



Phone: 317.912.3499 Email: [sams@metricenv.com](mailto:sams@metricenv.com)  
6971 Hillsdale Court, Indianapolis, IN 46250



[www.metricenv.com](http://www.metricenv.com)

**Complex Environment. Creative Solutions.**

Certified DBE/MBE/SBE

INDIANAPOLIS | GARY | CINCINNATI

please consider the environment before printing this e-mail

"Notice: If you are not the intended recipient of this E-mail, be aware that any disclosure, copying, distribution, or use of this e-mail and/or any



From: [Coon, Matthew](#)  
To: [Diane Hunter](#)  
Cc: [Carmany- eorge, Karstin \(FHWA\)](#); [Sam Snell](#); [Ross, Anthony](#); [Miller, Shaun \(INDOT\)](#)  
Subject: FHWA Project: Des. No. 1702775; Miller Street McKay Road Intersection Improvement Project, City of Shelbyville, Shelby County, Indiana  
Date: Tuesday, December 22, 2020 1:03:00 PM  
Attachments: [MillerMcKayProject\\_Des1702775\\_ArchDist etter\\_2020-12-22.pdf](#)

---

**External Message:** *This message originated outside of Metric Environmental.  
Do not click links or open attachments unless you recognize the sender and know the content is safe.*

**Des. No.: 1702775**

**Project Description: Miller Street/McKay Road Intersection Improvement Project**

**Location: City of Shelbyville, Addison Township, Shelby County, Indiana**

The City of Shelbyville, with funding from the Federal Highway Administration (FHWA) and administrative oversight from the Indiana Department of Transportation (INDOT), proposes to proceed with an intersection improvement project at Miller Street and McKay Road (Des. No. 1702775). The Section 106 Early Coordination Letter for this project was originally distributed on January 23, 2020.

As part of Section 106 of the National Historic Preservation Act, an Archaeology Report has been prepared and is ready for review and comment by consulting parties.

Please review this documentation located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with any comments that you may have. If a hard copy of the materials is needed, please respond to this email with your request as soon as you can.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. Tribal consulting parties may enter the process at any time and are encouraged to respond to this notification with any comments or concerns at their earliest convenience. Tribal contacts may contact Shaun Miller at [smiller@indot.in.gov](mailto:smiller@indot.in.gov) or 317-416-0876 or Kari Carmany-George at FHWA at [k.carmanygeorge@dot.gov](mailto:k.carmanygeorge@dot.gov) or 317-226-5629.

Thank you in advance for your input,

**Matt Coon**

**Archaeologist, Cultural Resources Office**

Indiana Department of Transportation  
100 North Senate Ave., **N758-Environmental Services**  
Indianapolis, IN 46204  
**Phone: 317-697-9752**

From: [Donna Dennison](#)  
To: [Sam Snell](#)  
Subject: RE: FHWA Project: Des. No. 1702775; Miller Street McKay Road Intersection Improvement Project, City of Shelbyville, Shelby County, Indiana  
Date: Monday, January 4, 2021 :0 :3 AM  
Attachments: [image001.png](#)  
[image002.png](#)  
[image003.png](#)  
[image004.png](#)  
[image005.png](#)

---

Good Morning, Please continue to include me as a consulting party for this project. While the Shank Cemetery was not found in the proposed work area we know that is close so I would like to continue being part of this project.

Thank you!

*Donna J Dennison  
Head of Genealogy and History  
Shelby County Public Library  
52 W Broadway St.  
Shelbyville IN 46176  
317-398-8144 ext 302*

Division of Historic Preservation & Archaeology · 402 W. Washington Street, W274 · Indianapolis, IN 46204-2739  
Phone 317-232-1646 · Fax 317-232-0693 · dhpa@dnr.IN.gov · www.IN.gov/dnr/historic



January 21, 2021

Samuel P. Snell  
Metric Environmental, LLC  
6971 Hillsdale Court  
Indianapolis, Indiana 46250

Federal Agency: Indiana Department of Transportation (“INDOT”),  
on behalf of Federal Highway Administration, Indiana Division (“FHWA”)

Re: Indiana archaeological short report (Snell/Copenhaver, 12/22/2020) regarding the Miller Street  
and McKay Road intersection improvement project in the City of Shelbyville, Addison Township,  
Shelby County, Indiana (Des. No. 1702775; DHPA No. 24936)

Dear Mr. Snell:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), 36 C.F.R. Part 800, and the “Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana,” the staff of the Indiana State Historic Preservation Officer (“Indiana SHPO”) has reviewed your December 22, 2020, submission, which enclosed the Indiana archaeological short report (Snell/Copenhaver, 12/22/2020) for the aforementioned project in Shelby County, Indiana.

As previously indicated, for the purposes of the Section 106 review of this federal undertaking, we agree with the conclusions of the historic property short report that there are no historic properties listed or eligible for inclusion in the National Register of Historic Places (“NRHP”) within the project’s APE.

Additionally, based on the submitted information and the documentation available to the staff of the Indiana SHPO, with the exception of the reported location of the Shanks Cemetery (CR-73-8 in the DHPA SHAARD database system), we have not identified any currently known archaeological resources listed in or eligible for inclusion in the “NRHP”) within the proposed project area. It is our understanding that the conclusion of the INDOT’s geophysical investigations is that the boundaries of the Shanks Cemetery (CR-73-8) lie outside of the proposed project area. We concur with the opinion of the archaeologist, as expressed in the Indiana archaeological short report (Snell/Copenhaver, 12/22/2020), that no further archaeological investigations appear necessary at the proposed project area, with the condition that, should the boundaries of the proposed project area be altered to include additional areas within the southeastern quadrant of the intersection, then additional archaeological investigations and/or archaeological monitoring may be required. In such a case, submission of such information to Indiana DNR-DHPA staff for review and comment would be required.

As a reminder, if any portion of the proposed project area is within 100 feet of a cemetery, then a cemetery development plan may be necessary under IC 14-21-1-26.5. The aforementioned cemetery must be avoided by all project activities, and provisions of relevant state statutes regarding cemeteries (including IC 14-21-1 and IC 23-14) must be adhered to. Please also be aware of Indiana Code 23-14-44-1 and Indiana Code 23-14-44-2, regarding restrictions on roads and utility construction in cemeteries.

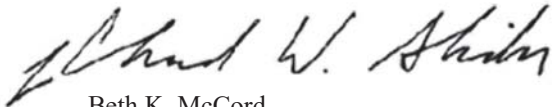
If any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29) requires that the discovery be reported to the Indiana Department of Natural Resources, Division of Historic Preservation and Archaeology (“IDNR-DHPA”) within

two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29 does not obviate the need to adhere to applicable federal statutes and regulations, including but not limited to 36 C.F.R. Part 800.

The Indiana SHPO staff's archaeological reviewer for this project is Wade T. Tharp, and the structures reviewer is Danielle Kauffmann. However, if you have a question about the Section 106 process, please contact initially the INDOT Cultural Resources staff members who are assigned to this project.

In all future correspondence about the Miller Street and McKay Road intersection improvement project in Shelbyville, Shelby County (Des. No. 1702775), please continue to refer to DHPA No. 24936.

Very truly yours,



Beth K. McCord  
Deputy State Historic Preservation Officer

BKM:DMK:WTT:wt

emc: Robert Dirks, PE, FHWA  
Anuradha Kumar, INDOT  
Shaun Miller, INDOT  
Anthony Ross, INDOT  
Susan Branigin, INDOT  
Samuel P. Snell, Metric Environmental, LLC  
Donna Dennison, Shelby County Public Library  
Joshua Biggs, Indiana Landmarks, Central Regional Office  
Danielle Kauffmann, Indiana DNR-DHPA  
Wade T. Tharp, Indiana DNR-DHPA



Division of Historic Preservation & Archaeology · 402 W. Washington Street, W274 · Indianapolis, IN 46204-2739  
Phone 317-232-1646 · Fax 317-232-0693 · [dhpa@dnr.IN.gov](mailto:dhpa@dnr.IN.gov) · [www.IN.gov/dnr/historic](http://www.IN.gov/dnr/historic)



March 15, 2021

Candace Hudziak  
Metric Environmental, LLC  
6971 Hillsdale Court  
Indianapolis, Indiana 46250

Federal Agency: Indiana Department of Transportation (“INDOT”),  
on behalf of Federal Highway Administration, Indiana Division (“FHWA”)

Re: Indiana Department of Transportation’s finding of “No Historic Properties Affected” on behalf  
of the Federal Highway Administration regarding the Miller Street and McKay Road  
intersection improvement project in Shelbyville, Addison Township, Shelby County, Indiana  
(Des. No. 1702775; DHPA No. 24936)

Dear Ms. Hudziak:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), implementing regulations at 36 C.F.R. Part 800, and the “Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program in the State of Indiana,” the staff of the Indiana State Historic Preservation Officer (“Indiana SHPO”) has reviewed your February 25, 2021 submission which enclosed INDOT’s finding and documentation for the aforementioned project, received by our office the same day.

As previously indicated, for the purposes of the Section 106 review of this federal undertaking, we agree that there are no historic properties listed or eligible for inclusion in the National Register of Historic Places (“NRHP”) within this project’s area of potential effects.

Additionally, as previously indicated, based on the submitted information and the documentation available to the staff of the Indiana SHPO, with the exception of the reported location of the Shanks Cemetery (CR-73-8 in the DHPA SHAARD database system), we have not identified any currently known archaeological resources listed in or eligible for inclusion in the NRHP within the proposed project area. It is our understanding that the conclusion of the INDOT’s geophysical investigations is that the boundaries of the Shanks Cemetery (CR-73-8) lie outside of the proposed project area. We concur with the opinion of the archaeologist as expressed in the Indiana archaeological short report (Snell/Copenhaver, 12/22/2020) that no further archaeological investigations appear necessary at the proposed project area, with the condition that, should the boundaries of the proposed project area be altered to include additional areas within the southeastern quadrant of the intersection, then additional archaeological investigations and/or archaeological monitoring may be required. In such a case, submission of such information to Indiana SHPO staff for review and comment would be required.

Accordingly, we concur with INDOT’s February 24, 2021, Section 106 finding of “No Historic Properties Affected” on behalf of FHWA for this federal undertaking.

If any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and -29) requires that the discovery be reported to the Indiana SHPO within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and -29 does not obviate the need to adhere to applicable federal statutes and regulations, including but not limited to 36 C.F.R. Part 800.

The archaeological reviewer for this project on the Indiana SHPO staff is Wade T. Tharp and the structures reviewer is Danielle Kauffmann. However, if you have questions about our comments or about the review process, please contact initially the INDOT Cultural Resource Office staff members assigned to this project.

In any future correspondence regarding the Miller Street and McKay Road intersection project in Shelby County, (Des. No. 1702775), please continue to refer to DHPA No. 24936.

Very truly yours,

A handwritten signature in black ink, appearing to read "Beth K. McCord".

Beth K. McCord  
Deputy State Historic Preservation Officer

BKM:DMK:dmk

emc: Robert Dirks, PE, FHWA  
Anuradha Kumar, INDOT  
Susan Branigin, INDOT  
Shaun Miller, INDOT  
Anthony Ross, INDOT  
Candace Hudziak, Metric Environmental, LLC  
Samuel P. Snell, Metric Environmental, LLC  
Donna Dennison, Shelby County Public Library  
Joshua Biggs, Indiana Landmarks, Central Regional Office  
Diane Hunter, Miami Tribe of Oklahoma  
Wade T. Tharp, DNR-DHPA  
Danielle Kauffmann, DNR-DHPA

**From:** [Coon, Matthew](#)  
**To:** [Diane Hunter](#)  
**Cc:** [Carmany-George, Karstin \(FHWA\)](#); [Miller, Shaun \(INDOT\)](#); [Ross, Anthony](#); [Karen Garrard](#)  
**Subject:** FHWA Project: Des. No. 1702775; Miller Street/McKay Road Intersection Improvement Project, City of Shelbyville, Addison Township, Shelby County, Indiana  
**Date:** Thursday, February 25, 2021 2:08:30 PM  
**Attachments:** [MillerMcKayProject\\_Des1702775\\_DistLtrFinding 2021-02-25.pdf](#)

---

**External Message:** *This message originated outside of Metric Environmental.  
Do not click links or open attachments unless you recognize the sender and know the content is safe.*

**Des. No.: 1702775/DHPA No. 24936**

**Project Description: Miller Street/McKay Road Intersection Improvement Project**

**Location: City of Shelbyville, Addison Township, Shelby County, Indiana**

The City of Shelbyville, with funding from the Federal Highway Administration (FHWA) and administrative oversight from the Indiana Department of Transportation (INDOT), proposes to proceed with the Miller Street/McKay Road Intersection Improvement Project Des. No. 1702775, in the City of Shelbyville, Addison Township, Shelby County, Indiana.

INDOT, on behalf of FHWA, has signed a determination of “No Historic Properties Affected” for this Section 106 undertaking. In accordance with 36 CFR 800.4(d)(1), you and the other consulting parties that responded to the early coordination letter are being provided the documentation for this finding. You can view the determination of “No Historic Properties Affected” electronically by accessing INDOT’s Section 106 document posting website IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE). If a hard copy of the materials is needed, please respond to this email as soon as you can.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. Tribal consulting parties may enter the process at any time and are encouraged to respond to this notification with any comments or concerns at their earliest convenience. Tribal contacts may contact Shaun Miller at [smiller@indot.in.gov](mailto:smiller@indot.in.gov) or 317-416-0876 or Kari Carmany-George at FHWA at [K.CarmanyGeorge@dot.gov](mailto:K.CarmanyGeorge@dot.gov) or 317-226-5629.

Thank you in advance for your input,

**Matt Coon**

**Archaeologist, Cultural Resources Office**

Indiana Department of Transportation

100 North Senate Ave., **N758-Environmental Services**

Indianapolis, IN 46204

**Phone: 317-697-9752**

AFFP

Public Notice Des. No. 1702775

## Affidavit of Publication

STATE OF INDIANA }  
COUNTY OF SHELBY } SS

Hope Stevens, being duly sworn, says:

That she is Accounting Clerk of the The Shelbyville News, a daily newspaper of general circulation, printed and published in Shelbyville, Shelby County, Indiana; that the publication, a copy of which is attached hereto, was published in the said newspaper on the following dates:

March 12, 2021

Publisher's Fee: \$ 0.00

That said newspaper was regularly issued and circulated on those dates.

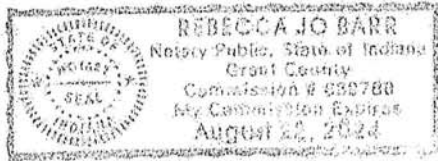
SIGNED:

*Hope Stevens*

Subscribed to and sworn to me this 12th day of March 2021.

*Rebecca Jo Barr*

Rebecca Jo Barr, Notary Public 08/22/2024



30198328 61180199

Public Notice  
Des. No. 1702775

The City of Shelbyville is planning to proceed with an intersection improvement project funded in part by the Federal Highway Administration (FHWA). The project is located at the intersection of Miller Street and McKay Road in the City of Shelbyville, Addison Township, Shelby County, Indiana.

The preferred alternative would upgrade the existing intersection to a single-lane roundabout. Additional items to be constructed as a part of the project include new sidewalks and cross walks, an enclosed drainage system with new storm inlets, curbs and gutter. An additional 1.72 acres of permanent right-of-way and 0.10 acre of temporary right-of-way will be needed.

The proposed action does not impact properties listed in or eligible for the National Register of Historic Places. INDOT, on behalf of the FHWA, has issued a "No Historic Properties Affected" finding for the project due to the fact that no historic properties are present within the Area of Potential Effects (APE). In accordance with the National Historic Preservation Act, the views of the public are being sought regarding the effect of the proposed project on the historic elements as per 36 CFR 800.2(d), 800.3(e) and 800.6(a)(4). The documentation can be viewed electronically by accessing INDOT's Section 106 document posting website IN SCOPE at <http://erms.indot.in.gov/Section106Documents>. Persons with limited internet access may request project information to be mailed to them. Please contact Sam Snell, phone -- 317-912-3499, email -- [sams@metricenv.com](mailto:sams@metricenv.com).

This documentation serves as the basis for the "No Historic Properties Affected" finding. The views of the public on this effect finding are being sought. Please reply with any comments to Sam Snell of Metric Environmental, LLC (317-912-3499, [sams@metricenv.com](mailto:sams@metricenv.com)) no later than April 11, 2021 to respond.

In accordance with the "Americans with Disabilities Act", if you have a disability for which the City of Shelbyville needs to provide accessibility to the document(s) such as interpreters or readers, please contact Matthew House, City of Shelbyville Engineer, City Hall, 44 W. Washington Street, Shelbyville, IN 46176, phone -- 317-392-5102, email -- [mhouse@cityofshelbyvillein.com](mailto:mhouse@cityofshelbyvillein.com).  
hspaxlp

Metric Environmental  
6971 Hillsdale Ct  
Indianapolis, IN 46250



# **APPENDIX E**

## **Red Flag and Hazardous Materials**



Date: May 15, 2020

To: Site Assessment & Management  
Environmental Policy Office - Environmental Services Division  
Indiana Department of Transportation  
100 N Senate Avenue, Room N642  
Indianapolis, IN 46204

From: Elayna Stoner  
Metric Environmental, LLC  
6971 Hillsdale Court  
Indianapolis, Indiana 46250  
elaynas@Metricenv.com

Re: RED FLAG INVESTIGATION  
DES # 1702775  
Intersection Improvement  
South Miller Street and West McKay Road  
City of Shelbyville, Shelby County, Indiana

#### PROJECT DESCRIPTION

The proposed intersection improvement project is located at the intersection of South Miller Street and West McKay Road in the City of Shelbyville. Specifically, the project is located in Sections 1 and 2, Township 11 North, Range 3 East as illustrated on the *Shelbyville, Indiana 7.5-minute United States Geological Survey (USGS) topographic quadrangle*.

The primary purpose of the project is to provide an improved intersection that meets current and projected traffic capacity and provides upgraded pedestrian facilities. The project is needed due to the increased traffic volumes along three of the four legs of the intersection. An additional need for the project is a result of the large number of students who walk to school and cross the intersection to access the adjacent sidewalks.

The preferred alternate will upgrade the existing intersection to a single-lane roundabout. Sidewalks and cross walks will be built to facilitate the safe movement of pedestrians. Additional permanent and temporary right-of-way will be required to construct the project. The exact amount will be established as the design process advances.

Bridge and/or Culvert Project: Yes ☐ No ☒ Structure # N/A

If this is a bridge project, is the bridge Historical? Yes ☐ No ☐ , Select ☐ Non-Select ☐

(Note: If the project involves a historical bridge, please include the bridge information in the Recommendations Section of the report).

Proposed right of way: Temporary ☒ # Acres 0.10 Permanent ☒ # Acres 1.5 Not Applicable ☐

Type of excavation: Excavation for the new roadway work and storm water inlets will extend approximately 2ft. to 4ft. in depth.

Maintenance of traffic: Traffic will be maintained as possible while the roundabout is built to the southwest of the existing intersection. During construction work to tie the legs of the roundabout into the existing roadways, lane restrictions and closures will require the use of a detour for a short duration of time.

Work in waterway: Yes ☐ No ☒ Below ordinary high water mark: Yes ☐ No ☐

State Project: ☐ LPA: ☒

Any other factors influencing recommendations: N/A

## INFRASTRUCTURE TABLE AND SUMMARY

<b>Infrastructure</b> Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Religious Facilities	<b>1*</b>	Recreational Facilities	<b>3</b>
Airports <sup>1</sup>	<b>N/A</b>	Pipelines	<b>3</b>
Cemeteries	<b>N/A</b>	Railroads	<b>1</b>
Hospitals	<b>N/A</b>	Trails	<b>4</b>
Schools	<b>7</b>	Managed Lands	<b>N/A</b>

<sup>1</sup>In order to complete the required airport review, a review of public airports within 3.8 miles (20,000 feet) is required.

### **Explanation:**

Religious Facilities\*: Although unmapped on the Indiana GIO Database, further desktop review revealed that one (1) religious facility is located within the 0.5 mile search radius. The facility, Shelbyville-Victory Fellowship, is located approximately 0.25 mile southwest of the project area. No impact is expected.

Schools: Seven (7) schools are located within the 0.5 mile search radius. Shelbyville High School is located northwest adjacent to the project area. Coordination with the Shelbyville Central School Corporation will occur.

Recreational Facilities: Three (3) recreational facilities are located within the 0.5 mile search radius. The nearest facility, the Shelbyville High School and Middle School campus is located adjacent to the project area. Coordination with the Shelbyville Central School Corporation will occur.

Pipelines: Three (3) pipeline segments are located within the 0.5 mile search radius. The nearest segment is located approximately 0.12 mile south of the project area. No impact is expected.

Railroads: One (1) railroad segment is located within the 0.5 mile search radius. The segment is located approximately 0.33 mile southeast of the project area. No impact is expected.

Trails: Four (4) trail segments are located within the 0.5 mile search radius. The nearest segment, a planned trail, is located within the project area. Coordination with Shelbyville Park and Recreation Department will occur.

## WATER RESOURCES TABLE AND SUMMARY

<b>Water Resources</b> Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
NWI - Points	<b>1</b>	Canal Routes - Historic	<b>N/A</b>
Karst Springs	<b>N/A</b>	NWI - Wetlands	<b>4</b>
Canal Structures – Historic	<b>N/A</b>	Lakes	<b>2</b>
NPS NRI Listed	<b>N/A</b>	Floodplain - DFIRM	<b>1</b>
NWI-Lines	<b>N/A</b>	Cave Entrance Density	<b>N/A</b>
IDEM 303d Listed Streams and Lakes (Impaired)	<b>N/A</b>	Sinkhole Areas	<b>N/A</b>
Rivers and Streams	<b>8</b>	Sinking-Stream Basins	<b>N/A</b>

### Explanation:

NWI - Points: One (1) NWI - Point is located within the 0.5 mile search radius. The feature is located approximately 0.33 mile north of the project area. No impact is expected.

Rivers and Streams: Eight (8) river/stream segments are located within the 0.5 mile search radius. The nearest segment is located approximately 0.14 mile east of the project area. No impact is expected.

NWI - Wetlands: Four (4) wetlands are located within the 0.5 mile search radius. The nearest feature is located approximately 0.23 mile northwest of the project area. No impact is expected.

Lakes: Two (2) lakes are located within the 0.5 mile search radius. The nearest feature is located approximately 0.22 mile northwest of the project area. No impact is expected.

Floodplain - DFIRM: One (1) floodplain polygon is located within the 0.5 mile search radius. The floodplain polygon is located approximately 0.08 mile east of the project area. No impact is expected.

## URBANIZED AREA BOUNDARY SUMMARY

### Explanation:

This project lies within the City of Shelbyville UAB. Post construction Storm Water Quality Best Management Practices (BMPs) may need to be considered. Coordination with the appropriate MS4 will be necessary

## MINING AND MINERAL EXPLORATION TABLE AND SUMMARY

<b>Mining/Mineral Exploration</b> Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Petroleum Wells	<b>N/A</b>	Mineral Resources	<b>N/A</b>
Mines – Surface	<b>N/A</b>	Mines – Underground	<b>N/A</b>

Explanation: No Mining/Mineral Exploration resources were identified within the 0.5 mile search radius.



## HAZARDOUS MATERIAL CONCERNS TABLE AND SUMMARY

<b>Hazardous Material Concerns</b> Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Superfund	N/A	Manufactured Gas Plant Sites	N/A
RCRA Generator/ TSD	N/A	Open Dump Waste Sites	N/A
RCRA Corrective Action Sites	N/A	Restricted Waste Sites	N/A
State Cleanup Sites	1	Waste Transfer Stations	N/A
Septage Waste Sites	N/A	Tire Waste Sites	N/A
Underground Storage Tank (UST) Sites	N/A	Confined Feeding Operations (CFO)	N/A
Voluntary Remediation Program	N/A	Brownfields	1
Construction Demolition Waste	N/A	Institutional Controls	N/A
Solid Waste Landfill	N/A	NPDES Facilities	1
Infectious/Medical Waste Sites	N/A	NPDES Pipe Locations	N/A
Leaking Underground Storage (LUST) Sites	N/A	Notice of Contamination Sites	N/A

### Explanation:

**State Cleanup Sites:** One (1) State Cleanup Site is located within the 0.5 mile search radius. The site, Blue River Career Center (AI ID#: 52175, Regulatory ID#: 200407017) is located approximately 0.46 mile north of the project area at 789 Saint Joseph Street. According to the Revised Remediation Work Plan dated August 2019, contaminants of concern (COCs) at the site include Tetrachloroethene (PCE), Trichloroethene (TCE), and Vinyl chloride (VC) in soil; benzene, PCE, TCE, and VC in groundwater; and benzene, ethylbenzene, and 1,2,4-trimethylbenzene for indoor air vapor. Impacted areas appear to be delineated to within the property, and groundwater was observed to be north/northeast away from the project area. No impact is expected.

**Brownfields:** One (1) brownfield site is located within the 0.5 mile search radius. The site, Blue River Career Center (AI ID#: 52175, Regulatory ID#: 4960017) is located approximately 0.46 mile north of the project area at 789 Saint Joseph Street. This site is also described in detail in the State Cleanup Site section of this document. No impact is expected.

**NPDES Facilities:** One (1) NPDES Facility is located within the 0.5 mile search radius. The facility, Shelbyville High School Football Facility (2003 South Miller Street, Permit#: INR10K568) is located 0.19 mile northwest of the project area. No impact is expected.

## **ECOLOGICAL INFORMATION SUMMARY**

The Shelby County listing of the Indiana Natural Heritage Data Center information on endangered, threatened, or rare (ETR) species and high-quality natural communities is attached with ETR species highlighted. A preliminary review of the Indiana Natural Heritage Database by INDOT Environmental Services did not indicate the presence of ETR species within the 0.5 mile search radius. Coordination with IDNR and USFWS will occur.

A review of the USFWS database did not indicate the presence of endangered bat species in or within 0.5 mile of the project area. The range-wide programmatic consultation for the Indiana Bat and Northern Long-eared Bat will be completed according to the most recent "Using the USFWS's IPaC System for Listed Bat Consultation for INDOT Projects".

An inquiry using the USFWS Information for Planning and Consulting (IPaC) website did not indicate that the presence of the federal endangered species, the Rusty Patched Bumble Bee, in or within 0.5 mile of the project area. No impact is expected.

#### **RECOMMENDATIONS SECTION**

Include recommendations from each section. If there are no recommendations, please indicate N/A:

##### **INFRASTRUCTURE:**

The Shelbyville High School is located northwest adjacent to the project area. Coordination with Shelbyville High School will occur.

One recreational facility, the Shelbyville Senior High School and Middle School, is located adjacent to the project area. Coordination with Shelbyville High School will occur.

One planned trail is located within the project area. Coordination with Shelbyville Park and Recreation Department will occur.

**WATER RESOURCES:** N/A

**URBANIZED AREA BOUNDARY:** This project lies within the Shelbyville UAB. Post construction Storm Water Quality Best Management Practices (BMPs) may need to be considered. Coordination with the appropriate MS4 will be necessary.

**MINING/MINERAL EXPLORATION:** N/A

**HAZARDOUS MATERIAL CONCERNS:** N/A

**ECOLOGICAL INFORMATION:** Coordination with IDNR and USFWS will occur. The range-wide programmatic consultation for the Indiana Bat and Northern Long-eared Bat will be completed according to the most recent "Using the USFWS's IPaC System for Listed Bat Consultation for INDOT Projects".

INDOT Environmental Services concurrence:

Nicole  
Fohey-  
Breting

Digitally signed by  
Nicole Fohey-Breting  
Date: 2020.06.25  
15:56:33 -04'00'

(Signature)

Prepared by:  
Elayna Stoner  
Metric Environmental

##### **Graphics:**

A map for each report section with a 0.5 mile search radius buffer around all project area(s) showing all items identified as possible items of concern is attached. If there is not a section map included, please change the YES to N/A:

**SITE LOCATION:** YES

**INFRASTRUCTURE:** YES

**WATER RESOURCES:** YES



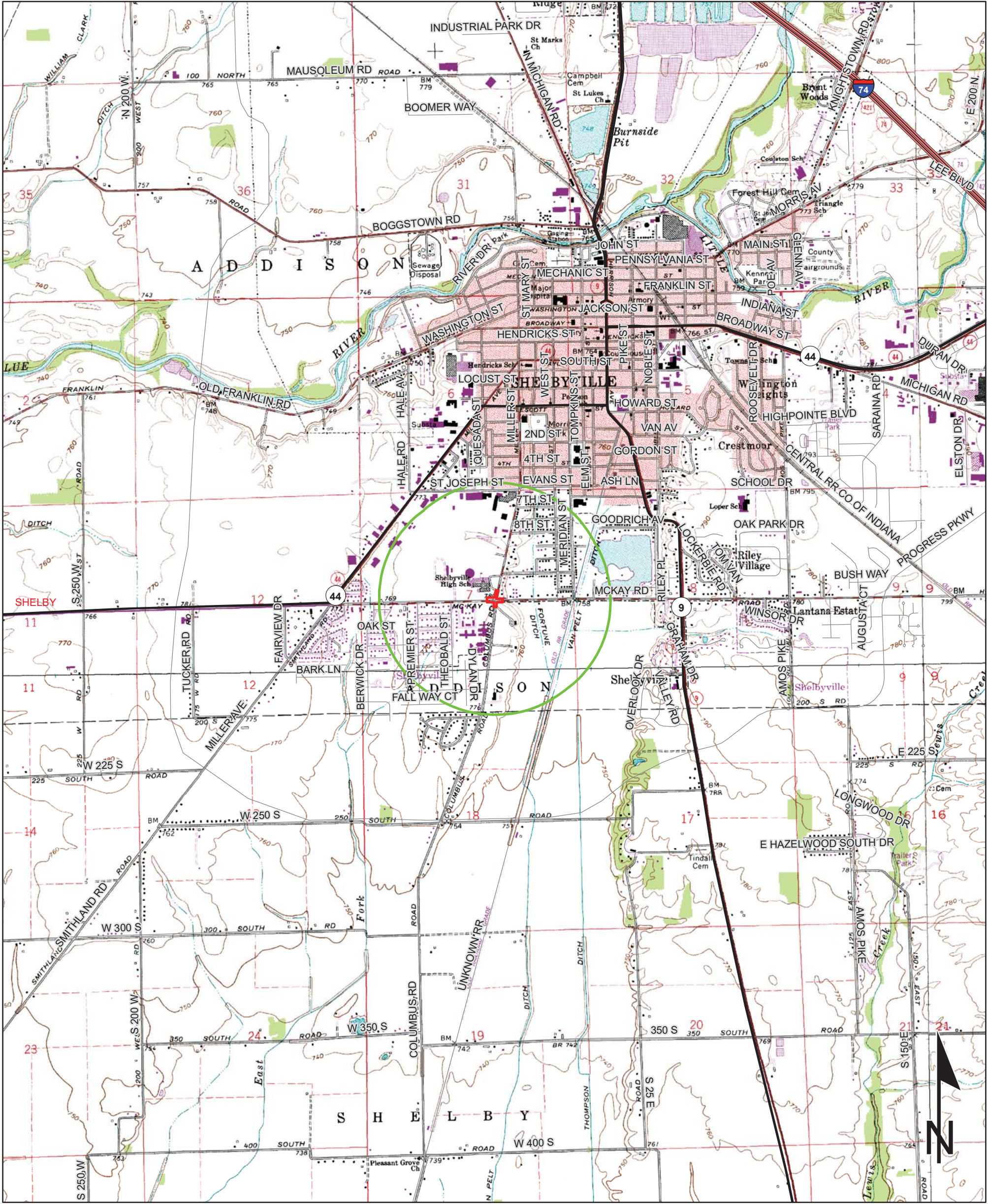
URBANIZED AREA BOUNDARY: YES

MINING/MINERAL EXPLORATION: N/A

HAZARDOUS MATERIAL CONCERNS: YES



Red Flag Investigation - Site Location  
South Miller Street & West McKay Road  
Des. No. 1702775, Intersection Improvement  
Shelby County, Indiana



Sources: 0.55 0.275 0 0.55 Miles  
**Non Orthophotography**  
**Data** - Obtained from the State of Indiana Geographical Information Office Library  
**Orthophotography** - Obtained from Indiana Map Framework Data ([www.indianamap.org](http://www.indianamap.org))  
**Map Projection:** UTM Zone 16 N **Map Datum:** NAD83  
This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

**SHELBYVILLE AND LEWIS CREEK  
QUADRANGLES INDIANA  
7.5 MINUTE SERIES  
(TOPOGRAPHIC)**



Red Flag Investigation - Infrastructure  
South Miller Street & West McKay Road  
Des. No. 1702775, Intersection Improvement  
Shelby County, Indiana



Sources: 0.15 0.075 0 0.15 Miles  
**Non Orthophotography**  
**Data** - Obtained from the State of Indiana Geographical Information Office Library  
**Orthophotography** - Obtained from Indiana Map Framework Data ([www.indianamap.org](http://www.indianamap.org))  
**Map Projection:** UTM Zone 16 N **Map Datum:** NAD83  
This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

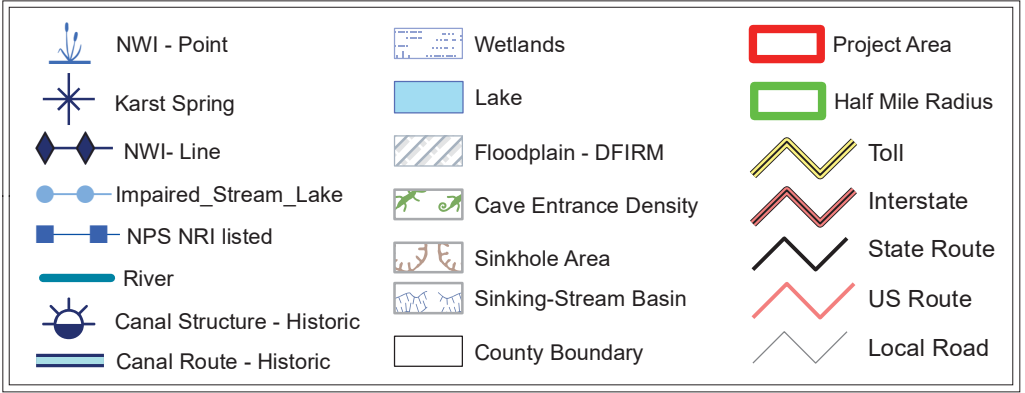
	Religious Facility		Recreation Facility		Project Area
	Airport		Pipeline		Half Mile Radius
	Cemeteries		Railroad		Toll
	Hospital		Trails		Interstate
	School		Managed Lands		State Route
			County Boundary		US Route
					Local Road



Red Flag Investigation - Water Resources  
South Miller Street & West McKay Road  
Des. No. 1702775, Intersection Improvement  
Shelby County, Indiana



Sources: 0.15 0.075 0 0.15 Miles  
**Non Orthophotography**  
Data - Obtained from the State of Indiana Geographical Information Office Library  
**Orthophotography** - Obtained from Indiana Map Framework Data ([www.indianamap.org](http://www.indianamap.org))  
Map Projection: UTM Zone 16 N Map Datum: NAD83  
This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.





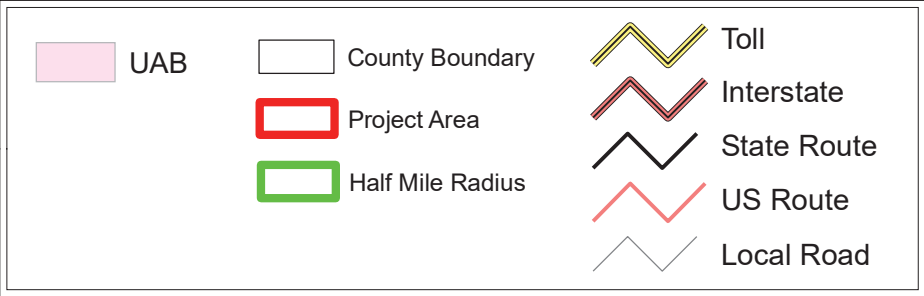
Red Flag Investigation - Urbanized Area Boundary  
South Miller Street & West McKay Road  
Des. No. 1702775, Intersection Improvement  
Shelby County, Indiana



**Sources:**  
**Non Orthophotography**  
**Data** - Obtained from the State of Indiana Geographical Information Office Library  
**Orthophotography** - Obtained from Indiana Map Framework Data ([www.indianamap.org](http://www.indianamap.org))  
**Map Projection:** UTM Zone 16 N **Map Datum:** NAD83

0.15 0.075 0 0.15 Miles

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.





Red Flag Investigation - Hazardous Material Concerns  
South Miller Street & West McKay Road  
Des. No. 1702775, Intersection Improvement  
Shelby County, Indiana



	Brownfield		RCRA Generator/TSD		Institutional Controls
	RCRA Corrective Action Sites		Restricted Waste Site		County Boundary
	Confined Feeding Operation		Septage Waste Site		Project Area
	Notice_Of_Contamination		Solid Waste Landfill		Half Mile Radius
	Construction/Demolition Site		State Cleanup Site		Toll
	Infectious/Medical Waste Site		Superfund		Interstate
	Leaking Underground Storage Tank		Tire Waste Site		State Route
	Manufactured Gas Plant		Underground Storage Tank		US Route
	NPDES Facilites		Voluntary Remediation Program		Local Road
	NPDES Pipe Locations		Waste Transfer Station		
	Open Dump Waste Site				



# Indiana County Endangered, Threatened and Rare Species List

## County: Shelby

Species Name	Common Name	FED	STATE	GRANK	SRANK
<b>Mollusk: Bivalvia (Mussels)</b>					
Epioblasma rangiana	Northern Riffleshell	LE	SE	G2	S1
Epioblasma triquetra	Snuffbox	LE	SE	G3	S1
Lampsilis fasciola	Wavyrayed Lampmussel		SSC	G5	S3
Obovaria subrotunda	Round Hickorynut	C	SE	G4	S1
Pleurobema clava	Clubshell	LE	SE	G1G2	S1
Ptychobranchus fasciolaris	Kidneyshell		SSC	G4G5	S2
Quadrula cylindrica cylindrica	Rabbitsfoot	LT	SE	G3G4T3	S1
Simpsonaias ambigua	Salamander Mussel	C	SSC	G3	S2
Toxolasma lividus	Purple Lilliput	C	SSC	G3Q	S2
Villosa fabalis	Rayed Bean	LE	SE	G2	S1
Villosa lienosa	Little Spectaclecase		SSC	G5	S3
<b>Insect: Odonata (Dragonflies &amp; Damselflies)</b>					
Enallagma divagans	Turquoise Bluet		SR	G5	S3
<b>Bird</b>					
Haliaeetus leucocephalus	Bald Eagle		SSC	G5	S2
<b>Mammal</b>					
Myotis sodalis	Indiana Bat	LE	SE	G2	S1
Taxidea taxus	American Badger		SSC	G5	S2
<b>Vascular Plant</b>					
Crataegus coccinea var. coccinea	Scarlet Hawthorn		ST	G5	S2
<b>High Quality Natural Community</b>					
Forest - flatwoods central till plain	Central Till Plain Flatwoods		SG	G3	S2
<b>Other Significant Feature</b>					
Geomorphic - Nonglacial Erosional Feature - Water Fall and Cascade	Water Fall and Cascade			GNR	SNR

Indiana Natural Heritage Data Center  
Division of Nature Preserves  
Indiana Department of Natural Resources  
This data is not the result of comprehensive county surveys.

Fed: LE = Endangered; LT = Threatened; C = candidate; PDL = proposed for delisting  
State: SE = state endangered; ST = state threatened; SR = state rare; SSC = state species of special concern; SX = state extirpated; SG = state significant; WL = watch list  
GRANK: Global Heritage Rank: G1 = critically imperiled globally; G2 = imperiled globally; G3 = rare or uncommon globally; G4 = widespread and abundant globally but with long term concerns; G5 = widespread and abundant globally; G? = unranked; GX = extinct; Q = uncertain rank; T = taxonomic subunit rank  
SRANK: State Heritage Rank: S1 = critically imperiled in state; S2 = imperiled in state; S3 = rare or uncommon in state; G4 = widespread and abundant in state but with long term concern; SG = state significant; SH = historical in state; SX = state extirpated; B = breeding status; S? = unranked; SNR = unranked; SNA = nonbreeding status unranked

# **APPENDIX F**

## **Water Resources**

# **WATERS DETERMINATION REPORT**

**SOUTH MILLER ST. AND MCKAY RD.  
INTERSECTION IMPROVEMENT  
DES. NO. 1702775  
ADDISON TOWNSHIP, SHELBY COUNTY, INDIANA**

**Prepared for:**  
Zimmer Construction Engineering, LLC

April 29, 2020



**Prepared by:**

**Metric Environmental, LLC**

**Complex Environment. Creative Solutions.**

6971 Hillsdale Court  
Indianapolis, IN 46256  
Telephone: 317.207.4286  
[www.metricenv.com](http://www.metricenv.com)

## **WATERS OF THE U.S. DETERMINATION REPORT**

**South Miller St. and McKay Rd.  
Intersection Improvement  
Addison Township, Shelby County, Indiana  
Des. No. 1702775  
Prepared By: Zachary Root, Metric Environmental, LLC  
April 29, 2020**

**Date of Waters Field Investigation:** September 9, 2019

**Location:**

Section 7; Township 12 North; Range 7 East  
Shelbyville, IN 7.5 minute USGS Topographic Quadrangles (**Exhibit 2**)  
Addison Township, Shelby County, Indiana  
12-Digit HUC Watersheds: 051202050503 and 051202050502  
Latitude: 39.504700 Longitude: -85.786973

**FEMA Flood Insurance Rate Map (FIRM):**

No mapped floodplains are located within the project study limits (PSL). The nearest mapped floodplain was located approximately 56 ft. southeast of the PSL and was associated with Fortune Ditch. The FIRM map for this area is provided as **Exhibit 3**.

**National Wetlands Inventory (NWI) Information:**

No mapped NWI polygons are located within the PSL. The nearest mapped NWI polygon is located approximately 330 ft. east of the PSL and was associated with Fortune Ditch. The NWI map is provided as **Exhibit 3**.

**Karst Feature Information:**

No mapped karst features were found within 0.5 mi. of the PSL during the desktop review.

**USGS National Hydrography Dataset (NHD) Information:**

No mapped NHD flowlines are located within the PSL.

**Soils:**

According to the Natural Resources Conservation Service (NRCS) Soil Survey Geographic (SSURGO) Database for Shelby County, Indiana, the PSL contained five mapped soil units, listed in the table below. The NRCS soil survey map is provided as **Exhibit 3**.

South Miller St. and McKay Rd.  
Intersection Improvement  
Des. No. 1702775  
Addison Township, Shelby County, Indiana  
Metric Project No. 18-0147





Map Unit Symbol	Map Unit Name	Hydric Rating (%)
Br	Brookston silty clay loam, 0 to 2 percent slopes	Hydric (10)
CrA	Crosby silt loam, 0 to 2 percent slopes	Hydric (7)
HeF	Hennepin loam, 25 to 50 percent slope	Not Hydric (0)
Sm	Sleeth loam	Hydric (10)
Wc	Westland clay loam, 0 to 1 percent slopes	Hydric (95)

#### Attached Documents:

Maps of the project area (**Exhibits 1-4**)

Photo Location Map (**Exhibit 5**)

Site Photographs

Wetland Determination Data Form(s)

Preliminary Jurisdictional Determination Form

#### Project Description:

The proposed project (Des. 1702775) includes upgrading the existing intersection of McKay Rd. and S. Miller St., located in Addison Township, Shelby County, Indiana. The intersection currently has an all-way stop control with the south, east, and north legs having one approach lane. The west approach is one lane and has an unmarked additional width. The project includes an upgrade to a single-lane roundabout.

#### Field Reconnaissance:

The wetland determination field visit was conducted on September 9, 2019 by Cory Shumate of Metric Environmental, LLC. The PSL consists of the area that has the potential to be impacted, based on the provided design scenario. This area was evaluated for the presence of wetlands and Waters of the United States. This investigation was conducted in accordance with the *1987 U.S. Army Corps of Engineers (USACE) Wetland Delineation Manual* and the *August 2010 Midwest Regional Supplement (version 2.0) Manual*.

A Location Map showing the project location is provided as **Exhibit 1**. The proposed project is located in the central portion of Shelby County, Indiana, at the McKay Rd. and S. Miller St. intersection. The PSL extended along McKay Rd. for approximately 1,104 ft. and extended along S. Miller St. approximately 1,004 ft. The PSL also extends from the centerline of the McKay Rd. and S. Miller Rd. intersection northwest approximately 180 ft., southwest approximately 245 ft., and southeast 212 ft. An aerial map of sampling points and water features is provided as **Exhibit 4**. A photo location map is provided as **Exhibit 5** and site photographs are attached.

The site was investigated for evidence of hydrophytic vegetation, hydric soil, and wetland hydrology to determine if the project impacts wetlands and other Waters of U.S. The sampling point (SP) locations were chosen in possible wetland areas within the PSL. The site was composed

South Miller St. and McKay Rd.  
Intersection Improvement  
Des. No. 1702775  
Addison Township, Shelby County, Indiana  
Metric Project No. 18-0147



of mostly residential and commercial properties throughout the PSL, which were dominated by maintained lawn consisting of tall false rye grass (*Schedonorus arundinaceus*, FACU), Kentucky blue grass (*Poa pratensis*, FAC) and red fescue (*Festuca rubra*, FACU). A few white mulberry trees were found in the eastern portion. One sampling point was taken and is identified as SP-1. The sampling point, recorded on the USACE Wetland Determination Data Form and shown on **Exhibit 4**, provided the following information:

**Sampling Plot Data Summary Table**

Plot #	Photo #s	Lat/Long	Hydrophytic Vegetation	Hydric Soils	Wetland Hydrology	Within Wetland
SP-1	24-26	39.50462 -85.78537	No	No	No	No

**Streams:**

No streams were observed within the PSL during the field reconnaissance.

**Wetlands:**

No wetlands were identified within the PSL during field reconnaissance.

**Additional Sampling Point(s):**

An additional sampling point was taken in an area where a wetland was suspected but did not meet the three required wetland criteria. A description of this sampling point is included below.

**Sampling Point 1 (SP-1)**

SP-1 was located at the toe of a convex slope south of McKay Rd. The dominant vegetation at this sampling point was white mulberry (*Morus alba*, FAC) in the tree stratum, gray dogwood (*Cornus racemosa*, FAC) and black elder (*Sambucus nigra*, FAC) in the sapling/shrub stratum, tall false rye grass (*Schedonorus arundinaceus*, FACU) and white clover (*Trifolium repens*, FACU) in the herb stratum, and Japanese honeysuckle (*Lonicera japonica*, FACU) in the woody vine stratum. This didn't meet any of the hydrophytic vegetation indicators. To a depth of 20 in., the soil in the test pit was a silt loam and exhibited a matrix color of 10YR 4/2 (70 percent) with 10YR 5/2 (30 percent) faint redox concentrations. This did not meet any of the hydric soil indicators. No indicators of wetland hydrology were observed. Since none of the three required wetland criteria were met, this area did not qualify as a wetland.

**Culverts and Drains:**

One culvert was identified within the PSL. The culvert was a corrugated metal pipe (CMP) under a residential driveway. This culvert served to aid in roadside drainage and stormwater conveyance and did not carry jurisdictional waters due to a lack of an OHWM, bed and bank, and lack of a significant nexus to any jurisdictional Waters of the U.S. Location of the culvert is shown on **Exhibits 4 and 5**, and attached photosheet.

South Miller St. and McKay Rd.  
Intersection Improvement  
Des. No. 1702775  
Addison Township, Shelby County, Indiana  
Metric Project No. 18-0147

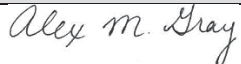




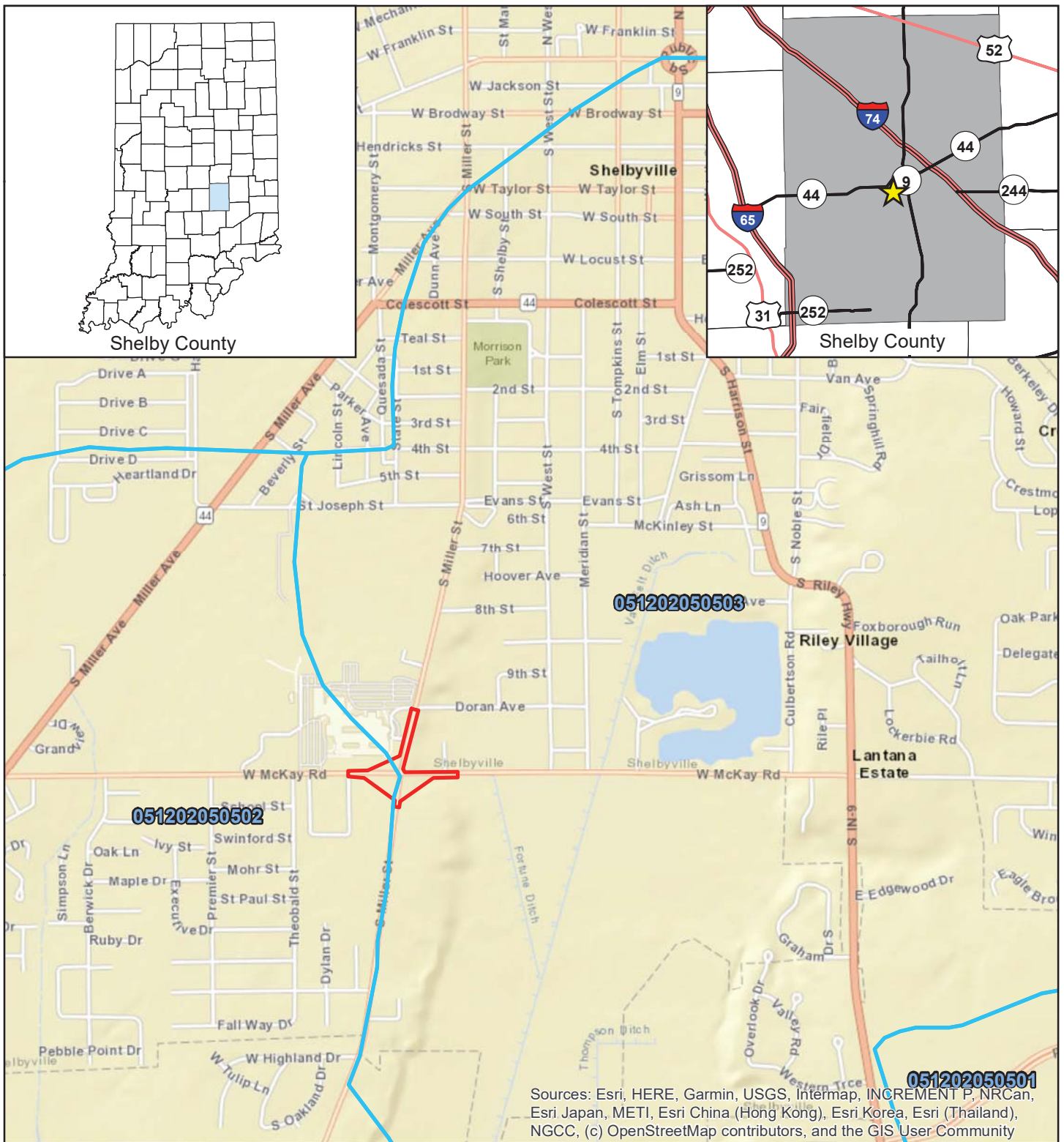
**Conclusion:**

No wetlands or streams were identified within the project study limits. If the project area changes to areas that could potentially impact wetlands and Waters of the U.S., additional delineation might be required. Should impacts become necessary, mitigation might be required. The INDOT Environmental Services Division should be contacted immediately if impacts will occur. The final determination of jurisdictional waters is ultimately made by the U.S. Army Corps of Engineers. This report is our best judgment based on the guidelines set forth by the Corps.

**Acknowledgements:**

This waters determination has been prepared based on the best available information, interpreted in light of the investigator's training, experience and professional judgement in conformance with the 1987 *Corps of Engineers Wetlands Delineation Manual*, the appropriate regional supplement, the USACE *Jurisdictional Determination Form Instructional Guidebook*, and other appropriate agency guidelines.

Metric Environmental Staff	Position	Contributing Effort	Signature/Date
Alex Gray	Natural Resources Project Manager I	QAQC	 4/29/20
Zachary Root	Environmental Scientist 2	Field Data Collection, Report Preparation	 4/29/2020
Cory Shumate	Environmental Scientist 2	Field Data Collection	 4/29/2020



  12-Digit HUC Watershed   Project Study Limits (PSL)

Exhibit 1 - Location Map  
 South Miller St. and McKay Rd.  
 Intersection Improvement  
 Addison Township, Shelby County, IN  
 Des. No.1702775  
 Metric Project No. 18-0147  
 Map Date: 9/3/2019  
 Map Author: Zachary Root

All locations approximate  
 2018 Basemap  
 Latitude: 39.504700 Longitude: -85.786973

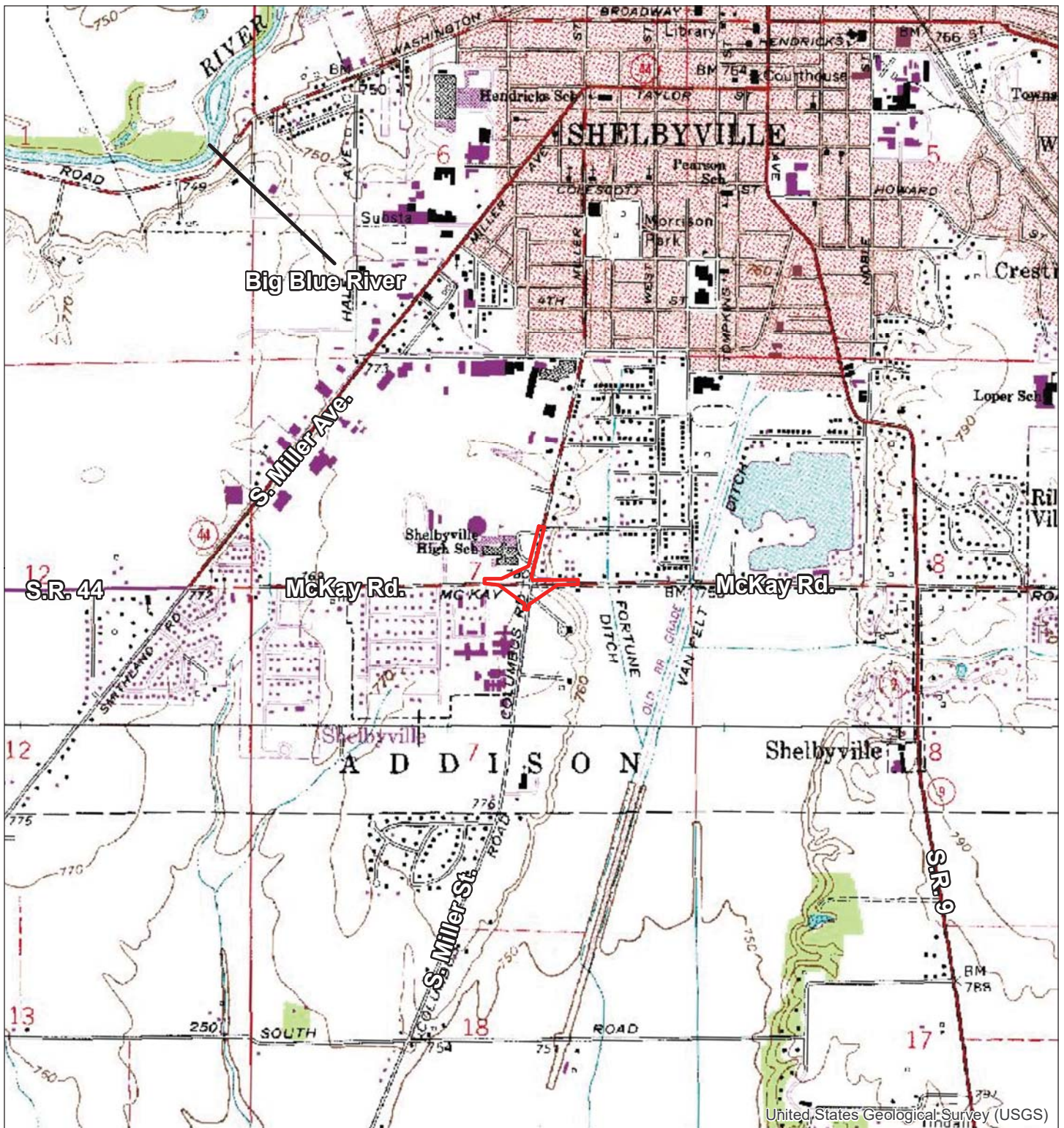


0 0.125 0.25 0.5  
 Miles



Exh. 1





Project Study Limits (PSL)

Exhibit 2A - USGS Topographic Map - Small Scale  
Shelbyville, IN 7.5 minute Quadrangle  
South Miller St. and McKay Rd.  
Intersection Improvement  
Addison Township, Shelby County, IN  
Des. No. 1702775  
Metric Project No. 18-0147  
Map Date: 9/3/2019  
Map Author: Zachary Root

All locations approximate  
Source: Indiana Spatial Data Portal (1980)

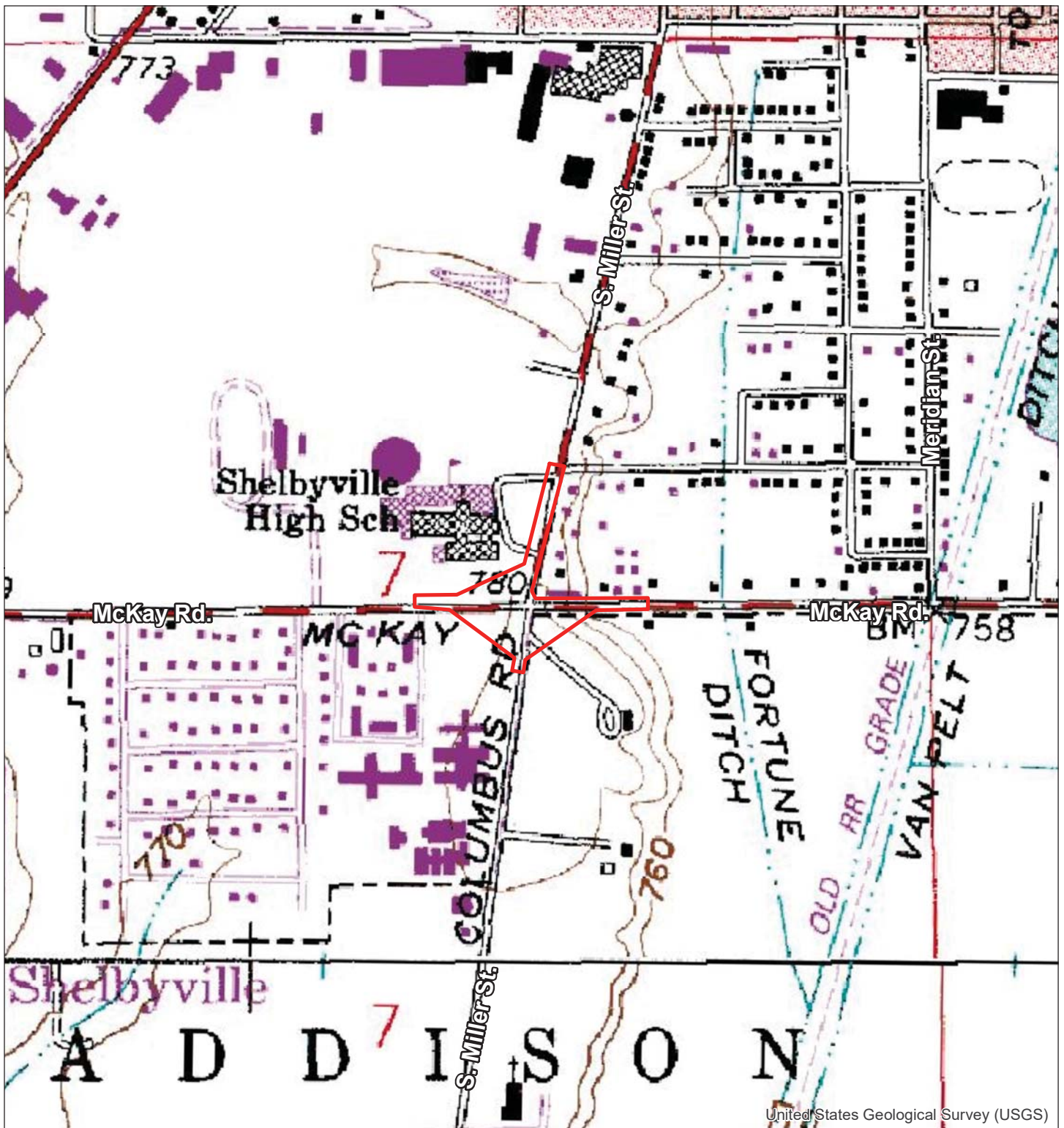


0 650 1,300 2,600  
Feet



Exh. 2A





Project Study Limits (PSL)

Exhibit 2B - USGS Topographic Map - Large Scale  
 Shelbyville, IN 7.5 minute Quadrangle  
 South Miller St. and McKay Rd.  
 Intersection Improvement  
 Addison Township, Shelby County, IN  
 Des. No.1702775  
 Metric Project No. 18-0147  
 Map Date: 4/9/2020  
 Map Author: Zachary Root

All locations approximate  
 Source: Indiana Spatial Data Portal (1980)

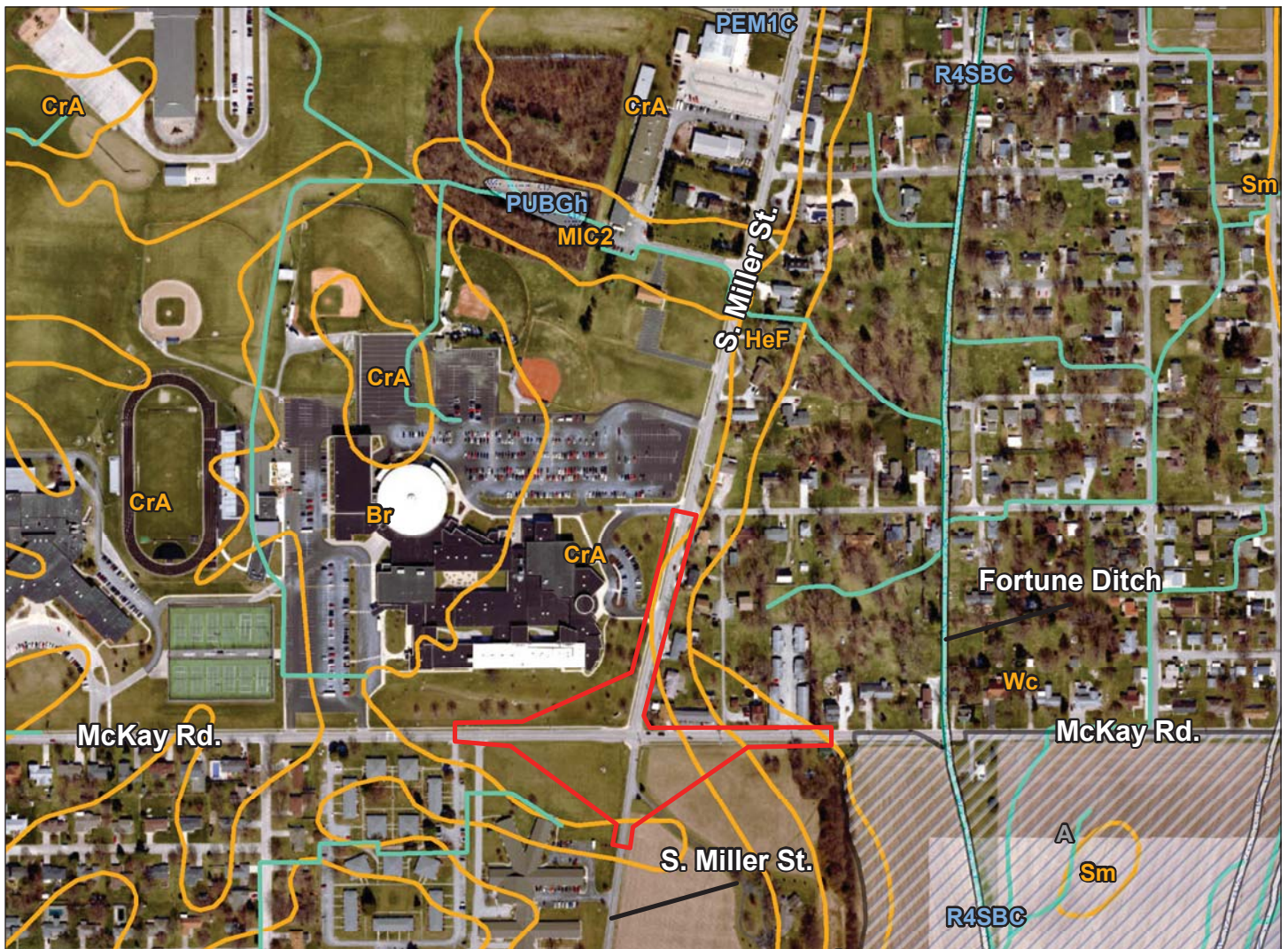


0 265 530 1,060  
 Feet



Exh. 2B





Symbol	Map Unit Name	Hydric Rating
Br	Brookston silty clay loam, 0 to 2 percent slopes	Hydric (95%)
CrA	Crosby silt loam, 0 to 2 percent slopes	Hydric (7%)
HeF	Hennepin loam, 25 to 50 percent slopes	Not Hydric (0%)
Sm	Sleeth loam	Hydric (10%)
Wc	Westland clay loam, 0 to 1 percent slopes	Hydric (95%)

Indiana Office of Information Technology, Indiana University Spatial Data Portal, UITS, Woolpert Inc.

- Project Study Limits (PSL)
- NWI Wetlands
- Floodplain - Zone A - 1% Chance Annual Flood
- NHD Flowline
- NRCS Soil Survey

Exhibit 3 - NWI, FIRM, NHD and NRCS Soil Map  
 South Miller St. and McKay Rd.  
 Intersection Improvement  
 Addison Township, Shelby County, IN  
 Des. No.1702775  
 Metric Project No. 18-0147  
 Map Date: 4/9/2020  
 Map Author: Zachary Root

All locations approximate  
 Source: Indiana Spatial Data Portal (2017)

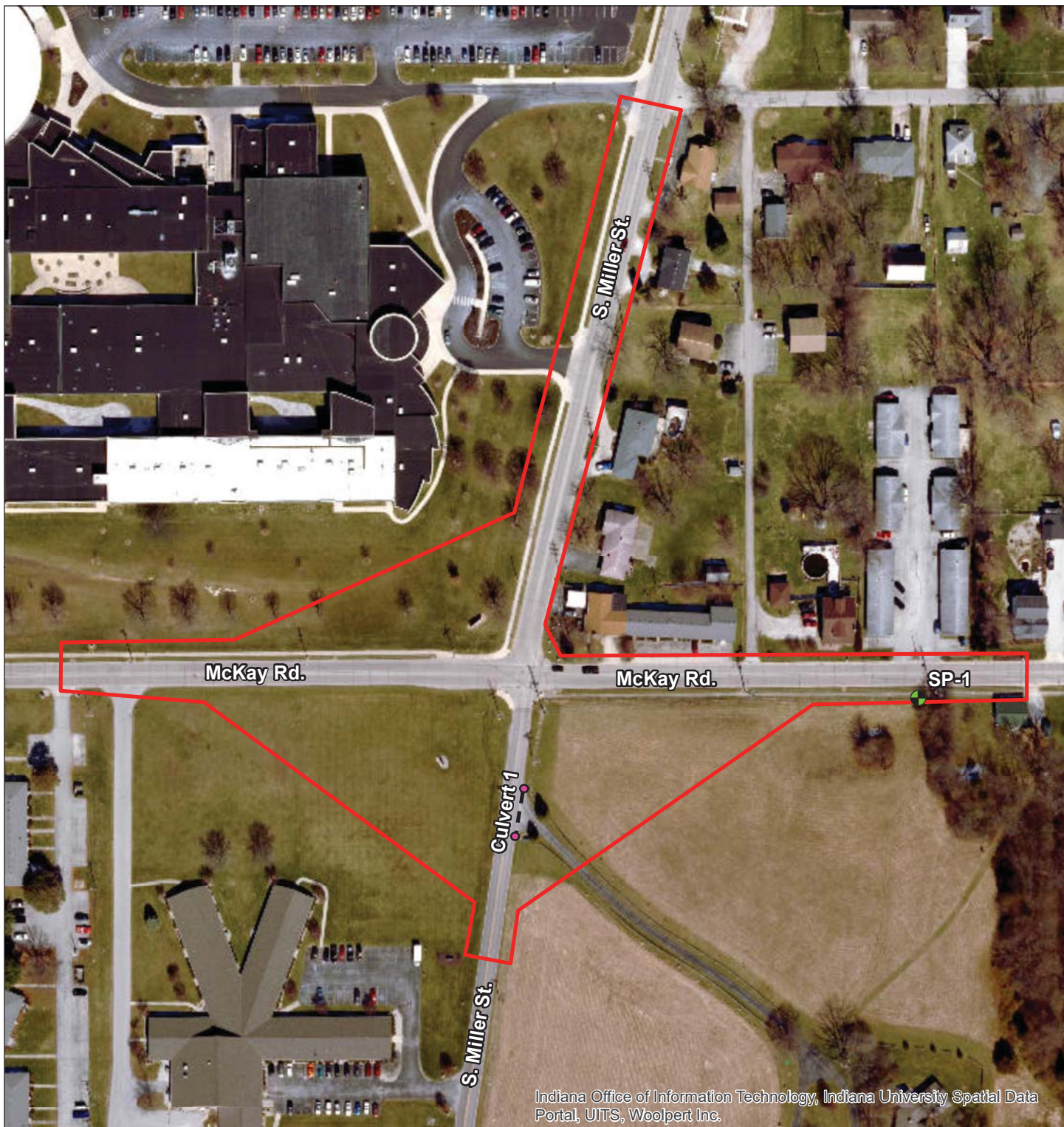


0 210 420 840  
 Feet



Exh. 3

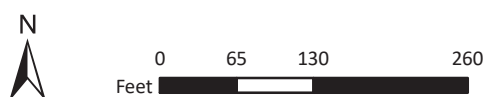




- Sampling Point (SP)
- Culvert Opening
- Roadside Ditch (RSD)
- Project Study Limits (PSL)
- Culvert (CV)

Exhibit 4 - Waters Delineation Map  
 South Miller St. and McKay Rd.  
 Intersection Improvement  
 Addison Township, Shelby County, IN  
 Des. No.1702775  
 Metric Project No. 18-0147  
 Map Date: 4/9/2020  
 Map Author: Zachary Root

All locations approximate  
 Source: Indiana Spatial Data Portal (2017)



Exh. 4









1. View from the western project study limits (PSL), looking southeast.



2. View of McKay Rd. right-of-way from the western PSL, looking east.



3. View of McKay Rd. ROW from the western PSL, looking east.



4. View from the western PSL, looking northeast.

#### **SITE PHOTOGRAPHS—9/9/2019**

South Miller St. and McKay Rd.  
Intersection Improvement  
Addison Township, Shelby County, Indiana  
Des. No. 1702775







5. View of upland area from the northwestern PSL, looking southwest.



6. View of upland area and the McKay Rd., S. Miller St. intersection from the northwestern PSL, looking southeast.



7. View of upland area from the northwestern PSL, looking east.



8. View of upland area, looking west.

**SITE PHOTOGRAPHS—9/9/2019**

South Miller St. and McKay Rd.

Intersection Improvement

Addison Township, Shelby County, Indiana

Des. No. 1702775







9. View of upland area, looking northwest.



10. View of McKay Rd. ROW, looking west.



11. View of S. Miller Rd. ROW, looking northeast.



12. View of S. Miller St. ROW, looking southwest.

**SITE PHOTOGRAPHS—9/9/2019**

South Miller St. and McKay Rd.

Intersection Improvement

Addison Township, Shelby County, Indiana

Des. No. 1702775





13. View of S. Miller St. ROW, looking northeast.



14. View of S. Miller St. ROW from the northern PSL, looking southwest.



15. View of S. Miller St. ROW from the northern PSL, looking southwest.



16. View of S. Miller St. ROW, looking northeast.

**SITE PHOTOGRAPHS—9/9/2019**

South Miller St. and McKay Rd.  
Intersection Improvement  
Addison Township, Shelby County, Indiana  
Des. No. 1702775







17. View of S. Miller St. ROW, looking southwest.



18. View of S. Miller St. ROW, looking northeast.



19. View of McKay Rd. ROW, looking east.



20. View of McKay Rd. ROW, looking west.

**SITE PHOTOGRAPHS—9/9/2019**

South Miller St. and McKay Rd.  
Intersection Improvement  
Addison Township, Shelby County, Indiana  
Des. No. 1702775







21. View of McKay Rd. ROW, looking east.



22. View of McKay Rd. ROW from the eastern PSL, looking west.



23. View of McKay Rd. ROW from the eastern PSL, looking west.



24. View of SP-1, upland sampling point, soil profile.

**SITE PHOTOGRAPHS—9/9/2019**

South Miller St. and McKay Rd.  
Intersection Improvement  
Addison Township, Shelby County, Indiana  
Des. No. 1702775







25. View of SP-1, upland sampling point, looking west.



26. View of SP-1, upland sampling point, looking east.



27. View of McKay Rd. ROW, looking east.



28. View of McKay Rd. ROW, looking west.

**SITE PHOTOGRAPHS—9/9/2019**

South Miller St. and McKay Rd.  
Intersection Improvement  
Addison Township, Shelby County, Indiana  
Des. No. 1702775







29. View of upland area from the southeastern PSL, looking northeast.



30. View of upland area and the McKay Rd, S. Miller St. intersection from the southeastern, looking northwest.



31. View of upland area from the southeastern PSL, looking southwest.



32. View of McKay Rd. ROW, looking east.

**SITE PHOTOGRAPHS—9/9/2019**

South Miller St. and McKay Rd.  
Intersection Improvement  
Addison Township, Shelby County, Indiana  
Des. No. 1702775







33. View of S. Miller St. ROW, looking southwest.



34. View of Culvert 1, and S. Miller St. ROW, looking northeast.



35. View of Culvert 1 and S. Miller St. ROW, looking southwest.



36. View of S. Miller St. ROW from the southern PSL, looking northeast.

**SITE PHOTOGRAPHS—9/9/2019**

South Miller St. and McKay Rd.

Intersection Improvement

Addison Township, Shelby County, Indiana

Des. No. 1702775







37. View of S. Miller St. ROW from the southern PSL, looking northeast.



38. View of upland area from the southwestern PSL, looking northeast.



39. View of upland area and McKay Rd., S. Miller St intersection from the southeastern PSL, looking northeast.



40. View of upland area from the southwestern PSL, looking northwest.

**SITE PHOTOGRAPHS—9/9/2019**

South Miller St. and McKay Rd.  
Intersection Improvement  
Addison Township, Shelby County, Indiana  
Des. No. 1702775





41. View of S. Miller St. ROW, looking southwest.



41. View of McKay Rd. ROW, looking west.

**SITE PHOTOGRAPHS—9/9/2019**

South Miller St. and McKay Rd.  
Intersection Improvement  
Addison Township, Shelby County, Indiana  
Des. No. 1702775





# **APPENDIX G**

## **Public Involvement**



**CORPORATE - MARION Office**

145 E. Third Street, Marion, IN 46952  
Ph (765) 662-1284 Fax (765) 668-7412  
marion@rmasurveying.com

**WABASH Office**

261 S. Wabash Street, Wabash, IN 46992  
Ph (260) 563-4467 Fax (260) 563-0462  
wabash@rmasurveying.com

**MUNCIE Office**

333 E. Washington Street, Muncie, IN 47305  
Ph (765) 289-1917 Fax (765) 289-1931  
muncie@rmasurveying.com

**MONTICELLO Office**

134 N. Main Street, Monticello, IN 47960  
Ph (574) 583-0478 Fax (574) 583-0484  
monticello@rmasurveying.com

June 21, 2019

## NOTICE OF SURVEY

**South Miller Street & McKay Road Intersection Improvement  
City of Shelbyville, Indiana  
DES No. 1702775**

Dear Property Owner:

Our information indicates that you own or occupy property near the subject proposed sidewalk project. Our employees will be performing a survey of the project area in the near future. It may be necessary for them to come onto your property to complete this work. This is permitted by law per Indiana Code IC 8-23-7-26. They will show you their identification, if you are available, before coming onto your property. If you have sold this property, or it is occupied by someone else, please let us know the name and address of the new owner or current occupant so we can contact them about the survey.

At this stage, we generally do not know what effect, if any, our project can eventually have on your property. If we determine later that your property is involved, we will contact you with additional information.

The survey work will include mapping the location of features such as trees, buildings, fences and drives, and obtaining ground elevations. The survey is needed for the proper planning and design of this intersection improvement project. Please be assured of our sincere desire to cause you as little inconvenience as possible during this survey. If problems do occur, please contact our field crew or contact me at 765-662-1284, x7001.

Sincerely yours,

Jason Miller, PLS, PE, CFM, CPESC  
President/Director of Engineering

# **APPENDIX H**

## **Air Quality**



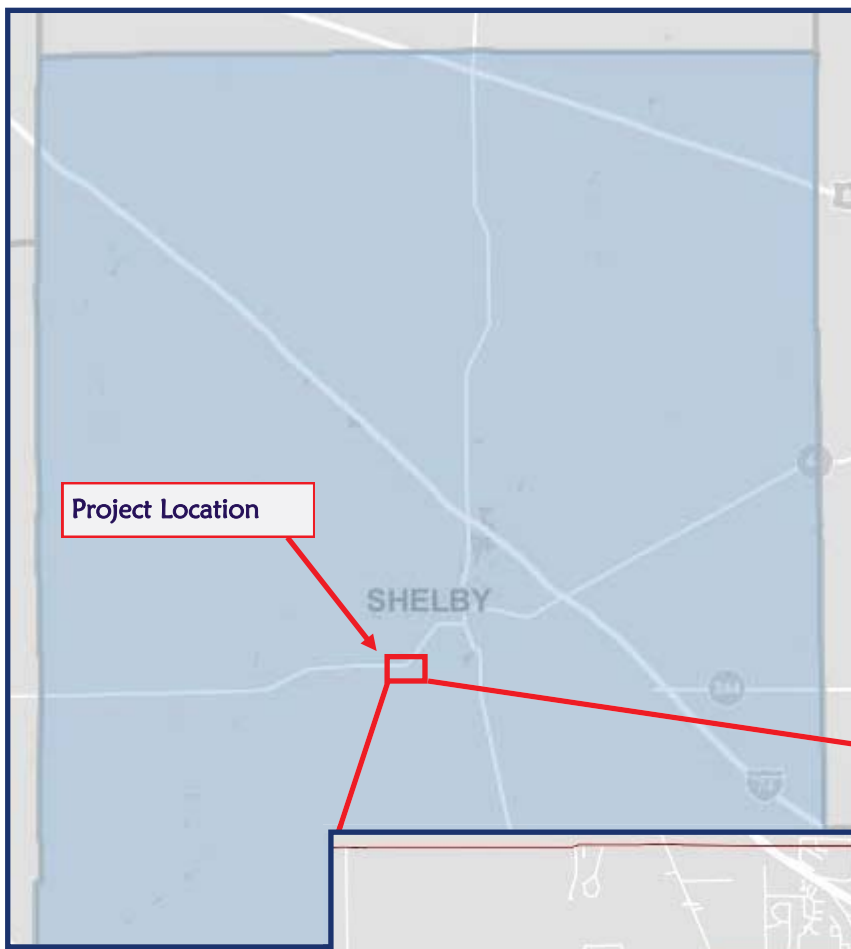
Indiana Department of Transportation (INDOT)  
State Preservation and Local Initiated Projects FY 2020 - 2024

SPONSOR	CONTR ACT # / LEAD DES	STIP NAME	ROUTE	WORK TYPE	LOCATION	DISTRICT	MILES	FEDERAL CATEGORY	Estimated Cost left to Complete Project*	PROGRAM	PHASE	FEDERAL	MATCH	2020	2021	2022	2023	2024
Indiana Department of Transportation	40506 / 1601973	A 01	I 74	Road Reconstruction (3R/4R Standards)	From 0.11 mi W of SR 244 to 0.11 mi E of SR 244	Greenfield	.22	STBG	\$5,712,129.00	Road Consulting	PE	\$33,210.00	\$3,690.00	\$36,900.00				
Comments:Added PE Phase																		
Indiana Department of Transportation	40506 / 1601973	A 44	I 74	Road Reconstruction (3R/4R Standards)	From 0.11 mi W of SR 244 to 0.11 mi E of SR 244	Greenfield	.22	NHPP	\$15,088,061.00	Road Construction	CN	\$7,985,846.17	\$887,316.24		\$14,548,391.42	(\$5,675,229.00)		
Comments:A Increase CN \$5,675,229.00 \$14,548,391 move from 2022 2021 IMPO Modification Letter 1/21/2021 AQC Exempt 12/2/2020 Lead Des 1601973 is not in the MPO area but des 1601978 and des 1601980 are in the MPO																		
Indiana Department of Transportation	40546 / 1700957	Init.	I 74	Bridge Deck Overlay	Rushville Road over I-74, 00.86 W SR 44	Greenfield	0	NHPP		Bridge Construction	CN	\$1,161,760.50	\$129,084.50	\$1,290,845.00				
Indiana Department of Transportation	40595 / 1700800	Init.	SR 44	HMA Overlay, Preventive Maintenance	from SR 9 N Jct. to 0.28 Mi. E of SR 9	Greenfield	.28	NHPP		Road Construction	CN	\$147,068.00	\$36,767.00	\$183,835.00				
										Safety Construction	CN	\$721,084.00	\$180,271.00	\$901,355.00				
Indiana Department of Transportation	41106 / 1800772	Init.	US 52	HMA Overlay, Preventive Maintenance	from 3.66 mi E of SR 9 (WCL Morristown) to 4.9 mi E of SR 9 (ECL Morristown)	Greenfield	1.231	STPBG		Road Construction	CN	\$536,871.20	\$134,217.80		\$671,089.00			
Indiana Department of Transportation	41165 / 1800505	Init.	I 74	Bridge Painting	WB over SR 44	Greenfield	0	NHPP		Bridge Construction	CN	\$203,658.30	\$22,628.70		\$226,287.00			
Shelbyville	41302 / 1702775	Init.	ST 1023	Intersection Improvement, Roundabout	The Intersection of Miller St and McKay Road in Shelbyville Indiana	Greenfield	.5	STPBG		Group III Program	RW	\$103,200.00	\$0.00			\$103,200.00		
										Group III Program	CN	\$2,442,000.00	\$0.00				\$2,442,000.00	
										Local Funds	RW	\$0.00	\$25,800.00			\$25,800.00		
										Local Funds	CN	\$0.00	\$610,500.00				\$610,500.00	
Indiana Department of Transportation	41752 / 1800664	Init.	I 74	ITS Traffic Management Systems	CCTV/DMS from 4.0 miles E of I-465 to Shelby/Decatur County Line, 4.6 miles E of SR 244	Greenfield	25.971	NHPP		Statewide Construction	CN	\$1,890,000.00	\$210,000.00				\$2,100,000.00	
										Statewide Consulting	PE	\$189,000.00	\$21,000.00			\$210,000.00		
Indiana Department of Transportation	42116 / 1900658	A 25	I 65	Bridge Painting	NBL over Big Blue River	Seymour	0	NHPP	\$1,260,148.00	Bridge Construction	CN	\$1,134,133.20	\$126,014.80			\$1,260,148.00		
Comments:IMPO. Add CN phase of \$1,260,148. Includes baby des of 1900659. IMPO Resolution 20-IMPO-010 dated 6/1/2020. AQC approved 6/24/2020																		
Indiana Department of Transportation	42211 / 1901538	A 01	SR 9	Bridge Thin Deck Overlay	over N BRANCH LEWIS CREEK, 04.54 S SR 44	Greenfield	0	STBG	\$348,000.00	Bridge Construction	CN	\$278,400.00	\$69,600.00			\$348,000.00		
Comments:Added CN																		
Indiana Department of Transportation	42260 / 1901508	A 01	SR 9	HMA Overlay, Preventive Maintenance	1.56 mi S of I-74 to 0.29 mi N of I-74	Greenfield	1.85	STBG	\$1,280,000.00	Road Construction	CN	\$1,024,000.00	\$256,000.00			\$1,280,000.00		
Comments:Added CN																		

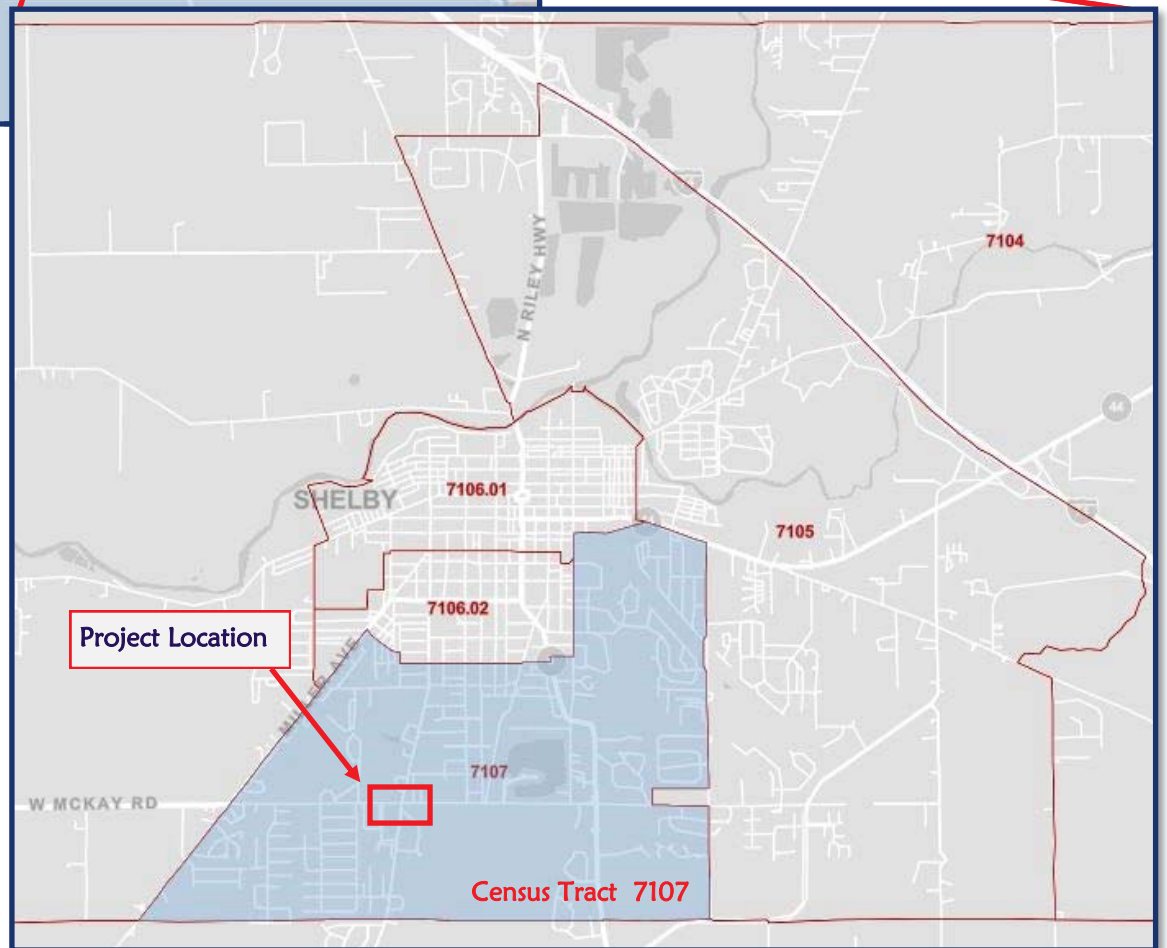
\*Estimated Costs left to Complete Project column is for costs that may extend beyond the four years of a STIP. This column is not fiscally constrained and is for information purposes.

# **APPENDIX I**

## **Environmental Justice**



Project Location



Project Location

Census Tract 7107

Source: U.S. Census Bureau

**Environmental Justice**  
Intersection Improvement Project  
Des. No. 1702775  
Miller Street and McKay Road  
Shelby County, Indiana





**HISPANIC OR LATINO ORIGIN BY RACE**

Survey/Program: American Community Survey  
TableID: B03002

Product: 2019: ACS 5-Year Estimates Detailed Tables  
Universe: Total population

	Shelby County, Indiana		Census Tract 7107, Shelby County, Indiana	
Label	Estimate	Margin of E...	Estimate	Margin of Error
▼ Total:	44,438	*****	6,400	±415
▼ Not Hispanic or Latino:	42,565	*****	6,201	±428
White alone	41,006	±85	6,006	±406
Black or African American alone	519	±142	108	±101
American Indian and Alaska Native alone	21	±35	0	±17
Asian alone	250	±110	71	±85
Native Hawaiian and Other Pacific Islander alone	0	±25	0	±17
Some other race alone	73	±85	16	±29
▼ Two or more races:	696	±178	0	±17
Two races including Some other race	0	±25	0	±17
Two races excluding Some other race, and three or more races	696	±178	0	±17
▼ Hispanic or Latino:	1,873	*****	199	±146

**POVERTY STATUS IN THE PAST 12 MONTHS BY SEX BY AGE**

Survey/Program: American Community Survey  
TableID: B17001

Product: 2019: ACS 5-Year Estimates Detailed Tables  
Universe: Population for whom poverty status is determined

	Shelby County, Indiana		Census Tract 7107, Shelby County, Indiana	
Label	Estimate	Margin of ...	Estimate	Margin of Error
▼ Total:	43,556	±183	6,108	±392
▼ Income in the past 12 months below poverty level:	4,903	±825	400	±204

Source: U.S. Census Bureau  
2019 American Community Survey 5 year Estimates; Shelby County, IN

**Environmental Justice**  
Intersection Improvement Project  
Des. No. 1702775  
Miller Street and McKay Road  
Shelby County, Indiana



US Census 2019 American Community Survey 5 year Estimates	Shelby County, Indiana	Census Tract 7107 Shelby County, Indiana
<b>LOW-INCOME</b>		
Population for whom poverty status is determined: Total	43,556	6,108
Population for whom poverty status is determined: Income in 2019 below poverty level	4,903	400
<b>Percent Low-Income</b> (Income in 2019 below poverty level) (Total population)	11.26%	6.55%
<b>125 Percent of COC</b> (125 x COC Percent Low-Income)	14.07%	<b>AC &lt; 125% COC</b>
<b>Low-Income EJ Impact</b>		<b>No</b>
<b>MINORITY</b>		
Total Population: Total	44,438	6,400
Not Hispanic or Latino	42,565	6,201
White alone	41,006	6,006
Black or African American alone	519	108
American Indian and Alaska Native alone	21	0
Asian alone	250	71
Native Hawaiian and Other Pacific Islander alone	0	0
Some other race alone	73	16
Two or more races	696	0
Hispanic or Latino	1,873	199
<b>Number Non-white/minority</b>	3,432	394
<b>Percent Non-white/Minority</b> (Total population - white alone) Total population	7.72%	6.16%
<b>125 Percent of COC</b> (125 x COC Percent Non-white/Minority)	9.65%	<b>AC &lt; 125% COC</b>
<b>Minority EJ Impact</b>		<b>No</b>

# **APPENDIX J**

## **Additional Studies**



**Land and Water Conservation Fund (LWCF) County Property List for Indiana (Last Updated July 2020)**

ProjectNumber	SubProjectCode	County	Property
1800537	1800537	Shelby	Blue River Park
1800544	1800544	Shelby	Blue River Park
1800548	1800548	Shelby	Blue River Park

\*Park names may have changed. If acquisition of publically owned land or impacts to publically owned land is anticipated, coordination with IDNR, Division of Outdoor Recreation, should occur.

**OPERATIONAL ANALYSIS**

**FOR**

**THE MILLER STREET AND**  
**MCKAY ROAD INTERSECTION**



City of  
Shelbyville  
Indiana

*May 25, 2018*

**PREPARED BY: Shrewsberry & Associates, LLC**  
**7321 Shadeland Station, Suite 160**  
**Indianapolis, IN 46256**

## **2.0** **Preparer Qualifications**

I certify that this Operations Analysis has been prepared by me, or under my immediate supervision, and that I have experience and training in the field of traffic and transportation planning.



A handwritten signature in cursive script that reads "Jill Palmer".

Jill Palmer, P.E.  
Indiana P.E. 10403866  
Shrewsberry & Associates, LLC  
(317) 841-4799



### **3.0 Introduction**

The intersection of Miller Street and McKay Road is located on the south side of the City of Shelbyville, Indiana between SR 44 and SR 9. The purpose of this report is to evaluate existing traffic conditions, project future traffic conditions, to identify any deficiencies, and to recommend improvement to address the deficiencies.

### **4.0 Existing Conditions**

The study intersection consists of McKay Road and Miller Street. McKay Road runs east and west, is currently classified as an arterial, and has a posted speed limit of 30 miles per hour (MPH). McKay Road is intersected by Miller Street which runs in a roughly north and south direction. South of McKay Road, Miller Street has been classified as a major collector, while north of McKay Road it is classified as an arterial. The posted speed limit along both legs of Miller Street is also 30 MPH.

The study intersection has an all-way stop control (AWSC) with the south, east, and north legs having one approach lane. The west approach has unmarked additional width that could allow a turn lane, but video of traffic flow shows that this approach functions as a single lane.

Shelbyville High School is located in the northwest quadrant of the study intersection. Also located nearby is Shelbyville Middle School and Thomas A. Hendricks Elementary School. Traffic from these schools was taken into consideration as a part of this operations analysis.



*Figure 1: Intersection Location*

## 5.0 Operations Analysis

For the operations analysis, Synchro<sup>1</sup> software was utilized to calculate capacity and level of service (LOS). The Synchro software is based on methodology outlined in the Highway Capacity Manual.<sup>2</sup> The LOS ratings indicate the average control delay for each movement or approach. Table 2 outlines the average thresholds for various types of intersection controls. A LOS A indicates the least amount of delay while a LOS F indicates the most congested conditions and the highest amount of delay. A LOS D or better is considered acceptable during peak periods.

Table 1: Level of Service Criteria

Level of Service (LOS)	Unsignalized Control Delay (seconds/vehicle)	Signalized Control Delay (seconds/vehicle)	Roundabout Control Delay (seconds/vehicle)
<b>A</b>	0 - 10	0 - 10	0 - 10
<b>B</b>	> 10 - 15	> 10 - 20	> 10 - 15
<b>C</b>	> 15 - 25	> 20 - 35	> 15 - 25
<b>D</b>	> 25 - 35	> 35 - 55	> 25 - 35
<b>E</b>	> 35 - 50	> 55 - 80	> 35 - 50
<b>F</b>	> 50	> 80	> 50

### 5.1 2018 Base Year

Traffic counts were taken at the study intersection in April 2018 on a day when school was in session. The traffic counts were taken for 13 hours from 7:00 AM – 8:00 PM. The morning peak hour was determined to be 7:30 – 8:30 AM. The afternoon peak, around the time of school dismissal, was 3:00 – 4:00 PM, and evening peak, including typical commuter traffic, was determined to be 4:45 – 5:45 PM. Traffic counts can be found in the attached Appendix A, and operation analysis reports can be found in Appendix B.

As shown in Table 2, the AWSC intersection of Miller Street and McKay Road currently operates at an overall unacceptable LOS during the morning and afternoon peaks. The evening peak operates better, but the westbound approach during this peak still operates at an unacceptable LOS. All three peak hours have movements that operate at a LOS less than D indicating congestion and delay are experienced by drivers at this intersection.

Table 2: 2018 All-Way Stop Control

Peak	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	Overall
<b>Morning</b>	<b>E</b>			<b>F</b>			<b>D</b>			<b>D</b>			<b>E</b>
<b>Afternoon</b>	<b>F</b>			<b>F</b>			<b>C</b>			<b>E</b>			<b>F</b>
<b>Evening</b>	<b>D</b>			<b>E</b>			<b>C</b>			<b>C</b>			<b>D</b>

### 5.2 2022 Construction Year

Traffic counts were forecasted to the proposed construction year of 2022 using a 2.0% annual growth rate. This rate is based on nearby Indiana Department of Transportation

<sup>1</sup> Synchro plus SimTraffic 10, Trafficware LLC

<sup>2</sup> Highway Capacity Manual, 6<sup>th</sup> Edition (HCM2016); Transportation Research Board, 2016.

traffic data history, and a review of land use within the area. At this time, Shelbyville Central School District anticipates a modest growth of 2% per year or less.

Table 3 shows the results of operations analysis with the existing AWSC in 2022. The congestion and delay continue to worsen with all three peak hour periods now operating at an overall unacceptable LOS.

Table 3: 2022 All-Way Stop Control

Peak	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	Overall
Morning	F			F			E			E			F
Afternoon	F			F			D			F			F
Evening	E			F			C			D			E

Two improvement alternatives were considered. Alternative A would be the construction of a single-lane roundabout at the intersection. Table 4 below shows that the installation of a single-lane roundabout will improve the overall LOS from an F in the morning peak to an A, from a LOS F in the afternoon peak to a B, and from a LOS E in the evening peak to a LOS A.

Table 4: 2022 Alternative A (Single-Lane Roundabout)

Peak	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	Overall
Morning	A			B			A			B			A
Afternoon	B			A			A			B			B
Evening	A			A			A			A			A

Alternative B would be the installation of a traffic control signal with dedicated left turn lanes. Per the *Indiana Design Manual*<sup>3</sup>, left-turn lanes are warranted on all approaches. Table 5 shows that the installation of a traffic signal with left-turn lanes will improve the overall LOS from an F in the morning peak to a LOS B, from a LOS F in the afternoon peak to a LOS B, and from a LOS E in the evening peak to a LOS B.

Table 5: 2022 Alternative B (Traffic Signal)

Peak	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	Overall
Morning	C	B		B	B		B	B		B	B		B
Afternoon	B	B		B	B		B	B		B	B		B
Evening	B	B		B	B		B	A		B	B		B

### 5.3 2042 Design Year

The 2.0% annual growth rate was used to forecast traffic volumes to 2042. The operations of the existing all-way stop control, Alternative A, and Alternative B were all analyzed for 2042.

The existing all-way stop control would experience significant delay and congestion in 2042. All approaches and movements now operate at a LOS F.

<sup>3</sup>Indiana Design Manual 2013; Chapter 46-4.01(02)



Table 6: 2042 All-Way Stop Control

Peak	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	Overall
Morning	F			F			F			F			F
Afternoon	F			F			F			F			F
Evening	F			F			F			F			F

Both the single lane roundabout (Alternative A), and the traffic signal with left-turn lanes (Alternative B) will operate at an acceptable LOS on all movements and approaches in 2042 as shown in the tables below.

Table 7: 2042 Alternative A (Single-Lane Roundabout)

Peak	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	Overall
Morning	B			D			C			C			C
Afternoon	D			C			C			D			C
Evening	B			C			B			B			B

Table 8: 2042 Alternative B (Traffic Signal)

Peak	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	Overall
Morning	D	B		B	B		C	B		C	B		B
Afternoon	C	B		C	B		C	B		C	B		B
Evening	C	B		B	B		B	B		B	B		B

## 6.0 Signal Warrants

Traffic signal warrants were conducted per the *Indiana Manual on Uniform Traffic Control Devices*<sup>4</sup> for the 2022 Construction Year and the 2042 Design Year. Warrant 1, Eight-Hour Vehicular Volume was evaluated against the projected traffic volumes.

In the 2022 construction year, a traffic control signal is not warranted under Condition A, Condition B, or a combination of Conditions A and B for Warrant 1, Eight-Hour Vehicular Volume.

In the 2042 design year a traffic control signal is not warranted under Condition A, Condition B, or a combination of Conditions A and B for Warrant 1, Eight-Hour Vehicular Volume. Traffic signal warrants can be found in Appendix C.

## 7.0 Recommendations

The recommended improvement for the intersection of Miller Street and McKay Road is Alternative A, a single-lane roundabout. The existing configuration of an all-way stop results in long delays. A roundabout will provide an acceptable LOS through the 2042 Design Year. While a traffic signal also provides acceptable LOS, signal warrants were not met in any of the analysis years, and so a signal should not be constructed.

<sup>4</sup> 2011 Indiana Manual on Uniform Traffic Control Devices with Revision 1 & 2 & 3